

**CITY OF CHESTERFIELD
PLANNING COMMISSION MEETING
MONDAY - MAY 14, 2007
7:00 PM**

AGENDA

I. ROLL CALL

II. INVOCATION

III. PLEDGE OF ALLEGIANCE

IV. PUBLIC HEARINGS

- A. P.Z. 11-2007 J&T Holdings (612 Cepi):** A request for a change of zoning from an "M3" Planned Industrial District to a "PC" Planned Commercial District for an approximately 0.75 acre tract of land located at 612 Cepi Drive south of the intersection of Chesterfield Airport Road and Cepi Drive.

The public may speak on this item tonight.

- B. P.Z. 15-2007 Cambridge Engineering (Kramer Commerce Center):** A request for an amendment to City of Chesterfield Ordinance 1717 to amend the parking setbacks and development requirements for a 19.8 acre tract of land zoned "PI" Planned Industrial District located north of the intersection of Spirit Drive North and Chesterfield Airport Road, south of I-64/ U.S. 40-61, and west of Trade Center Boulevard. (17V610040, 17V620083, 17V620094, and 17V629402)

The public may speak on this item tonight.

- C. P.Z. 18-2007 Stoneridge Office Building (Ordinance Amendment):** A request for an amendment to City of Chesterfield Ordinance 2145 to amend the parking requirements for a 9.3 acre "PC" Planned Commercial District located east of Highway 40 and Timberlake Manor at 14532 South Outer Forty Drive (19R610026 and 19R610312)

The public may speak on this item tonight. There will be no vote taken tonight.

V. APPROVAL OF MEETING MINUTES

- A. Approval of April 23, 2007 Planning Commission Minutes

VI. PUBLIC COMMENT

VII. SITE PLANS, BUILDING ELEVATIONS AND SIGNS

- A. **Bacana Brasil**: Amended Architectural Elevations for a 3.7 acre parcel zoned "C-8" Planned Commercial located on the north side of Chesterfield Parkway, east of Justus Post Road.

The Planning Commission may vote on this item tonight.

- B. **Larry Enterprises/Lynch Hummer (MPD Investments, LLC)**: Amended Site Development Concept Plan and Conceptual Landscape Plan for a 14.11 acre parcel zoned "PI" Planned Industrial District located west of Boone's Crossing on the north side of North Outer Forty Road.

The Planning Commission may vote on this item tonight.

- C. **MPD Investments, LLC (Larry Enterprises/Lynch Hummer Lot A1)**: Site Development Section Plan, Landscape Plan, Lighting Plan, and Architectural Elevations for a 7.02 acre parcel zoned "PI" Planned Industrial District located west of Boone's Crossing on the north side of North Outer Forty Road.

The Planning Commission may vote on this item tonight.

- D. **Mobil Mart (Clayton and Baxter)**: Amended Site Development Plan, Landscape Plan, Lighting Plan and Architectural Elevations for a gas station/convenience store in a "PC" Planned Commercial District located on the northwest corner of Baxter and Clayton Roads.

The Planning Commission may vote on this item tonight.

- E. **Pfizer PGM Biopharma Building Amended Site Development Concept Plan**: An Amended Site Development Concept Plan for a 200.51 acre lot of land located at the northeast corner of 700 Chesterfield Parkway West.

The Planning Commission may vote on this item tonight.

- F. **Pfizer PGM Biopharma Building Site Development Section Plan:** A Site Development Section Plan, Landscape Plan and Architectural Elevations for a 200.51 acre lot of land located at the northeast corner of 700 Chesterfield Parkway West.

The Planning Commission may vote on this item tonight.

- G. **The Village at Olde Baxter Square:** A Record Plat for a 2.0 acre lot of land zoned "R-4" Residence District, under a Planned Environment Unit Procedure, located at the southwest corner of Old Baxter Road and Century Lake Drive.

The Planning Commission may vote on this item tonight.

- H. **Tuscany Reserve:** A Site Development Plan, Tree Stand Delineation, Tree Preservation Plan, Landscape Plan, and Architectural Elevations for a 58.15 acre lot of land zoned "E-One Acre" Estate Residence District, and "E-Two Acre" Estate Residence District, located north of Strecker Road and east of Church Road.

The Planning Commission may vote on this item tonight.

- I. **Chesterfield Commons Six Lot 3 (Chick-Fil-A):** Amended Sign Package for a "C-8" Planned Commercial District located on the north side of Chesterfield Airport Road and 1,450 feet west of Boones Crossing.

The Planning Commission may vote on this item tonight.

VIII. OLD BUSINESS

- A. **P.Z. 9-2006 St. Luke's Episcopal Presbyterian Hospitals (Parcel D):** A request for a change of zoning from "NU" Non-Urban District, "FPNU" Flood-Plain Non-Urban District and "R1" Residence District/FPR1" Flood-Plain Residence District to "MU" Medical Use District for four (4) parcels of land located ¼ mile north of the Woods Mill and Conway Roads intersection. The total area to be rezoned 14.0 acres. (Locator Numbers: 18Q420023,18Q510014,18Q510025, 18Q510036)

The Planning Commission may vote on this item tonight.

- B. P.Z. 02-2007 The Estates at Upper Kehrs Mill (Miceli Construction):** A request for a change of zoning from “NU” Non-Urban to “E” One Acre District for a 10.2 acre tract of land located on the eastern side of Kehrs Mill Road, 4,100 feet south of its intersection with Wild Horse Creek Road. (19U530062, 19U530392)

The Planning Commission may vote on this item tonight.

- C. P.Z. 07-2007 Spirit Valley Business Park (18652, 18630, 18650, and 18660 Olive Road):** A request for a change of zoning from “NU” Non-Urban to “PI” Planned Industrial for 52.89 acres of land located south of Olive Street Road, east of Wardenburg. (17W420057, 17W420035, 17W230010, 17W230021)

The Planning Commission may vote on this item tonight.

- D. P.Z. 10-2007 Surrey Place (St. Luke’s Episcopal Presbyterian Hospital):** A request to amend Conditional Use Permit Number 561 to add hospital as a permitted use for an approximately 10.751 acre tract of land located at 14701 Olive Boulevard, east of the intersection of Ladue Road and Olive Boulevard. (17R530197)

The Planning Commission may vote on this item tonight.

- E. P.Z. 11-2007 J&T Holdings (612 Cepi):** A request for a change of zoning from an “M3” Planned Industrial District to a “PC” Planned Commercial District for an approximately 0.75 acre tract of land located at 612 Cepi Drive south of the intersection of Chesterfield Airport Road and Cepi Drive.

The Planning Commission may vote on this item tonight.

- F. P.Z. 14-2007a Wilson Bluffs (SMS Group, L.L.C.):** A request for a change of zoning from “NU” Non-Urban District to “E-One Acre” Estate District for a 3.28 acre tract of land located on the west side of Wilson road, directly north of Wilson Manors II Subdivision.

The Planning Commission may vote on this item tonight.

- G. P.Z. 15-2007 Cambridge Engineering (Kramer Commerce Center):** A request for an amendment to City of Chesterfield Ordinance 1717 to amend the parking setbacks and development requirements for a 19.8 acre tract of land zoned "PI" Planned Industrial District located north of the intersection of Spirit Drive North and Chesterfield Airport Road, south of I-64/U.S. 40-61, and west of Trade Center Boulevard. (17V610040, 17V620083, 17V620094, and 17V629402)

The Planning Commission may vote on this item tonight.

- H. P.Z. 16-2007 Valley Gates Subdivision (16845 N. Outer 40 Road):** A request for an amendment to City of Chesterfield Ordinance 2154 to allow for a change to the parking and building setbacks, building height and number of permitted buildings for a 7.698 acre tract of land zoned "PC" Planned Commercial located north of North Outer Forty and east of Boone's Crossing at 16845 North Outer Forty Road. (17T520073)

The Planning Commission may vote on this item tonight.

IX. NEW BUSINESS

A. Appointment of Nominating Committee

X. COMMITTEE REPORTS

XI. ADJOURNMENT

Note: The Planning Commission will consider and act upon the matters listed above, and such other matters as may be presented at the meeting and determined to be appropriate for discussion at that time.

NOTICE OF PUBLIC HEARING CITY OF CHESTERFIELD PLANNING COMMISSION

NOTICE IS HEREBY GIVEN that the Planning Commission of the City of Chesterfield will hold Public Hearings on May 14, 2007, at 7:00 p.m. in the Council Chambers at the City Hall, 690 Chesterfield Parkway West, Chesterfield, Missouri 63017.

Said hearings will be as follows:

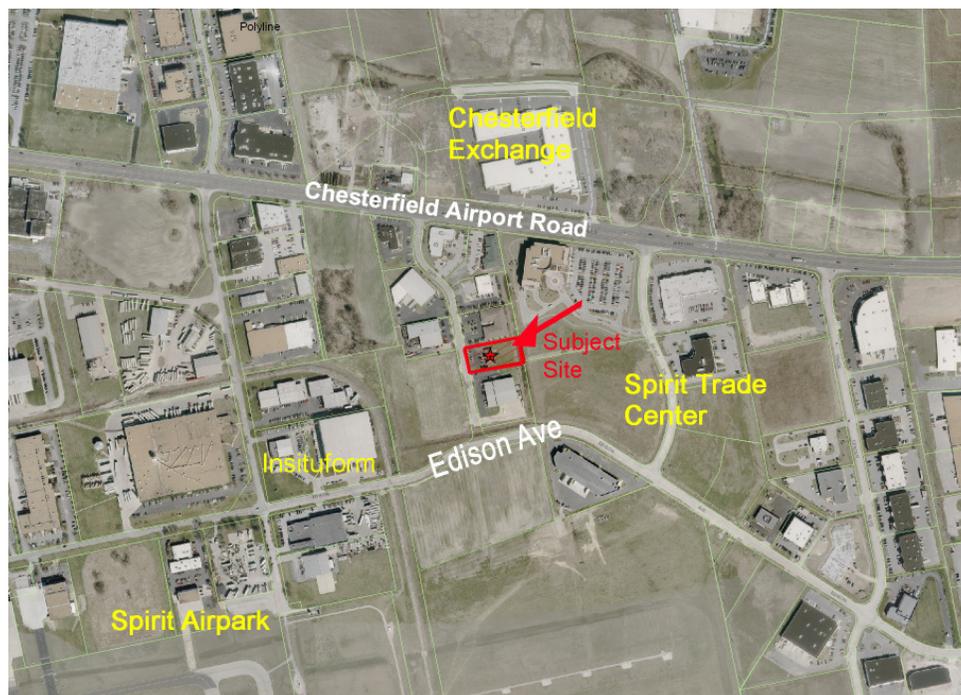
P.Z. 11-2007 J&T Holdings (612 & 614 Cepi): A request for a change of zoning from an “M3” Planned Industrial District to a “PC” Planned Commercial District for an approximately 0.75 acre tract of land located at 612 & 614 Cepi Drive south of the intersection of Chesterfield Airport Road and Cepi Drive. (17V330122)

Permitted Uses:

- (e) Associate work and storage areas required by a business, firm or service to carry on business operations.
- (x) Medical and dental offices.
- (z) Offices or office buildings.

Anyone interested in the proceedings will be given an opportunity to be heard. Copies of the request are available for review in the Department of Planning at the Chesterfield City Hall, 690 Chesterfield Parkway West during weekdays between the hours of 8:30 a.m. and 4:30 p.m. If you should need additional information about this project, please contact Ms. Jennifer Yackley, Project Planner, by telephone at 636-537-4743 or by email at jyackley@chesterfield.mo.us.

CITY OF CHESTERFIELD
Maurice L. Hirsch, Jr., Chair
Chesterfield Planning Commission



Description of Property

All of Lot 5 of the Amended Plat of Chesterfield Executive Park as recorded in Plat Book 231, Page 59 of the St. Louis County Records.

NOTICE OF PUBLIC HEARING CITY OF CHESTERFIELD PLANNING COMMISSION

NOTICE IS HEREBY GIVEN that the Planning Commission of the City of Chesterfield will hold Public Hearings on Monday, May 14, 2007, in the Council Chambers at the City Hall, 690 Chesterfield Parkway West, Chesterfield, Missouri 63017.

Said hearings will be as follows:

P.Z. 15-2007 Cambridge Engineering (Kramer Commerce Center): a request for an amendment to City of Chesterfield Ordinance 1717 to amend the parking setbacks and development requirements.

Anyone interested in the proceedings will be given an opportunity to be heard. Copies of the request are available for review in the Department of Planning at the Chesterfield City Hall, 690 Chesterfield Parkway West during weekdays between the hours of 8:30 a.m. and 4:30 p.m. If you should need additional information about this project, please contact Mrs. Mara Perry, Senior Planner, by telephone at 636-537-4744 or by email at mperry@chesterfield.mo.us

CITY OF CHESTERFIELD
Maurice L. Hirsch, Jr., Chair
Chesterfield Planning Commission



Description of Property

A tract of land being part of U.S. Survey 1010 and part of Lot 1 of the Subdivision of Lands of Spencer Tyler, according to the plat thereof recorded in Deed Book 7 Page 25 of the St. Louis County Recorder's Office located in U.S. Survey 419, all in Township 45 North, Range 3 East of the 5th Principal Meridian, St. Louis County, Missouri, and being more particularly described as follows:

Beginning at the intersection of the Southerly line of Missouri Interstate Highway 64 and the Northeasterly line of above said U.S. Survey 419, said point also being the Northwesterly corner of a tract of land as conveyed to Kramer Assets Group, LLC, as recorded in Book 11776 Page 2307 of above said records; thence along the Southerly line of said Interstate 64 and the Northerly line of said Kramer Assets Group, LLC and the Northerly line of a tract of land as conveyed to Kramer Assets Group, LLC, as recorded in Book 12523 Page 1389 of above said records the following courses and distances; thence South 83 degrees 04 minutes 20 seconds East 611.18 feet to a point of curvature to the left for which the radius point bears North 06 degrees 55 minutes 40 seconds East 34517.47; thence along said curve to the left with a chord which bears South 83 degrees 05 minutes 43 seconds East 27.51 feet an arc length of 27.51 feet to the Northwesterly corner of a tract of land as conveyed to Terra Investments, L.L.C. by instrument recorded in Book 11999 Page 1013 of said Recorder's Office; thence along the Westerly and Northerly lines of said Terra Investments, L.L.C. tract the following courses and distances; thence South 06 degrees 55 minutes 24 seconds West 1095.67 feet; thence North 83 degrees 06 minutes 20 seconds West 289.72 feet to the Northeasterly line of above said U.S. Survey 419, said point also being the Southwesterly line of said U.S. Survey 1010; thence along said Northeasterly line of U.S. Survey 419 North 10 degrees 51 minutes 03 seconds West 688.01 feet; thence departing said Northeasterly line North 83 degrees 04 minutes 20 seconds West 494.08 feet to the Southwesterly line of above said Lot 1 of the Subdivision of Lands of Spencer Tyler, said point also being on the Southeasterly line of a tract of land as conveyed to Danna Development, L.L.C. by instrument recorded in Book 12836 Page 324 of above said records; thence along said Southwesterly line of Lot 1 of the Subdivision of the Lands of Spencer Tyler, North 10 degrees 53 minutes 25 seconds West 462.87 feet to the Southerly line of above said Missouri Interstate Highway 64; thence along said Southerly line South 83 degrees 04 minutes 20 seconds East 494.41 feet to the Point of Beginning and containing 727,738 square feet or 16.707 acres more or less according to calculations performed by Stock and Associates Consulting Engineers, Inc. on June 28, 2006.

KRAMER COMMERCE CENTER PRELIMINARY PLAN

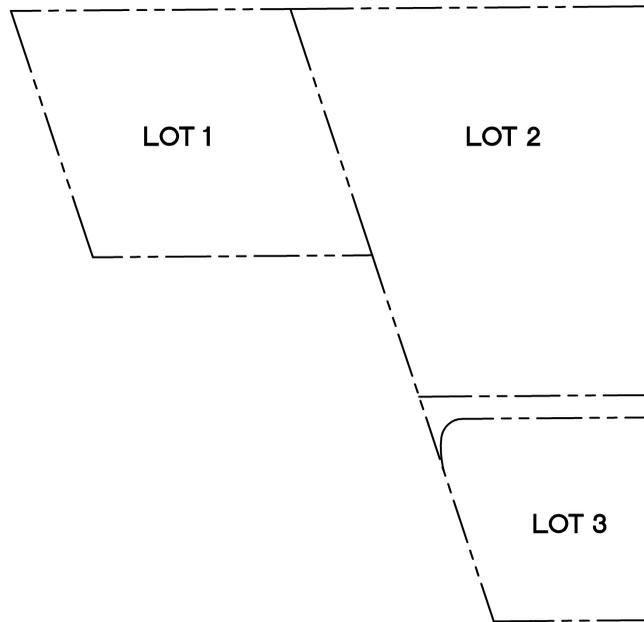
A TRACT OF LAND BEING PART OF U.S. SURVEY 1010 AND PART OF LOT 1
OF THE SUBDIVISION OF SPENCER TYLER ESTATE IN U.S. SURVEY 419 TOWNSHIP 45
NORTH, RANGE 3 EAST OF THE 5TH PRINCIPAL MERIDIAN ST. LOUIS COUNTY, MISSOURI

GENERAL NOTES

- BOUNDARY AND TOPOGRAPHIC SURVEY BY STOCK AND ASSOCIATES CONSULTING ENGINEERS, INC.
- GRADING & STORM WATER PER THE CITY OF CHESTERFIELD AND THE ST. LOUIS COUNTY DEPARTMENT OF HIGHWAYS AND TRAFFIC.
- NO SLOPE SHALL EXCEED A 3:1 SLOPE, EXCLUDING LANDSCAPING BERMS.
- ALL UTILITIES SHOWN HAVE BEEN LOCATED BY THE ENGINEER FROM AVAILABLE RECORDS. THEIR LOCATION SHOULD BE CONSIDERED APPROXIMATE. THE CONTRACTOR HAS THE RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES, PRIOR TO CONSTRUCTION, TO HAVE EXISTING UTILITIES FIELD LOCATED. SHOULD ANY CONFLICTS BE EVIDENT, THE CONTRACTOR SHALL NOTIFY THE OFFICE OF THE ENGINEER IMMEDIATELY.
- ARCHITECTURAL ELEVATIONS, SITE LANDSCAPING PLANS, SITE LIGHTING PLANS AND SITE SIGNAGE PLANS SHALL BE SUBMITTED TO THE CITY OF CHESTERFIELD AS INDIVIDUAL LOTS ARE DEVELOPED ON THEIR SITE DEVELOPMENT SECTION PLAN.
- FOR CLARITY, STREET LIGHTS WERE NOT SHOWN ON THIS PLAN. STREET LIGHTS SHALL BE PROVIDED ALONG LONG ROAD CROSSING DRIVE AS REQUIRED PER THE CITY OF CHESTERFIELD SUBDIVISION ORDINANCE SECTION 1003.320.
- ON-SITE STORM WATER DRAINAGE REQUIREMENTS SHALL BE IN ACCORDANCE WITH THE CHESTERFIELD VALLEY MASTER STORM WATER DRAINAGE PLAN.
- OFF-SITE GRADING EASEMENTS, IF REQUIRED, SHALL BE EXECUTED AND RECORDED PRIOR TO THE COMMENCEMENT OF ANY OFF-SITE GRADING.
- CROSS-ACCESS EASEMENTS WHERE REQUIRED, SHALL BE EXECUTED AND RECORDED AS INDIVIDUAL LOTS ARE DEVELOPED.
- ALL BUILDINGS SHALL BE CONSTRUCTED OF DECORATIVE MASONRY, GLASS TILT-UP CONCRETE AND/OR PRE CAST CONCRETE PANELS, DESIGN TO SIMILAR TO BUILDINGS CONSTRUCTED WITHIN THE SPIRIT TRADE CENTER DEVELOPMENT.
- REQUIRED SITE SETBACKS:
BUILDING AND STRUCTURE SETBACKS:
- 90' FROM NEW ROW OF I-64/US 40-61
- 55' FROM SOUTHERN DISTRICT BOUNDARY AT BUILDING "B"
- 165' FROM SOUTHERN DISTRICT BOUNDARY AT BUILDING "C"
- 30' FROM WESTERN DISTRICT BOUNDARY
- 90' FROM EASTERN DISTRICT BOUNDARY
- 90' FROM THE PROPOSED LONG ROAD CROSSING DRIVE
- 10' FROM THE PROPOSED ROW BETWEEN BUILDING "A" AND BUILDING "C"
PARKING AND LOADING SETBACKS:
- 30' FROM NEW ROW OF I-64/US 40-61
- 30' FROM SOUTHERN DISTRICT BOUNDARIES
- 25' FROM THE EAST AND WEST DISTRICT BOUNDARIES
- 15' FROM ROW FROM PRINCIPAL INTERNAL STREETS
- 10' FROM INTERNAL LOT LINES - WITH THE EXCEPTION OF SHARED DRIVEWAYS
- NO TREES AND LANDSCAPING MAY BE LOCATED WITHIN THE HIGH-WATER LIMITS OF THE CHESTERFIELD VALLEY MASTER STORM WATER DRAINAGE DITCHES.
- ALL SIDEWALKS WITHIN THE DEVELOPMENT WILL NOT BE MAINTAINED BY THE CITY OF CHESTERFIELD.
- PRIOR TO SPECIAL USE PERMIT ISSUANCE BY THE SAINT LOUIS COUNTY DEPARTMENT OF HIGHWAYS AND TRAFFIC, A SPECIAL CASH ESCROW OR A SPECIAL ESCROW SUPPORTED BY AN IRREVOCABLE LINE OF CREDIT, MUST BE ESTABLISHED WITH THE SAINT LOUIS COUNTY.
- ALL BUILDINGS AND ROADWAYS SHALL BE ELEVATED A MINIMUM 1 FOOT ABOVE THE MAX. HIGH-WATER ELEVATION IN THE CHESTERFIELD MASTER MODEL.
- STREET TREES SHALL BE PROVIDED ALONG BOTH SIDES OF THE PUBLIC ROAD IN ACCORDANCE WITH SECTION 1003.340 OF THE CITY OF CHESTERFIELD SUBDIVISION ORDINANCE.
- STREET LIGHTING ALONG LONG ROAD CROSSING DRIVE TO BE REQUIRED AS PER THE CHESTERFIELD VALLEY MASTER STREET LIGHTING PLAN.
- SLEEVES FOR FUTURE TELECOMMUNICATIONS SERVICES SHALL BE INSTALLED ADJACENT AND/OR PARALLEL TO ANY PROPOSED ROADWAY, OR OTHER LOCATION AS DIRECTED BY THE CITY OF CHESTERFIELD IN ORDER TO FACILITATE THE INSTALLATION OF UTILITIES AND TELECOMMUNICATION INFRASTRUCTURE FOR CURRENT
- TEMPORARY OFF-SITE GRADING EASEMENTS SHALL BE EXECUTED AND RECORDED PRIOR TO APPROVAL OF ANY OFF-SITE GRADING
- ALL PROPOSED IMPROVEMENTS SHALL BE CONSTRUCTED TO ST. LOUIS COUNTY STANDARDS.
- ALL GRADING AND DRAINAGE TO BE IN CONFORMANCE WITH ST. LOUIS COUNTY AND MSD STANDARDS.
- NO SLOPES WITHIN SAINT LOUIS COUNTY RIGHT-OF-WAY SHALL EXCEED 3 (HORIZONTAL) TO 1 (VERTICAL).
- STORM WATER SHALL BE DISCHARGED AT AN ADEQUATE NATURAL DISCHARGE POINT. SINKHOLES ARE NOT ADEQUATE DISCHARGE POINTS.
SUBJECT PROPERTY LIES WITHIN FLOOD ZONE "X" (AREA OF 500-YEAR FLOOD, AREA OF 100-YEAR FLOOD WITH AVERAGE DEPTHS OF LESS ONE (1) FOOT OR WITH DRAINAGE AREA LESS THAN ONE (1) SQUARE MILE, AND AREAS PROTECTED BY LEVEE FROM 100-YEAR FLOOD) AND FLOOD ZONE "AH" (FLOOD DEPTHS OF 1-3 FEET, USUALLY AREAS OF PONDING, BASE FLOOD ELEVATIONS = 454 AND 455 PER THE NATIONAL FLOOD INSURANCE PROGRAM FLOOD INSURANCE RATE MAP FOR ST. LOUIS COUNTY, MISSOURI AND INCORPORATED AREAS. THE MAP IS IDENTIFIED AS MAP NO. 22089C0120 W WITH AN EFFECTIVE DATE OF AUGUST 2, 1995 AND REVISED TO REFLECT LOMR DATED APRIL 17, 2000.
- A BOUNDARY ADJUSTMENT PLAT WILL BE PREPARED AND SUBMITTED PRIOR TO BUILDING PERMIT APPLICATION.
- THIS SITE WILL BE REQUIRED TO PARTICIPATE IN THE CITY OF CHESTERFIELD'S VALLEY WIDE MITIGATION BANK, THERE IS 1.36 ACRES OF WETLANDS DEDICATED ON THIS SITE WHICH WILL REQUIRE A TOTAL OF 1.40 ACRES OF MITIGATION CREDIT TOTALING \$35,063.00.
- ALL UTILITIES SHALL BE INSTALLED UNDERGROUND.

LEGEND

EXISTING SANITARY SEWER	
PROPOSED SANITARY SEWER	
EXISTING STORM SEWER	
PROPOSED STORM SEWER	
EXISTING TREE	
EXISTING BUILDING	
EXISTING CONTOUR	
SPOT ELEVATION	
EXISTING UTILITIES	
FOUND 1/2" IRON PIPE	
SET IRON PIPE	
FOUND CROSS	
FOUND STONE	
FIRE HYDRANT	
LIGHT STANDARD	
BUSH	
SIGN	
NOTES PARKING SPACES	
GUY WIRE	
POWER POLE	
WATER VALVE	
DENOTES RECORD INFORMATION	
HANDICAPPED PARKING	

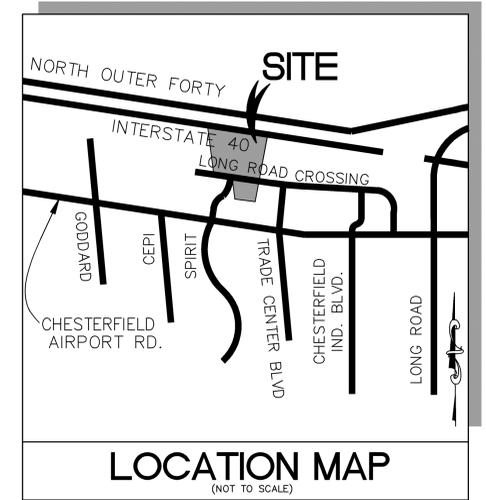


ABBREVIATIONS

C.O.	-	CLEANOUT
DB.	-	DEED BOOK
E	-	ELECTRIC
FL	-	FLOWLINE
FT	-	FEET
FND.	-	FOUND
G	-	GAS
M.H.	-	MANHOLE
N/F	-	NOW OR FORMERLY
PB.	-	PLAT BOOK
PG.	-	PAGE
P.V.C.	-	POLYVINYL CHLORIDE PIPE
R.C.P.	-	REINFORCED CONCRETE PIPE
SQ.	-	SQUARE
T	-	TELEPHONE CABLE
TYP.	-	TYPICAL
V.C.P.	-	VETRIFIED CLAY PIPE
W	-	WATER
(86'W)	-	RIGHT-OF-WAY WIDTH

PERTINENT DATA

OWNER	=	KRAMER ASSET GROUP L.L.C.
ZONING	=	M3 & P.C. ORDINANCE No. 129-78
LOCATOR NUMBER	=	17V62-0083, 17V62-0094 & 17V61-0040
SITE ACREAGE	=	16.707 Acres ±
FIRE DISTRICT	=	CHESTERFIELD FIRE PROTECTION DISTRICT
SCHOOL DISTRICT	=	ROCKWOOD R6
SEWER DISTRICT	=	M.S.D.
WATER SHED	=	BONHOMME CREEK
WATER SERVICE	=	ST. LOUIS CO. WATER COMPANY
GAS SERVICE	=	LACLEDE GAS COMPANY
ELECTRIC SERVICE	=	AMERENUE ELECTRIC COMPANY
PHONE SERVICE	=	S.W.B.T.



PROPERTY DESCRIPTION

Total Tract
A tract of land being part of U.S. Survey 1010 and part of Lot 1 of the Subdivision of Lands of Spencer Tyler, according to the plat thereof recorded in Deed Book 7 Page 25 of the St. Louis County Recorder's Office located in U.S. Survey 419, all in Township 45 North, Range 3 East of the 5th Principal Meridian, St. Louis County, Missouri, and being more particularly described as follows:

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SURVEYOR'S CERTIFICATION

This is to certify that Stock and Associates Consulting Engineers, Inc. has prepared this Preliminary Plan from record survey information only and does not represent a property boundary survey.
STOCK AND ASSOCIATES CONSULTING ENGINEERS INC.
L.S. No. 222-D

Daniel Ehlmann, Missouri L.S. No. 2215

This Preliminary Plan was approved by the City of Chesterfield Planning Commission and duly verified on the ___ day of ___, of 2007, by the Chairperson of said Commission, authorizing the recording of this Amended Site Development Concept Plan pursuant to Chesterfield Ordinance Number 200, as attested to by the Director of Planning and the City Clerk.

PREPARED FOR:
KRAMER ASSET GROUP L.L.C.
#17825 CHESTERFIELD AIRPORT RD
CHESTERFIELD, MO 63005
(636)532-2233

SITE BENCHMARK

ELEV.=458.00
RAILROAD SPIKE IN POWER POLE IS APPROXIMATELY
150' SOUTH OF SOUTHWEST CORNER OF CAMBRIDGE
ENGINEERING WAREHOUSE BUILDING.

M.S.D. BENCHMARK

M.S.D. BENCHMARK #11-44 ELEV.=461.85
750' ON SOUTHWEST CORNER OF CONCRETE STEP
TO HOUSE #17839 CHESTERFIELD AIRPORT ROAD.

M.S.D. P# XXXXX
BASE MAP # 17-V

04/27/07
GEORGE M. STOCK E-25116

04/27/07 - REVISED PER CITY COMMENTS

KRAMER COMMERCE CENTER
PRELIMINARY PLAN

STOCK & ASSOCIATES
Consulting Engineers, Inc.

257 Chesterfield Business Parkway
St. Louis, MO 63005
PH. (636) 530-9100
FAX (636) 530-9130
e-mail: general@stockassoc.com
Web: www.stockassoc.com

DRAWN BY: DATE CHECKED BY: DATE FOR NUMBER: SHEET:
A.C.D. 04/12/07 D.P.B. 04/12/07 99-190.04 1 of 3

CHESTERFIELD EXCHANGE
ORD. No. 1718
STOCK PROJECT No. 99-2117

WALLER-HOCH
ORD. No. 1718
STOCK PROJECT No. 200-2257

LOT 3
124,535 Sq. Ft.
2.858 Acres
PROPOSED BUILDING "B"
STOCK PROJECT
No. 207-4103

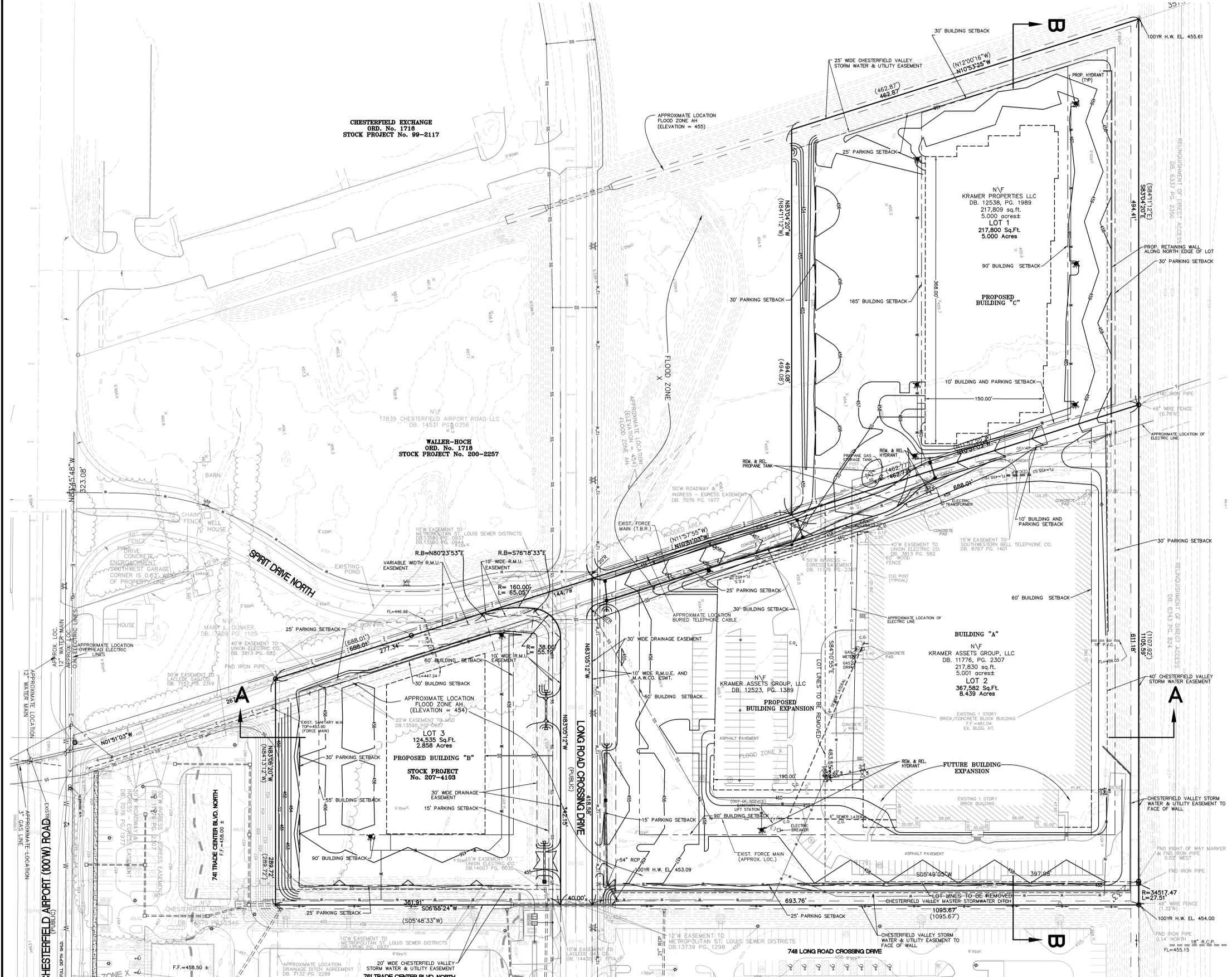
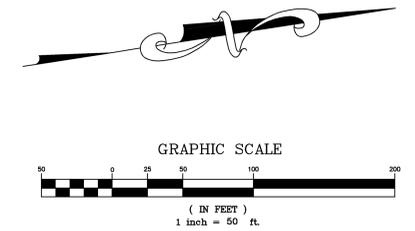
TERRA CORPORATE PARK
ORD. No. 2245
STOCK PROJECT No. 99-2098

N/F
KRAMER PROPERTIES LLC
DB. 12538, PG. 1989
217,809 sq.ft.
5.000 acre±
LOT 1
217,800 Sq.Ft.
5.000 Acres
PROPOSED
BUILDING "C"

N/F
KRAMER ASSETS GROUP, LLC
DB. 11776, PG. 2307
217,830 sq.ft.
5.001 acres±
LOT 2
367,582 Sq.Ft.
8.439 Acres
BUILDING "A"

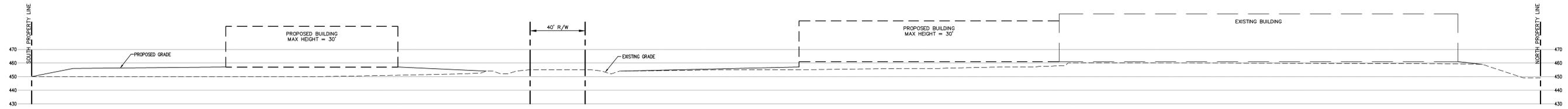
N/F
KRAMER ASSETS GROUP, LLC
DB. 12523, PG. 1389
PROPOSED
BUILDING EXPANSION

N/F
KRAMER ASSETS GROUP, LLC
DB. 11776, PG. 2307
217,830 sq.ft.
5.001 acres±
LOT 2
367,582 Sq.Ft.
8.439 Acres
FUTURE BUILDING
EXPANSION



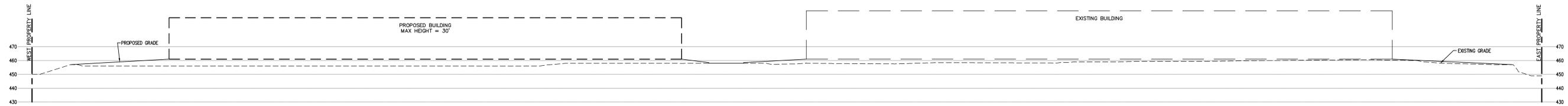
04/27/07 - REVISED PER CITY COMMENTS

KRAMER COMMERCE CENTER	
PRELIMINARY PLAN	
	257 Chesterfield Business Parkway St. Louis, MO 63005 PH. (636) 530-9100 FAX (636) 530-9130 e-mail: general@stockassoc.com Web: www.stockassoc.com
M.S.D. P# XXXXX BASE MAP # 17-W	DRAWN BY: A.C.D. DATE CHECKED BY: DATE FOR NUMBER: 99-190.04 04/27/07 GEORGE M. STOCK E-25116 04/12/07 D.P.B. 04/12/07 2 of 3



SECTION A-A

SCALE: HORIZONTAL 1"=30'
VERTICAL 1"=30'



SECTION B-B

SCALE: HORIZONTAL 1"=30'
VERTICAL 1"=30'

04/27/07 - REVISED PER CITY COMMENTS

M.S.D. P# XXXXX
BASE MAP # 17-V

KRAMER COMMERCE CENTER

PRELIMINARY PLAN

STOCK & ASSOCIATES
Consulting Engineers, Inc.

257 Chesterfield Business Parkway
St. Louis, MO 63005
PH. (636) 530-9100
FAX (636) 530-9130
e-mail: general@stockassoc.com
Web: www.stockassoc.com

04/27/07
GEORGE M. STOCK E-25116

DATE: 04/12/07	CHECKED BY: A.C.D.	DATE: 04/12/07	DESIGNED BY: D.P.B.	DATE: 04/12/07	JOB NUMBER: 99-1990.04	SHEET: 3 of 3
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NOTICE OF PUBLIC HEARING CITY OF CHESTERFIELD PLANNING COMMISSION

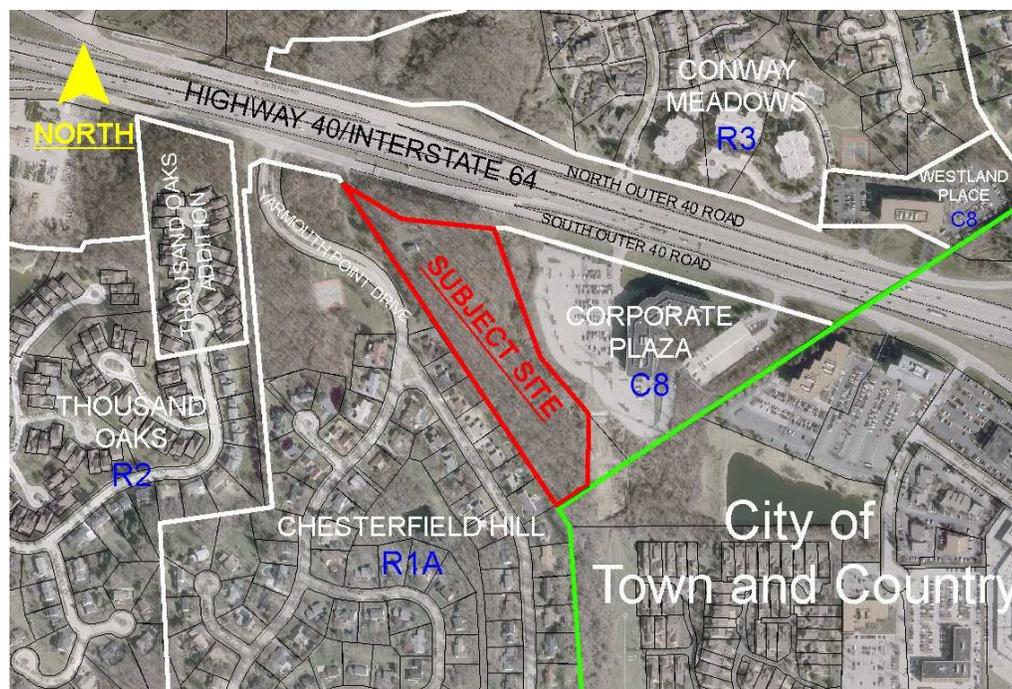
NOTICE IS HEREBY GIVEN that the Planning Commission of the City of Chesterfield will hold Public Hearings on Monday, May 14, 2007, in the Council Chambers at the City Hall, 690 Chesterfield Parkway West, Chesterfield, Missouri 63017.

Said hearings will be as follows:

P.Z. 18-2007 Stoneridge Office Building (Ordinance Amendment): a request for an amendment to City of Chesterfield Ordinance 2145 to amend the parking requirements for a 9.3 acre "PC" Planned Commercial District located East of Highway 40 and Timberlake Manor at 14532 South Outer Forty Drive (19R610026 and 19R610312)

Anyone interested in the proceedings will be given an opportunity to be heard. Copies of the request are available for review in the Department of Planning at the Chesterfield City Hall, 690 Chesterfield Parkway West during weekdays between the hours of 8:30 a.m. and 4:30 p.m. If you should need additional information about this project, please contact Mr. Jarvis Myers, Project Planner, by telephone at 636-537-4741 or by email at jmyers@chesterfield.mo.us

CITY OF CHESTERFIELD
Maurice L. Hirsch, Jr., Chair
Chesterfield Planning Commission



Description of Property

A Tract of land being part of U.S. Survey 369, Township 45 North, Range 4 East of the 5th Principal Meridian, St. Louis County, Missouri, and being part of the property as acquired by the Missouri Department of Transportation by instrument recorded in Book 5054 Page 349 of the Recorder of Deeds Office in St. Louis County, Missouri.

V.A.

**PLANNING COMMISSION
OF THE CITY OF CHESTERFIELD
AT CHESTERFIELD CITY HALL
APRIL 23, 2007**

The meeting was called to order at 7:03 p.m.

I. PRESENT

ABSENT

Mr. David Asmus
Mr. David Banks
Mr. Fred Broemmer
Ms. Wendy Geckeler
Dr. Lynn O'Connor
Ms. Lu Perantoni
Mr. Gene Schenberg
Ms. Victoria Sherman
Chairman Maurice L. Hirsch, Jr.

Councilmember Connie Fults, Council Liaison
City Attorney Rob Heggie
Mr. Michael Herring, City Administrator
Mr. Mike Geisel, Acting Director of Planning
Ms. Annissa McCaskill-Clay, Assistant Director of Planning
Ms. Aimee Nassif, Senior Planner
Ms. Mara Perry, Senior Planner
Mr. Jarvis Myers, Project Planner
Ms. Mary Ann Madden, Planning Assistant

II. INVOCATION: Commissioner Asmus

III. PLEDGE OF ALLEGIANCE – All

Chair Hirsch welcomed Councilmember Connie Fults to the dais, who is now serving as Council Liaison to the Planning Commission. He then acknowledged the attendance of Councilmember Jane Durrell, Ward I; Councilmember Bruce Geiger, Ward II; Councilmember Lee Erickson, Ward II; Councilmember Bob Nation, Ward IV; and City Administrator Mike Herring.

PUBLIC HEARINGS – Commissioner Geckeler read the “Opening Comments” for the Public Hearings.

- A. P.Z. 10-2007 Surrey Place (St. Luke’s Episcopal Presbyterian Hospital):** A request to amend Conditional Use Permit Number 561 to add hospital as a permitted use for an approximately 10.751 acre tract of land located at 14701 Olive Boulevard, east of the intersection of Ladue Road and Olive Boulevard. (17R530197)

Ms. Annissa McCaskill-Clay, Assistant Director of Planning, explained the Public Hearing posting requirements and then gave a PowerPoint presentation showing photographs of the site and surrounding area. She noted that the Comprehensive Land Use designation for the subject site is “Residential”.

PETITIONER’S PRESENTATION:

1. Mr. Mike Doster, Doster, Mickes, Attorney representing the Petitioner, 17107 Chesterfield Airport Road, Chesterfield, MO stated the following:
 - The proposal is the result of a Joint Venture between St. Luke’s Episcopal Presbyterian Hospital and Rehab Care, Inc. St. Luke’s will own the land upon which the proposed building will be located; the Joint Venture will own the building; and Rehab Care will manage the operations that will be contained within the proposed building.
 - They are requesting an amendment to the Conditional Use Permit to provide for the use of “hospital”. The word “hospital” has to be used because it is the word used in the list of conditional uses in the R-1A District.
 - The current uses on the site are “nursing home” and “associated self-care units and recreation facilities for ages 55 and up”. The Petitioner is proposing to add a specialized, limited use facility providing general physical rehabilitative and medical services, including nursing, clinical and therapeutic services.
 - The building will be limited to 35 beds for the Rehab Facility in addition to the 140 beds permitted under the existing Conditional Use Permit. The proposed building will be a single-story structure. There is an exposed lower level on the back side, which will be used for offices, storage, and deliveries. The existing building, known as Surrey Place, is a single-story building.
 - There will be no emergency room, no operating room, no surgical services, or no helipad on the subject site. The Petitioner is willing to restrict the use of “hospital” in the same fashion as language used in the Ordinance for St. John’s Rehabilitation facility.
 - The Certificate of Need has already been issued with respect to this request.
 - The Monarch Fire Protection District has approved the Preliminary Development Plan.

Responding to questions from the Commission, Mr. Doster stated the following:

- **Regarding whether outpatient services will be offered at the proposed facility:** There is the possibility of outpatient services. There will be facilities within the building that will provide rehabilitation services. Residents in the existing nursing home facility will be able to use the services of the proposed rehab facility. The facility is primarily designed to accommodate St. Luke's Hospital's needs. The Hospital wants to be able to move patients to a rehabilitation facility, as opposed to keeping them in the hospital. At this time, St. Luke's has identified a need to place 28 beds – the proposal provides for 35 beds, which allows for a little expansion. Primarily, the patients entering the facility will be coming from St. Luke's Hospital, which is approximately two miles away. Rehab patients will stay at the facility until they are able to return home or the nursing home facility.
- **Regarding whether the facility would be used for outpatient therapy for patients who have returned to their homes after being treated at the rehab facility.** This is a possibility, but it is not likely. The primary addition is the 35 beds for patients who need acute rehabilitation services. It is not likely they will be returning very often once they are released.
- **Regarding whether the Certificate of Need is site-specific in its language:** The Certificate of Need is confined to the proposed use at the subject location. It specifically refers to 35 beds and the use of the facility by St. Luke's Hospital. Commissioner O'Connor requested that the Certificate of Need be made a part of the next meeting packet on this petition.
- **Regarding cross access to the west:** Cross access to the west already exists. Kendall Bluffs subdivision will also be using this access. Mr. George Stock of Stock & Associates stated that there are two points of access for Kendall Bluffs – the main entrance is on Olive Street Road, which serves both Surrey Place and Kendall Bluffs. There is also an emergency access point - a connection point from Kendall Bluffs that ties into the parking lot, which was a condition of the Monarch Fire District.
- **Regarding the sidewalk along Olive:** The sidewalk will be five-feet wide along the full length of the property along Olive.

2. Mr. George Stock, Stock & Associates, 257 Chesterfield Business Parkway, Chesterfield, MO stated the following:

- The boundaries of the proposed site include Olive Street Road to the south; The Villages of Kendall Bluffs to the west and north; and the old Riverwoods Subdivision to the east.
- The eastern half of the site basically remains untouched with the exception of some parking spaces.
- The addition is 23,400 square feet. It is attached to the north leg of the existing Surrey Place and goes west.

- New parking is being proposed in the southwest portion of the site. The main entrance is in the south leg of the rehab hospital.
- Just to the west of the existing parking, which currently serves Surrey Place, is a detention basin. They intend to re-configure the parking lot to provide a total of 146 parking spaces, which would serve the 140 beds, the 35 requested beds, and the 80 employees. The new parking would be in the area of the existing parking lot and will be expanded to the west. The existing dry detention basin will also be moved further to the west and will be brought into compliance with the current MSD standards – resulting in a larger detention basin.
- The fire access road goes around the north side of the building, which also comes into a service area for the rehab hospital. Another fire access drive is provided and is the sole purpose of meeting the fire hose length, which is mandated by the Monarch Fire District.
- The improvements are limited to the western half of the site. They are trying to work with the topography as much as possible. They are utilizing the existing access, which is currently on Olive Boulevard. The site is serviced by utilities, which are of adequate capacity to serve the additional building.

For clarification, Chair Hirsch reminded the Commission that they would not be voting on the site this evening; there could be a vote on a change to the Conditional Use Permit.

Responding to questions from the Commission, Mr. Stock stated the following:

- **Regarding whether the existing berm will be removed on the southern portion of the site between Olive Road and the facility:** The berm will not be removed. They are not changing the embankment or the grading within the right-of-way.
- **Regarding the height of the building:** The proposed building is a single-story building. The north elevation has a lower level that is exposed to the service bay area.
- **Regarding whether the language in the Conditional Use Permit (C.U.P.) allows for a three-story building:** Mr. Doster replied that the existing C.U.P. refers to a height limitation of three building levels. The Petitioner is not asking for a three-story building. Surrey Place is currently one-story. If the Commission wants to amend the language to state “one building level”, the Petitioner is agreeable to it. Ms. Mara Perry, Senior Planner, stated that since the “three building levels” language is part of the existing C.U.P., it was carried forward. She noted that the language could be amended this evening.
- **Regarding whether the C.U.P. includes any language that deals with buffering the lower level from the neighboring residential area:** Ms. McCaskill-Clay stated that the C.U.P. requirements includes language which states “All provisions of the City of Chesterfield City Code shall apply to this development except as specifically modified below . . .” The

Petitioner would have to meet the requirements of the Tree Manual regarding buffering against “Residential”. The Petitioner will be required to submit a Landscape Plan for the Commission’s review.

- **Regarding deliveries to the site in relationship to the proximity of neighboring residential areas:** There are no lots immediately adjacent to the north property line, where the deliveries would be taking place. Residences are at least 200 feet to the north of the property line and 80 feet lower than the subject site.
 - **Regarding restriction of delivery hours:** Ms. Perry stated that there are no restrictions to the delivery hours specifically stated in the C.U.P. Commissioner Broemmer stated that since the site is zoned “Residential”, trash pick-up would be restricted to after 7:00 a.m.
3. Mr. Don Miller, representing St. Luke’s Hospital, 232 South Woods Mill Road, Chesterfield, MO indicated he was available for questions.
 4. Mr. Brian Samberg, representing Rehab Care, 7733 Forsyth Boulevard, St. Louis, MO indicated he was available for questions.

SPEAKERS IN FAVOR: None

SPEAKERS IN OPPOSITION: None

SPEAKERS – NEUTRAL:

1. Mr. Steve Lander, 679 Old Riverwoods Lane, Chesterfield, MO stated the following:
 - The Villages of Kendall Bluffs, adjacent to Surrey Place, consists of 115 attached units and shares a common entrance with Surrey Place.
 - A traffic study was done on the site at the time Kendall Bluffs was going through its zoning change. Speaker recalls that the Surrey Place entrance was established to be a “D-rated” intersection by the firm conducting the study. With the addition of the 115 units of Kendall Bluffs, the intersection was projected to become an “F”. There are no other entrances or exits serving the 140-bed, 80-employee nursing facility and the 115 units of Kendall Bluffs. This “F” rating, along with other concerns, resulted in a “no vote” by the Planning Commission of 1 to 8 on the Kendall Bluffs project.
 - He has concern regarding motor safety in connection with the proposed addition to Surrey Place. He feels that left-hand turns in or out of Surrey Place leaves motorists open for broad-side collisions. He feels that adding 60 employees, along with the additional use, would increase the volume of traffic and the danger to motorists in an already hazardous entrance.
 - He feels it is imperative that, as a condition of approval, a new traffic study be conducted with its results being a guide for the Commission’s decision.
 - He feels that a disclosure should be required for potential buyers whose property would back up to the new addition.

REBUTTAL:

Mr. Doster stated the following:

- He does not believe the size of the proposed addition justifies doing a traffic study.
- The parking spaces for this facility are counted differently than other types of commercial or residential activity. The calculation for 35 beds allows 18 parking spaces because the number of visits to patients in this type of facility is very minimal. They plan on having approximately 60 employees, but at any one time, it's not likely that they will have more than 40 employees on site.

Commissioner Sherman asked if traffic was discussed with the Staff. Ms. Perry stated that the issue of traffic was not discussed.

Commissioner Sherman asked for information on the Olive Road configuration in front of the subject site. Mr. Mike Geisel, Acting Director of Planning, stated that in conjunction with the construction of the Kendall Bluffs subdivision, the entrance was relocated. There was a median that had to be modified to allow for left-turns in. Considering the fact that a residential development has been approved behind Surrey Place that accesses through the same entrance, restricting left turns into the site would be very difficult at this point. There is a limited amount of queuing room for left turns.

ISSUES:

1. Will there be any type of outpatient care at the proposed facility?
2. What are the requirements of the Certificate of Need?
3. What type of buffering would be provided for the subject site?
4. Address motor safety – particularly whether or not the access point would create a hazard for residents living in the area, as well as for motorists traveling along Olive Boulevard.
5. Is a traffic study for the site needed?
6. Should potential property owners at Kendall Bluffs be provided with a disclosure of the proposed addition?
7. Amend the C.U.P. language stating that there would be no emergency room, no operating rooms, no surgical services, or no helipad.
8. Amend the C.U.P. language regarding the number of building levels to be no more than one level, excluding basement and cellar areas.
9. Provide the traffic study done for Kendall Bluffs.
10. Provide a traffic accident report for the subject location.
11. Can the speed on Olive Boulevard be reduced as opposed to installing stop signs and stop lights in the residential area? Ms. McCaskill-Clay stated that Olive Boulevard is owned by the Missouri Department of Transportation. Commissioner Broemmer pointed out that the speed has been reduced in other municipalities and felt it could be reduced here also. Mr. Geisel stated that the speed on a roadway is directly related to a motorist's perception.

Without the presence of a police officer or a radar trailer, motorists will not necessarily reduce their speed even if the speed limit is lowered.

12. Show the differential in elevations of the subject site in relationship to Kendall Bluffs.

- B. P.Z. 12-2007 City of Chesterfield (Tree Manual):** A request to repeal City of Chesterfield Ordinance 2335 and replace it with a new ordinance that revises the procedures and requirements for Tree Protection Sureties, Escrows and Landscape Installation Bonds.

Project Planner Jarvis Myers gave a PowerPoint presentation. Mr. Myers stated that three amendments are being proposed to the Tree Manual with respect to:

- When Landscape Bond are collected;
- When Tree Preservation Escrows/Sureties are collected; and
- The calculation method for Escrows/Surety.

Amendment 1 - Section XIII F. **LANDSCAPE PROPOSALS (page 20)**

Current language:

Prior to the signing of any mylar for recording at St. Louis County, a landscape bond is required. The requirements for landscape installation and landscape maintenance bonds in the City of Chesterfield are set forth in Table 5.

Proposed language: (changes in bold)

Prior to the signing of any mylar for **a record plat or approval of any Municipal Zoning Authorization other than for a display house**, a landscape bond shall be posted. The requirements for landscape installation and landscape maintenance bonds in the City of Chesterfield are set forth in Table 5. **Alternatively, landscape installation surety may be included in the subdivision escrow, as approved by the Director of Planning.**

The proposed change would allow developers to seek funds from financial institutions to gain the bond.

Amendment 2 - Section XIV **SURETY AND ESCROW PROCEDURES (page 20)**

Current language:

Prior to the signing of any mylar to be recorded at St. Louis County, a surety or cash escrow shall be posted to account for trees that die, or are damaged beyond repair, as a result of grading or construction damage.

Proposed language: (changes in bold)

Prior to the **issuance of any grading permit or improvement plan approval**, a surety or cash escrow shall be posted to account for trees that die, or are damaged beyond repair, as a result of grading or construction damage.

The proposed change would help developers obtain funds from financial institutions for escrow or surety.

**Amendment 3: Section XIV
SURETY AND ESCROW PROCEDURES (page 21)**

Current language:

The amount of the surety or cash escrow shall be in the amount of \$20,000 for each acre contained in the tree preservation area. The amount may be pro-rated for any tree preservation area less than one (1) acre.

Proposed language: (changes in bold)

The amount of the surety or cash escrow shall be in the amount of **\$10,000 per 100 lineal feet, or portion thereof, of wooded canopy perimeter to be preserved adjacent to any proposed clearing, grading, or other disturbance; or \$20,000 per acre to be preserved whichever is less.**

The proposed language includes an additional method of calculating the amount required for a Tree Protection Surety. The current method calculates the entire area of protected canopy. The alternative method eliminates the entire canopy requirement for the Tree Protection Surety and calculates only the perimeter of the tree protected area.

SPEAKERS IN FAVOR: None

SPEAKERS IN OPPOSITION: None

SPEAKERS – NEUTRAL: None

REBUTTAL: None

ISSUES: None

C. P.Z. 16-2007 Valley Gates Subdivision (16845 N. Outer 40 Road):

A request for an amendment to City of Chesterfield Ordinance 2154 to allow for a change to the parking and building setbacks, building height and number of permitted buildings for a 7.698 acre tract of land zoned "PC" Planned Commercial located north of North Outer Forty and east of Boone's Crossing at 16845 North Outer Forty Road. (17T520073)

Ms. Aimee Nassif, Senior Planner, gave a PowerPoint presentation showing photographs of the site and surrounding area. Ms. Nassif explained the Public Hearing posting requirements and stated the following:

- The subject site is located just north of North Outer Forty Road and next to the Summit Center, which is zoned "Planned Commercial". On the other side of the site is the Chesterfield Valley Nursery, zoned "Non-Urban".
- The Preliminary Plan for Valley Gates Subdivision went through the process in 2005. The original plan showed two buildings being 90,000 square feet in size with open space at 40% and was rezoned from "Non-Urban" to "Planned Commercial". At that time, the Attachment A was written very specifically to what the Preliminary Plan showed – requiring two buildings with a specific square footage.
- The Petitioner is now requesting the following amendments:
 - A setback change for the buildings and parking.
 - A height change from two-stories to three-stories. - Retail buildings would remain at two-stories, as noted in the Attachment A. All other buildings would have a maximum height of three-stories. The requested change does not violate anything in the Ordinance or the Comprehensive Plan.
 - An increase in the number of buildings allowed.
- Starting in 2007, Staff has changed how the Attachment A is written for a rezoning. The Attachment A is no longer being written to the Preliminary Plan with respect to a specific number of lots and specific square footage. If an issue is made at the Public Hearing about the number of lots and/or square footage, it will then be included in the Attachment A.
- At the present time, the Attachment A is written requiring that the City Codes be met, along with the requirements of the Comprehensive Plan and any other specifically-exclusive design requirements specific to the site.
- The proposed Attachment for the subject petition does not limit the number of square footage or the number of buildings. The build-able square footage amount is dependent upon the floor area ratio requirement, the open space requirement, and the required setbacks.
- The setbacks for the subject site are written to Code and the open space is remaining at 40%.

- The Department of Planning has no issues with the Ordinance Amendment request. Staff was awaiting MoDOT comments before finalizing the Attachment A.
- The Comprehensive Plan designates the site as “Mixed Commercial” use.

Commissioner Perantoni asked what the height limit is for buildings in the urban core of the City. Ms. Nassif replied that the height limit is 75 feet.

PETITIONER’S PRESENTATION:

1. Mr. George Stock, Stock & Associates, 257 Chesterfield Business Parkway, Chesterfield, MO stated the following:
 - He is representing Summit Outer Forty Investors.
 - Ordinance 2154 was approved in February 2005. That Ordinance was written around a Preliminary Plan showing two 45,000 square foot buildings. At that time, the owners believed that the market would be for a flex-type building of office and retail. For the past two years, there has not been any activity for this type of building.
 - The Petitioner is now requesting amendments to react to the current market. The request is for smaller buildings, possibly three-stories in height, on six lots. The lots would range in size from 1acre to 1.5 acres.
 - The request does not increase the allowable square footage of 90,000 sq. ft. The open space will remain at 40%.
 - Access to the development remains the same with an entrance to the Outer Road. Permit comments have been received from MoDOT but the final comments from MoDOT have not yet been received.
 - Cross access is still allowed to the west to the Summit Ice Rink.

Responding to questions from the Commission, Mr. Stock stated the following:

- **Regarding cross access to the east:** If it is not already in the Ordinance, they are willing to provide cross access to the east. City Attorney Heggie pointed out that the Section I.1 of the draft Attachment A regarding “Access/Access Management” requires access to adjacent properties, as stated below:

Provide cross access easements or other appropriate legal instruments guaranteeing permanent access to adjacent properties as directed by the City of Chesterfield.

SPEAKERS IN FAVOR: None

SPEAKERS IN OPPOSITION: None

SPEAKERS – NEUTRAL: None

REBUTTAL: None

ISSUES: None

Commissioner Geckeler read the Closing Comments for the Public Hearings.

V. APPROVAL OF MEETING MINUTES

Commissioner Broemmer made a motion to approve the minutes of the April 9, 2007 Planning Commission Meeting. The motion was seconded by Commissioner Schenberg and **passed by a voice vote of 9 to 0.**

VI. PUBLIC COMMENT

RE: P.Z. 28-2006 Chesterfield Neighborhood Office Park (17655 and 17659 Wild Horse Creek Road)

Petitioner:

1. Mr. Brandon A. Harp, Civil Engineering Design Consultants, representing Vision Ventures, 11402 Gravois Road, Ste. 100, St. Louis, MO stated the following:
 - They are requesting a rezoning from “Non-Urban” to “Planned Commercial” with the Wild Horse Creek Overlay District.
 - The Preliminary Development Plan, dated January 30, 2007, showed 48,000 square feet of total building floor area, which was comprised of four buildings at 12,000 square feet each. On the eight-acre site, this equated to 6,000 square feet per acre of building density.
 - Since that meeting, the Petitioner has worked to reduce the density of the overall floor area ratio on the site.
 - The Amended Preliminary Development Plan, dated April 4, 2007, shows a similar building configuration of four buildings; however, the building square footage has been reduced from 12,000 square feet per building to 10,000 square feet per building. This results in a reduction of 17% of the total building floor area on the eight acres.
 - In addition, the parking to the south of the buildings has been changed to phantom parking, which eliminated 34 net spaces. The plan also shows 42 additional phantom spaces. Seven parking spaces have been added just east of the southeastern-most building on the main drive aisle.
 - In order to make the site economically feasible, and taking into consideration the reduction in square footage, they are asking that the southern half of the roadway be financed by others. The southern half of the roadway is west of the child care center and just south of the subject development. It was noted that they would be designing and building the road.

Responding to questions from the Commission, Mr. Harp stated the following:

- **Regarding the floor area ratio and open space on the alternate proposal:** The floor area ratio on the alternate proposal is .13, which decreased from .15. The open space increased from 58% to 65%, which does not include the calculation for the phantom parking.
- **Regarding parking:** The January 30th plan had 192 parking spaces; the alternate plan, dated April 4th, has 158 parking spaces – a reduction of 34 spaces. The plan also shows 42 phantom parking spaces in the front (south of the building, north of the East/West Road). The 42 phantom spaces are in addition to the 158 spaces.

The original plan had 4 spaces/1000 as required by the Planned Commercial zoning ordinance. The alternate plan has a reduced square footage of 40,000 – at 4 spaces/1000, 160 spaces are required – 2 more than the 158 being provided. They meet the parking requirements of 3.3 spaces/1000 for office space and 4.5 spaces/1000 for medical office – splitting it as 20,000 square feet for office and 20,000 square feet for medical. The phantom parking would increase the parking above 4 spaces/1000 and would only be used if a particular user needed more than 4.5 spaces for medical or 3.3 spaces for general office.

- **Regarding the requirements of the Attachment A:** The Petitioner is willing to have the requirements of the Attachment A changed with respect to .13 floor area ratio and 10,000 square feet per building. They would like to keep the open space percentage at 58% because of the phantom parking.

Commissioner Banks asked if the City is permitted to grant the Petitioner's request to have the southern half of the roadway financed by others. City Attorney Heggie replied that the City could consider this request. Mr. Harp pointed out that the Developer doesn't have development rights to the south side of that part of the roadway and is currently obligated to pay for the entire road.

RE: P.Z. 02-2007 The Estates at Upper Kehrs Mill (Miceli Construction)

Petitioner:

1. Mr. David Volz, 10849 Indian Head, St. Louis, MO stated the following:
 - They do not yet have a final topographic survey so the original plan, presented at the last meeting, showed 30% tree retention. A revised plan has been submitted showing 40% tree retention, with a lot more trees along the northern edge of the site. When they have the final topographic survey, they will be able to make the final determination on tree retention, which they feel will be 35-40%.

- Regarding the issue of the cul-de-sac location, he noted that if the cul-de-sac is moved further to the south, more vegetation will be disturbed. On a 3:1 slope, every foot the cul-de-sac is moved to the south, three more feet of vegetation will be disturbed. They have located the cul-de-sac in a place where it would cause the least amount of disturbance on the site.
- The E-One zoning meets the City's Comprehensive Plan and is similar to the surrounding neighborhoods. Six of the eight lots are now one acre in size; the remaining two lots average out to .8 acre and are 20% larger than the adjacent lots in Country Place.
- With respect to protecting the "visual integrity" of Kehrs Mill Road, Speaker stated that the proposed site is quite a bit higher than Kehrs Mill Road. Speaker noted that driving south on Kehrs Mill Road, motorists will be looking into the second floor of homes in Tuscany Reserve. At the proposed site, they have been able to preserve the vegetation along the road and the house will be far above the road. As a motorist travels north, arriving at the southern property line, the car will be 50 feet lower than the finished floor of the house on Lot 8. They feel that the grade difference of the site, along with the vegetated buffer, will maintain the integrity of Kehrs Mill Road.

Commissioner Geckeler expressed concern with the tree removal on the north side with respect to the location of the cul-de-sac. She noted that the proximity of the contingent houses on the north is closer than the contingent houses on the south. Mr. Volz stated that they would be able to move the cul-de-sac about 10 feet to the south, but pointed out that there will be more disturbance. If the cul-de-sac is moved to the south, it is being pushed down the hill. The disturbance will not result from road construction; the disturbance will be caused by pushing the house further down the hill. Chair Hirsch pointed out that this issue would be reviewed at the Site Plan stage.

Responding to questions from the Commission, Mr. Volz stated the following:

- **Regarding concerns expressed by the Country Place residents with respect to possible erosion:** The drainage that currently goes to the north is about 1.9 acres of ground. When the development is complete, the drainage going to the north will be about 1.3 acres. With respect to the water flowing to the south, a sewer system will be installed near Lot 6 so most of the water flowing to the south will be piped through their detention basin.
- **Regarding concerns about possible erosion into Country Place's lakes:** They will test the lakes before and after construction to determine if any silt has entered the lakes as a result of the construction process. The developer has numerous enforcement factors for silt control that must be followed – such as, required escrows; stop work orders can be issued if a drainage problem arises; NPDES Permits are required; 404 Permits are required; DNR Permits are required; and EPA is also involved.

Councilmember Fults stated that if the Planning Commission chooses not to require testing of the lakes pre- and post-construction, the issue will be addressed at City Council.

Commissioner Perantoni stated that the Tuscan Place area is the introduction to the City and this area is very important visually. She felt that the one-acre lots for the proposed site are situated in a manner that would enhance the area and asked that the one-acre lots not be changed.

Mr. Geisel asked if the Petitioner would be both developing and selling the individuals homes. He stated that clarification is needed because of the controls that go with siltation. Bonds, pre-construction surveys, and the lake protection bonds will be required as part of the issuance of any grading permit. There are significantly different controls, and much more limited controls, if an individual building permit is issued. Mr. Volz stated that the developer is also the builder.

RE: P.Z. 14-2007a Wilson Bluffs (SMS Group, L.L.C.)

Petitioner:

1. Mr. Sean Sortor, 15543 Country Ridge Drive, Chesterfield, MO stated the following:
 - They have changed the zoning request from E-Half Acre to E-One Acre.
 - During Site Plan development, a lake protection bond will be required with respect to Chesterfield Lakes. Run-off issues will also be addressed at the Site Plan review.
 - They will work within the Tree Preservation guidelines with respect to tree removal. He plans on building his home on the site and will be preserving as many trees as possible.
 - At this time, nothing is designed for the rear of the property. An access will be provided at the south side of the property off Wilson Road. The driveway on the north side will be closed off.
 - There will be approximately over 200 feet between the two proposed homes.

Chair Hirsch clarified that the plans presented still show two pieces. However, the discussion at tonight's meeting is only about the 3.2 acre parcel on Wilson Road. E-One Acre zoning is being requested for this site with only two lots.

In Opposition:

1. Mr. Nelson Wainwright, representing Chesterfield Lakes Subdivision as Lake Committee Chairman, 39 Chesterfield Lakes Road, Chesterfield, MO stated the following:
 - He noted that he addressed the Commission last month on the subject petition. He thanked Staff for being responsive to their needs.

- He requested that the Commission require a \$1 million bond and pre- and post-construction soundings of the lakes.
- He invited the Commissioners and the Petitioner to walk the area to see how run-off could impact their lake.

VII. SITE PLANS, BUILDING ELEVATIONS AND SIGNS

- A. **Paddington Hill:** Amended Record Plat for a 7.8 acre site zoned “R-3” Residence District with a Planned Environment Unit

Commissioner Asmus, representing the Site Plan Committee, made a motion to approve the Amended Record Plat. The motion was seconded by Commissioner Sherman and **passed by a voice vote of 9 to 0.**

- B. **Somerset, Plat 1a, Lot 5 (1267 Somerset Drive):** A Residential Addition on an existing home zoned “R-2” Residence District and a “PEU” Planned Environmental Unit located at 1267 Somerset Field Drive in the Somerset Subdivision.

Commissioner Asmus, representing the Site Plan Committee, made a motion to approve the Residential Addition. The motion was seconded by Commissioner Perantoni and **passed by a voice vote of 9 to 0.**

(The meeting recessed for a five-minute break.)

VIII. OLD BUSINESS

- A. **P.Z. 28-2006 Chesterfield Neighborhood Office Park (17655 and 17659 Wild Horse Creek Road):** A request for a change of zoning from “NU” Non-Urban to “PC” Planned District with a “WH” Wild Horse Creek Road Overlay for 8.04 acre tract of land located north of Wild Horse Creek Road and west of Long Road. (18V510095 & 18V510040)

Ms. Aimee Nassif, Senior Planner, stated that the site is located in the area referred to as the “bowtie” area. The only permitted uses being requested are “Office, Medical Office, Parks and Wildlife Area”.

The only open issue deals with the parking requirement. The Wild Horse Creek Road Overlay states that a parking modification can be requested when good urban planning and architecture is shown and demonstrated. The Petitioner is asking for such a modification.

Chair Hirsch stated that the parking issue will require a separate vote because the Wild Horse Creek Road Overlay requires that there only be nine parking spaces per lot. He then asked how many total parking spaces the Planning Commission would be voting upon. Ms. Nassif clarified that the Commission would be voting on the density of the site - the overall square footage of the buildings. In the Wild Horse Creek Road Overlay, the parking is based upon the square footage for the site. The current proposed Preliminary Plan shows 192 parking spaces. Any vote by the Commission would be to allow for a specific amount of square footage of buildings and the parking for the square footage. The Alternate Plan shows 158 built parking spaces with 42 phantom parking spaces.

If the Alternate Plan of 40,000 square feet of development is approved, the maximum parking requirement would be 160 parking spaces with a possible 42 additional phantom parking spaces. The phantom parking would only be constructed at the direction of the City.

Commissioner Perantoni asked if the Commission is required to vote on the phantom parking at this time. Chair Hirsch replied that if the motion is to limit the parking to the amount as required in the Chesterfield Code, it would be 160 spaces for 40,000 square feet – the phantom parking would not be included in such a motion. Ms. Nassif stated if additional parking over the 160 spaces would be required at a later time, the Petitioner would have to submit an Amended Site Development Plan for approval.

Mr. Geisel stated that a motion could be made requiring the maximum number of parking spaces to be as dictated by Chesterfield Code. The motion could also include language that would allow the Petitioner to construct some specific amount of phantom parking at the City's direction.

Commissioner Geckeler asked for clarification on whether the limits of clearing and tree removal had been exceeded. Mr. Geisel replied that, unrelated to this petition – but on the same site, the City previously granted a grading permit to repair an eroded condition, which was creating problems for the railroad and for the airport. There were specific trees and clearing limits identified with the grading permit. They are in violation of this grading permit. The City had inventoried the trees so it is clear which trees have been disturbed. The remedial action that the City will take on imposing sanctions on the violation is dependent upon what action is taken on the subject petition. The trees that were designated to be saved would be removed as part of this petition, if approved. It does not eliminate the violation – but it changes the remedy. In most circumstances, Staff would discourage the Commission not to vote on a petition submitted by a Petitioner who is in violation. However, in this case, slowing the progress – or stopping work – would exacerbate the problem.

Chair Hirsch stated that two motions are needed on the petition:

- A motion on the parking requirements, which would need six affirmative votes for passage; and
- A vote on the rezoning request.

Commissioner Sherman asked if language has been provided in the Attachment A with respect to the Petitioner's request that a portion of the roadway be funded from other sources. Ms. Nassif replied that the Alternate Attachment A provides such language under Section I.K.2.

Ms. Nassif referred to the Alternate Attachment A noting the following:

- The Floor Area Ratio has been amended to a maximum of .13.
- The total square footage has been amended to a maximum of 40,000 square feet.
- The parking requirement would need to be amended requiring that it meet City Code.
- Landscaping would be required for the phantom parking area until built.

Commissioner Banks made a motion to waive the parking requirements of the Wild Horse Creek Road Overlay District and to substitute the requirements as provided in the Planned Commercial District of City Code for a maximum of 160 parking spaces. The motion was seconded by Commissioner Schenberg.

Upon roll call, the vote was as follows:

**Aye: Commissioner Banks, Commissioner Broemmer,
Commissioner O'Connor, Commissioner Perantoni,
Commissioner Schenberg, Commissioner Sherman,
Commissioner Asmus, Chairman Hirsch**

Nay: Commissioner Geckeler

The motion passed by a vote of 8 to 1.

City Attorney Heggie suggested that the Alternate Attachment A be amended as follows:

Section I.E.1.b.

~~Phantom parking may be utilized for 42 of the parking spaces for this development as shown on the preliminary plan as directed by the City of Chesterfield. The portion of the site containing the phantom parking shall remain as open space with appropriate landscaping as approved by the City of Chesterfield until said parking is constructed.~~

Commissioner Schenberg asked whether the sentence requiring the phantom parking area to remain as landscaped open space should remain. City Attorney Heggie stated that it could remain; however, he felt that the language is “cleaner” without it.

Chair Hirsch asked if there is any underlying code dealing with phantom parking. Mr. Geisel stated that the phantom parking area, if not constructed, must remain as open space and landscaped.

Commissioner Sherman made a motion to approve P.Z. 28-2006 Chesterfield Neighborhood Office Park (17655 and 17659 Wild Horse Creek Road), as amended in the above motion by Commissioner Banks, and using the Alternate Attachment A with the proposed amendment to Section I.E.1.b. as noted above. The motion was seconded by Commissioner Banks.

Upon roll call, the vote was as follows:

Aye: Commissioner Broemmer, Commissioner O’Connor, Commissioner Perantoni, Commissioner Schenberg, Commissioner Sherman, Commissioner Asmus, Commissioner Banks, Chairman Hirsch

Nay: Commissioner Geckeler

The motion passed by a vote of 8 to 1.

B. P.Z. 02-2007 The Estates at Upper Kehrs Mill (Miceli Construction): A request for a change of zoning from “NU” Non-Urban to “E” One Acre District for a 10.2 acre tract of land located on the eastern side of Kehrs Mill Road, 4,100 feet south of its intersection with Wild Horse Creek Road. (19U530062, 19U530392)

ISSUES:

1. Review language in the Westland Acres Ordinance dealing with drainage and siltation control with the possibility of adapting it to the subject petition. Ms. McCaskill-Clay stated that said language is based upon Chapter 12 of the City’s Municipal Code, Section 21, paragraphs 1 and 2, which require base screenings and required measurements. This language can be added to the Attachment A.
2. Can the bond amount be calculated at this time? Mr. Geisel stated that the bond amount cannot be calculated until survey information is received.
3. What is the setback of the Perry house from Kehrs Mill Road?
4. Retain more trees on the north side of the cul-de-sac. Commissioner Sherman noted that this issue is the concern of one Commissioner and may

not be a consensus. Chair Hirsch stated that this is a Site Plan issue rather than an issue for the Attachment A.

- C. **P.Z. 10-2007 Surrey Place (St. Luke's Episcopal Presbyterian Hospital)**: A request to amend Conditional Use Permit Number 561 to add hospital as a permitted use for an approximately 10.751 acre tract of land located at 14701 Olive Boulevard, east of the intersection of Ladue Road and Olive Boulevard. (17R530197)

Commissioner Sherman made a motion to hold **P.Z. 10-2007 Surrey Place (St. Luke's Episcopal Presbyterian Hospital)** until the Department can provide more information on the following issues:

- **The number of levels permitted in the development;**
- **The setback with respect to the fire lane;**
- **Clarification of the buffer requirements;**
- **Provide traffic study for the Kendall Bluffs development;**
- **Possibility of providing a disclosure to Kendall Bluffs property owners;**
- **Adding language to the Attachment A, similar to language used in the St. John's Rehabilitation facility, with respect to "hospital" use – not allowing emergency use, operating rooms, surgical services, or heliport or helipad;**
- **Possibility of providing a traffic accident report from the Police Department;**
- **Provide the differential between the residential properties in the Kendall Bluffs development and the proposed development.**
- **Provide the requirements of the Certificate of Need provided by the State, as well as whether the use would include outpatient services.**

The motion was seconded by Commissioner O'Connor.

Commissioner Broemmer stated he would like the scope of the traffic study to include the reducing of the speed limit on Olive Boulevard vs. installing a stop light. Chair Hirsch asked that this concern be held until a vote on the motion to hold is taken.

City Attorney Heggie felt that most of the issues stated above could be resolved this evening, with the exception of the issue of the traffic study.

Commissioner Asmus noted that the petition before the Commission tonight deals only with amending the C.U.P. by allowing "hospital" as a permitted use. He stated that he is not opposed to learning more about the traffic study but felt this was not the appropriate time to deal with it. He opposed the motion to hold.

Commissioner Schenberg stated that if all the issues, with the exception of traffic, could be resolved this evening, he didn't feel it was fair to prejudice the Petitioner

because “they’re the last one on the block”. If the development is approved and traffic issues arise because of it, he didn’t feel the Commission should presume the traffic issues wouldn’t be resolved. He didn’t think progress should be stopped because of a problem that might occur – he felt progress should be allowed and then if a problem arises, it should be resolved as necessary.

Commissioner Banks expressed concern that there is not a signalized intersection on Olive Street Road in connection with this project. He felt that the traffic study would aid the Commission’s judgment on whether the project has a possibility of working or not. He was not opposed to delaying the project for two weeks to get the information on traffic.

Ms. McCaskill-Clay stated that the draft Conditional Use Permit is based directly upon Conditional Use Permit 561, as approved by St. Louis County. The current Conditional Use Permit allows 100 self-care units associated with the C.U.P. development. It was pointed out that the site does not have 100 units but the C.U.P. does allow that many. The draft C.U.P. still includes this language – if the 100 units were built, it would substantially affect the amount of parking for the site. The C.U.P. was written when the Petitioner still owned the other 68 acres that has since become Kendall Bluffs. If the number of self-care units is adjusted, it may help address some of the parking and traffic concerns. It was noted that the patients using self-care units generate more traffic than the residents using the nursing care facilities.

Mr. Doster stated that there are currently 140 beds in the full-care nursing facility. The existing C.U.P. permits an additional 100 self-care units, which have not been built and which the Petitioner is not requesting. Mr. Doster indicated that the Petitioner would not be opposed to amending Section 2.ii of the C.U.P. as follows:

~~There shall be not more than 100 self-care units associated with this C.U.P. development.~~ The full care nursing facility shall have no more than 140 beds.

Chair Hirsch asked the Commissioners if their desire would be to have the City Attorney work with the Petitioner and Staff to draft amendments to the C.U.P. to resolve the open issues so the petition could be voted upon this evening. He also asked if the Commissioners still had any concerns with the traffic.

Commissioner Banks expressed concern about voting on the petition the same evening the Public Hearing was held. He did not see any problem with delaying the vote for two weeks.

Ms. Mara Perry, Senior Planner, pointed out that the petition is a Conditional Use Permit – not a petition for rezoning. The process for a Conditional Use Permit is to hold the Public Hearing and vote on the same night. Ms. McCaskill-Clay stated

that the Zoning Ordinance allows the Commission to delay the vote and/or to place conditions on the development.

At this point, Commissioner Sherman withdrew the motion to hold P.Z. 10-2007 Surrey Place (St. Luke's Episcopal Presbyterian Hospital).

It was agreed that the City Attorney, Staff and the Petitioner would work on amending specific language in the C.U.P.

Commissioner O'Connor made a motion to move Agenda Item VIII.C. P.Z. 10-2007 Surrey Place (St. Luke's Episcopal Presbyterian Hospital) below Agenda Item VIII.F. P.Z. 16-2007 Valley Gates Subdivision (16845 N. Outer 40 Road). The motion was seconded by Commissioner Asmus and **passed by a voice vote of 9 to 0**.

- D. **P.Z. 12-2007 City of Chesterfield (Tree Manual)**: A request to repeal City of Chesterfield Ordinance 2335 and replace it with a new ordinance that revises the procedures and requirements for Tree Protection Sureties, Escrows and Landscape Installation Bonds.

Councilmember Fults asked for clarification on how the amount required for a Tree Protection Surety would be calculated.

Mr. Jarvis Myers, Project Planner, stated that the Tree Manual allows two methods to calculate the required amount for a Tree Protection Surety. Staff would select the lesser amount for the bond amount. It is hoped this method will encourage petitioners to increase the tree preservation above the minimum requirement.

Mr. Geisel stated that the current language discourages tree retention. The existing language is based on the acreage to be preserved vs. the at-risk perimeter. The proposed language would allow two calculations so Staff can assess the risk based on the perimeter that is exposed to grading or the total acreage of trees to be disturbed. The proposed language would not penalize a developer that may want to leave a long buffer or a deep wooded area.

Commissioner Schenberg made a motion to approve P.Z. 12-2007 City of Chesterfield (Tree Manual). The motion was seconded by Commissioner O'Connor.

Upon roll call, the vote was as follows:

Aye: Commissioner Geckeler, Commissioner O'Connor, Commissioner Perantoni, Commissioner Schenberg, Commissioner Sherman, Commissioner Asmus, Commissioner Banks, Commissioner Broemmer, Chairman Hirsch

Nay: None

The motion passed by a vote of 9 to 0.

E. P.Z. 14-2007a Wilson Bluffs (SMS Group, L.L.C.): A request for a change of zoning from "NU" Non-Urban District to "E-Half Acre" Estate District for a 3.28 acre tract of land located on the west side of Wilson road, directly north of Wilson Manors II Subdivision.

ISSUES:

1. Is an easement required for construction for the back portion of the site? Ms. Nassif stated that an easement will be required for the back portion lot. The new zoning does not allow for any landlocked parcels. Mr. Geisel pointed out that the access is permanent. The lot split plat will show a permanent easement and it will be recorded with the St. Louis County Recorder's Office.
2. Should language be included in the Attachment A requiring prospective buyers to be informed that a road will be constructed? Ms. Nassif stated that it will be shown on the plat. Mr. Geisel stated that it will also show up on the owner's title policy.
3. Amend the language from E-Half Acre to E-One Acre. Chair Hirsch stated that future agendas would show the petition as E-One Acre.

F. P.Z. 16-2007 Valley Gates Subdivision (16845 N. Outer 40 Road): A request for an amendment to City of Chesterfield Ordinance 2154 to allow for a change to the parking and building setbacks, building height and number of permitted buildings for a 7.698 acre tract of land zoned "PC" Planned Commercial located north of North Outer Forty and east of Boone's Crossing at 16845 North Outer Forty Road. (17T520073)

Ms. Nassif stated that after the meeting packets went out, Staff received the agency comments from the Missouri Department of Transportation, which was the only outstanding issue.

ISSUES:

1. Check with the Public Works Department to make sure they have everything they need with respect to the subject petition.
2. Are three-story buildings allowed along Highway 40 vs. two-story buildings? Ms. Perry pointed out that the Larry Enterprises/Lynch Hummer site has been approved for a three-story building. The subject site is just to the east of Larry Enterprises. Ms. Nassif stated that the urban core is allowed buildings not to exceed 70 feet.

- C. P.Z. 10-2007 Surrey Place (St. Luke's Episcopal Presbyterian Hospital)**: A request to amend Conditional Use Permit Number 561 to add hospital as a permitted use for an approximately 10.751 acre tract of land located at 14701 Olive Boulevard, east of the intersection of Ladue Road and Olive Boulevard. (17R530197)

City Attorney Heggie proposed the following seven amendments to Conditional Use Permit #25: **(changes shown in bold)**

The following Conditional Uses shall be allowed in this development:

1. Hospital. As proposed, this use is to be a specialized, limited use facility that will provide general physical rehabilitative and medical services, including nursing, clinical and therapeutic services. **There shall be no emergency room, operating room, or surgical services permitted on the subject site. There shall be no heliport or helipad services permitted on the subject site.**
2. Nursing home ~~with associated self-care units and recreational facilities for the elderly.~~
 - i. ~~Nursing facilities and self-care units shall be contained in not more than two (2) buildings.~~
 - ii. ~~There shall be no more than 100 self-care units associated with this C.U.P. development.~~ The full care nursing facility shall have no more than 140 beds.

All provisions of the City of Chesterfield City Code shall apply to this development except as specifically modified below:

- ~~1. Self-care units shall be contained in a single building not more than three (3) building levels in height, excluding basement and cellar areas, underground parking and rooftop mechanical equipment.~~

2. All ~~other~~ buildings shall not exceed ~~two (2)~~ **one (1)** building levels in height, exclusive of basement and cellar areas, underground parking, **architectural features**, and rooftop mechanical equipment.

7. Structure Setbacks:
No building or structure, other than boundary walls, retaining walls, signs, light standards, flag poles or fences, shall be located within the following setbacks:
 - a. One hundred (100) feet of the right of way of Olive Boulevard
 - b. ~~Fifty~~ Ten (10) feet from northeastern limits of this C.U.P.
 - c. ~~Sixty (60)~~ **Fifty (50)** feet from the northern property boundary (N42°06'40"E)

8. No outdoor parking stall or internal roadway, except points of ingress, egress, **and the required fire lanes**, shall be located within the following setbacks:

Commissioner Banks expressed concern on voting on a petition with so many amendments verbally placed in the motion.

Commissioner Broemmer asked when the issue of traffic would be addressed. Chair Hirsch stated that traffic issues would be part of any Site Plan review. Mr. Geisel stated that, based upon the concerns raised this evening, he will investigate the traffic issues. He noted that Kendall Bluffs was recently approved allowing more than 100 units to utilize the intersection. He indicated he would report his findings back to the Commission.

Some of the Commissioners expressed concern about voting on the C.U.P. amendment prior to having information on the traffic issues.

Mr. Doster recommended that the petition be held for two weeks to give Mr. Geisel time to review the traffic issue. He noted, however, that the current C.U.P. allows considerably more density on the site than what is being requested. He pointed out that they are willing to eliminate the 100 self-care units and would be providing a very low traffic generator in its place. He felt that, conceptually, they are improving the traffic situation over what is permitted and he didn't think a traffic study is necessary under these circumstances.

Commissioner Asmus made a motion to hold P.Z. 10-2007 Surrey Place (St. Luke's Episcopal Presbyterian Hospital) until the next meeting of the Planning Commission. The motion was seconded by Commissioner Broemmer and **passed by a voice vote of 8 to 1. (Commissioner O'Connor voted "no".)**

IX. NEW BUSINESS - None

X. COMMITTEE REPORTS

Chair Hirsch announced that the **Ordinance Review Committee** is scheduled to meet on April 24, 2007 at 3:00 p.m.

Thirty minutes prior to the next Site Plan Committee Meeting, the **Committee of the Whole** will meet to review a revised draft of the Commission's By-Laws.

XI. ADJOURNMENT

The meeting adjourned at 9:45 p.m.

David Banks, Secretary



VII.A.

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

May 9, 2007

Planning Commission
City of Chesterfield
690 Chesterfield Parkway West
Chesterfield, MO 63017

The Planning Commission agenda for **May 14, 2007** will include the following item for your consideration:

Bacana Brasil: Amended Architectural Elevations for a 3.7 acre parcel zoned “C-8” Planned Commercial located on the north side of Chesterfield Parkway, east of Justus Post Road.

Dear Planning Commission:

Grant Alexander, of SP Architects, has submitted Amended Architectural Elevations for your review. The Department of Planning has reviewed this submittal and submits the following report.

BACKGROUND

1. On July 16, 1973, St. Louis County approved Ordinance 6815 which amended the St. Louis County zoning ordinance by changing the boundaries of various districts and the “C-8” and “FP C-8” Planned Commercial and Flood Plain Planned Commercial Districts. At that time, St. Louis County approved the preliminary plans and provided approval for the final development plan.
2. On October 22, 1990, the Planning Commission approved the site development plan and landscape plan for the restaurant building with one amendment. The Planning Commission requested that all elevations be constructed of the submitted reddish-brown brick masonry and split faced block concrete.

SUBMITTAL INFORMATION

1. The request is for amended architectural elevations to the existing elevations of a restaurant use.
2. The exterior building materials will be comprised of painted brick and wood, cultured stone over existing brick columns and a new wood trellis. The existing roof is to remain.

DEPARTMENTAL INPUT

The submittal was reviewed for compliance with all City of Chesterfield ordinances. The Department of Planning requests action on the Amended Architectural Elevations.

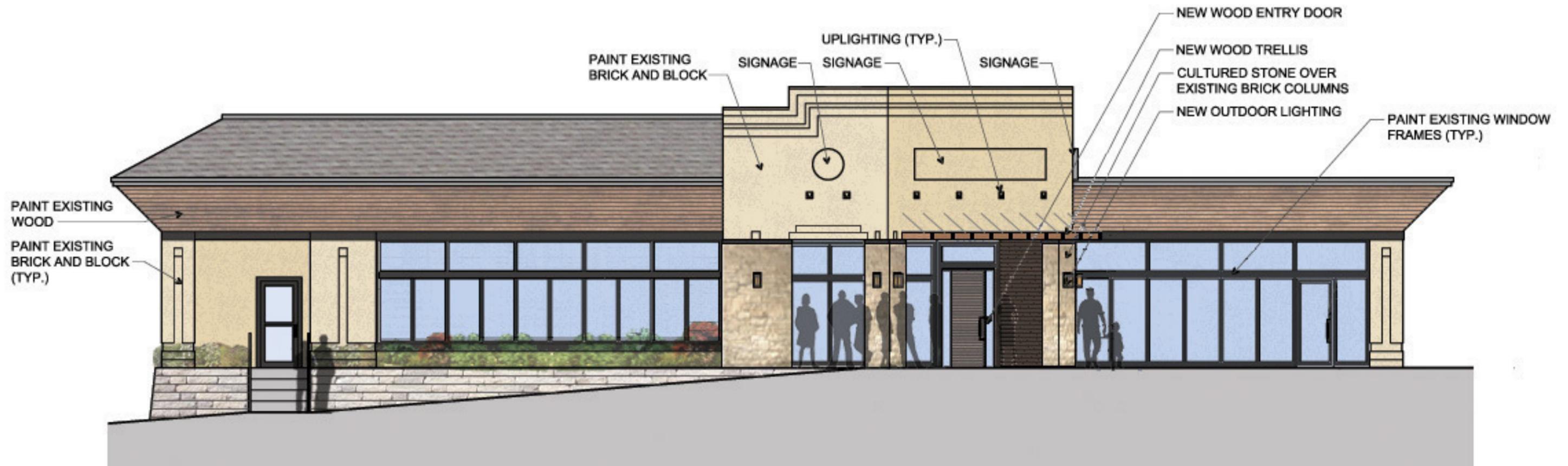
Respectfully submitted,

A handwritten signature in black ink, appearing to read 'Mara M. Perry', written in a cursive style.

Mara M. Perry, AICP
Senior Planner of Plan Review

Cc: City Administrator
City Attorney
Department of Public Works

Attachments:
Amended Architectural Elevations
Existing site plan for the site



SOUTH ELEVATION
1/8"=1'-0"

BACANA BRAZILIAN STEAKHOUSE

CHESTERFIELD, MISSOURI

EXTERIOR COLOR ELEVATIONS 4.18.07

SJP ARCHITECTS 314.872.9700



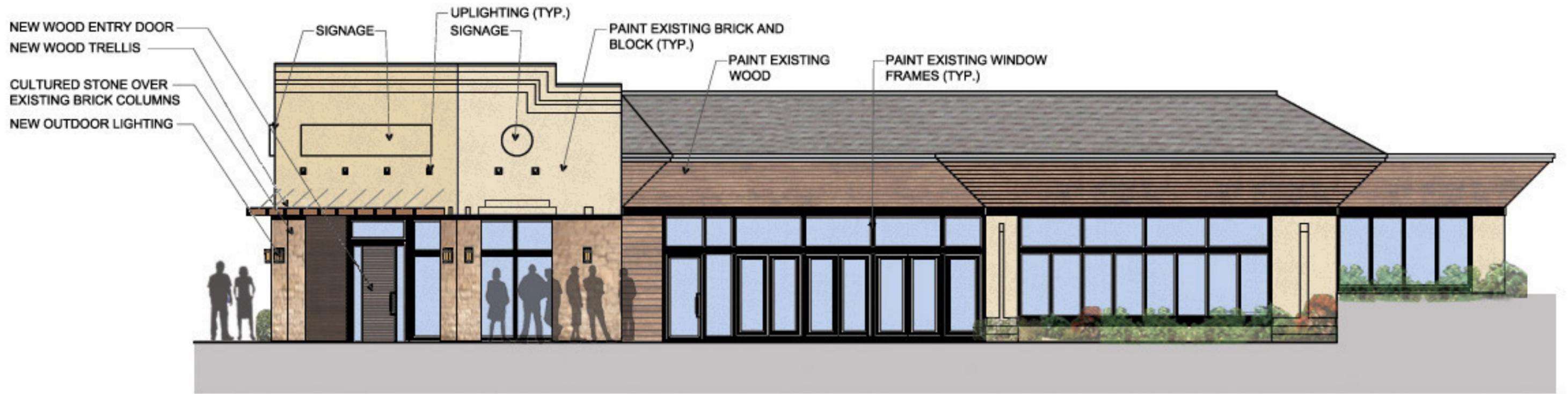
ENTRY ELEVATION
 1/8"=1'-0"

BACANA BRAZILIAN STEAKHOUSE

CHESTERFIELD, MISSOURI

EXTERIOR COLOR ELEVATIONS 4.18.07

SJP ARCHITECTS 314.872.9700



EAST ELEVATION
 1/8"=1'-0"

BACANA BRAZILIAN STEAKHOUSE

CHESTERFIELD, MISSOURI

EXTERIOR COLOR ELEVATIONS 4.18.07

SJP ARCHITECTS 314.872.9700



VII. B.

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

May 9, 2007

Planning Commission
City of Chesterfield
690 Chesterfield Parkway West
Chesterfield, MO 63017

The Planning Commission agenda for **May 14, 2007** will include the following item for your consideration:

Larry Enterprises/Lynch Hummer (MPD Investments, LLC): Amended Site Development Concept Plan and Conceptual Landscape Plan for a 14.11 acre parcel zoned "PI" Planned Industrial District located west of Boone's Crossing on the north side of North Outer Forty Road.

Dear Planning Commission:

Stock & Associates, has submitted, on behalf of MPD Investments, L.L.C. an Amended Site Development Concept Plan and Conceptual Landscape Plan for the above referenced project. The Department of Planning has reviewed this request and submits the following report.

BACKGROUND

1. On October 3, 2001, the City of Chesterfield approved Ordinance 1790 which rezoned the property from "NU" Non Urban District and "FPNU" Non Urban Flood Plain District to "PI" Planned Industrial District.
2. On January 21, 2004, the City of Chesterfield approved Ordinance 2055 which repealed Ordinance 1790 to allow for outdoor storage and a change to building setbacks.
3. On May 24, 2004, the Planning Commission approved the Site Development Concept Plan with a vote of 9-0.

SUBMITTAL INFORMATION

1. The request is for two new 66,000 square foot office buildings with first floor retail located on Lot A1 of the Larry Enterprises/ Lynch Hummer development. Exterior materials include architectural precast, brick accent panels and glass. Roof materials will be a flat membrane.

DEPARTMENTAL INPUT

The submittal was reviewed and is in compliance with all City of Chesterfield ordinances. The Department of Planning requests approval of the Amended Site Development Concept Plan and Conceptual Landscape Plan.

Respectfully submitted,

A handwritten signature in black ink, appearing to read 'Mara M. Perry', written in a cursive style.

Mara M. Perry, AICP
Senior Planner of Plan Review

Cc: City Administrator
City Attorney
Department of Public Works

Attachments:
Amended Site Development Concept Plan
Conceptual Landscape Plan

LARRY ENTERPRISES AND LYNCH HUMMER SUBDIVISION

AMENDED SITE DEVELOPMENT CONCEPT PLAN

FOR MPD INVESTMENTS DEVELOPMENT FOR A
"PLANNED INDUSTRIAL" DISTRICT
 A TRACT OF LAND BEING PART OF THE TRACTS OF LAND DESCRIBED IN DEED BOOK 7853 PAGE 1771
 OF THE ST. LOUIS COUNTY RECORDER OF DEEDS
 BEING PART OF THE SAME PROPERTY KNOWN AS THE RUPPEL FARM
 LOCATED IN U.S. SURVEY 125, TOWNSHIP 45 NORTH, RANGE 4 EAST OF THE 5TH PRINCIPAL MERIDIAN
 CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI
 RECORDED IN PLAT BOOK 352 PAGE 135
 ORDINANCE 2055

PERTINENT DATA

OWNER (1/9/07) = MPD INVESTMENTS, L.L.C.
 (SPECIAL WARRANTY DEED, DB. 17417, PG. 875)
 P.I. (ORD. #2055)
 EXISTING ZONING = 7.020 ADRFB ±
 LOCATOR NUMBER = CHESTERFIELD FIRE PROTECTION DISTRICT
 ACREAGE = ROCKWOOD R6
 FIRE DISTRICT = M.S.D.
 SCHOOL DISTRICT = BONHOMME CREEK
 SEWER DISTRICT = MISSOURI AMERICAN WATER COMPANY
 WATER SHED = LACLEDE GAS COMPANY
 WATER SERVICE = AMERENUE ELECTRIC COMPANY
 GAS SERVICE = SBC
 ELECTRIC SERVICE =
 PHONE SERVICE =

LEGEND

EXISTING SANITARY SEWER
 PROPOSED SANITARY SEWERS
 EXISTING STORM SEWER
 PROPOSED STORM SEWERS
 EXISTING TREE
 EXISTING BUILDING
 EXISTING CONTOUR
 PROPOSED CONTOUR
 PROPOSED INTERIM CONTOUR
 EXISTING SPOT ELEVATION
 PROPOSED SPOT ELEVATION
 EXISTING UTILITIES
 PROPOSED GAS
 PROPOSED WATER
 PROPOSED UNDERGROUND ELECTRIC

GENERAL NOTES

- BOUNDARY AND TOPOGRAPHIC SURVEY BY STOCK AND ASSOCIATES CONSULTING ENGINEERS, INC.
- ALL UTILITIES SHOWN HAVE BEEN LOCATED BY THE ENGINEER FROM AVAILABLE RECORDS. THEIR LOCATION SHOULD BE CONSIDERED APPROXIMATE. THE CONTRACTOR HAS THE RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES PRIOR TO CONSTRUCTION, TO HAVE EXISTING UTILITIES FIELD LOCATED. SHOULD ANY CONFLICTS BE EVENT, THE CONTRACTOR SHALL NOTIFY THE OFFICE OF THE ENGINEER IMMEDIATELY.
- NO GRADE SHALL EXCEED 3:1 SLOPE.
- FLOOR TO AREA RATIO (FAR):
 BLDGS. A & B = 132,000 S.F./LOT = 0.43
 BLDGS. C & D = 39,940 S.F./LOT = 0.13
 ENTIRE DEVELOPMENT - BLDGS. A, B, C & D = 171,940 S.F./LOT = 0.28
- AREA CALCULATIONS - (ENTIRE DEVELOPMENT BLDGS. A, B, C & D)
 TOTAL LOT AREA = 305,807 S.F. = 7.02 AC.
 BLDG. A = 66,000 S.F. = 1.50 AC.
 BLDG. B = 61,716 S.F. = 1.41 AC.
 BLDG. C = 9,600 S.F. = 0.22 AC.
 BLDG. D = 15,500 S.F. = 0.35 AC.
 ENTIRE DEVELOPMENT - BLDGS. A, B, C & D
 TOTAL LOT AREA = 614,705 S.F. = 14.11 AC.
 BLDG. A = 66,000 S.F. = 1.50 AC.
 BLDG. B = 61,716 S.F. = 1.41 AC.
 BLDG. C = 9,600 S.F. = 0.22 AC.
 BLDG. D = 15,500 S.F. = 0.35 AC.
- THE DEVELOPER SHALL BE REQUIRED TO CONTRIBUTE TO THE CHESTERFIELD VALLEY ROAD IMPROVEMENT FUND TRUST FUND.
- MAXIMUM HEIGHT OF ALL BUILDINGS EXCLUSIVE OF ROOF SCREENING AND PARAPET WALLS SHALL NOT EXCEED THREE STORIES OR 45 FEET (AS MEASURED FROM EXISTING GRADE) WHICHEVER IS LESS.
- ALL PROPOSED IMPROVEMENTS SHALL BE CONSTRUCTED TO CITY OF CHESTERFIELD STANDARDS.
- ON-SITE STORM WATER DRAINAGE REQUIREMENTS SHALL BE IN ACCORDANCE WITH THE CHESTERFIELD VALLEY MASTER STORM WATER DRAINAGE PLAN.
- GRADING AND STORM WATER PER M.S.D., CITY OF CHESTERFIELD AND THE MONARCH LEVEE DISTRICT.
- STORM WATER SHALL BE DISCHARGED AT AN ADEQUATE NATURAL DISCHARGE POINT. SINKHOLES ARE NOT ADEQUATE DISCHARGE POINTS.
- NO STEP ALLOWED AT ACCESSIBLE ENTRANCE DOORS.
- APPROVAL OF THIS PLAN DOES NOT CONSTITUTE APPROVAL OF SIGNAGE. SIGN APPROVAL IS A SEPARATE PROCESS.
- ALL UTILITIES SHALL BE INSTALLED UNDERGROUND. UTILITIES EASEMENTS THAT CROSS OVER CHESTERFIELD VALLEY MASTER STORMWATER EASEMENT SHALL BE SUBORDINATE TO THE CHESTERFIELD VALLEY STORMWATER EASEMENTS.
- SUBJECT PROPERTY LIES WITHIN FLOOD ZONE "AH" (SPECIAL FLOOD HAZARD AREAS INUNDATED BY 100-YEAR FLOOD DEPTHS OF 1 TO 3 FEET (USUALLY AREAS OF PONDING, BASE FLOOD ELEVATIONS DETERMINED, BASE FLOOD ELEVATION = 457.456) AND FLOOD ZONE "X" (AREAS PROTECTED BY LEVEES FROM 100-YEAR FLOOD) ACCORDING TO THE NATIONAL FLOOD INSURANCE PROGRAM FLOOD INSURANCE RATE MAP FOR ST. LOUIS COUNTY, MISSOURI AND INCORPORATED AREAS PER MAP NO. 15187AH WITH AN EFFECTIVE MAP DATE OF AUGUST 2, 1995 AND REVISED TO REFLECT LOWR DATED APRIL 17, 2000.
- STORMWATER DESIGN SHALL COMPLY WITH CHESTERFIELD VALLEY MASTER STORMWATER PLAN.

Brett Phillips, the owner of the property shown on this plan for and in consideration of being granted a permit to develop property under the provisions of Chapter 1003.150 "Planned Industrial" of the City of Chesterfield (applicable subsection) (present zoning).
 Ordinance No. 2055, do hereby agree that said property from the date of recording this plan shall be developed only as shown thereon, unless said plan is amended by the Planning Commission, or voided or vacated by order of the City of Chesterfield Council.
 (Signature) Brett Phillips

STATE OF MISSOURI)
 COUNTY OF ST. LOUIS)

On this _____ day of _____, 2007, before me personally appeared Brett Phillips, who being by me duly sworn, did say he is the OWNER of MPD INVESTMENTS, L.L.C., a Missouri limited liability company, and that said instrument was signed on behalf of said limited liability company, and that said Brett Phillips acknowledged said instrument to be the free act and deed of said limited liability company.

WITNESS WHEREOF, I have signed and sealed the foregoing the day and year first above written.

Notary Public
 Print Name

My commission expires:

This Amended Site Development Concept Plan was approved by the City of Chesterfield Planning Commission and duly verified on the _____ day of _____, 2007, by the Chairperson of said Commission, authorizing the recording of this Plan pursuant to Chesterfield Ordinance Number 2000 as attested to by the Director of Planning and the City Clerk.

Michael O. Gessel, Director of Planning

Marty DeMay, City Clerk

SHEET INDEX	
C1	AMENDED SITE DEVELOPMENT CONCEPT PLAN
C2	AMENDED SITE DEVELOPMENT CONCEPT PLAN-EASEMENTS
C3	AMENDED SITE DEVELOPMENT CONCEPT PLAN-ORDINANCE

04/30/07 - PER MONARCH FPD 4/24 AND CITY 4/26 COMMENTS
 04/18/07 - PER MONARCH FPD 3/20, CITY 3/30 AND M&DOT COMMENTS

LARRY ENTERPRISES & LYNCH HUMMER SUBDIVISION
 AMENDED SITE DEVELOPMENT CONCEPT PLAN

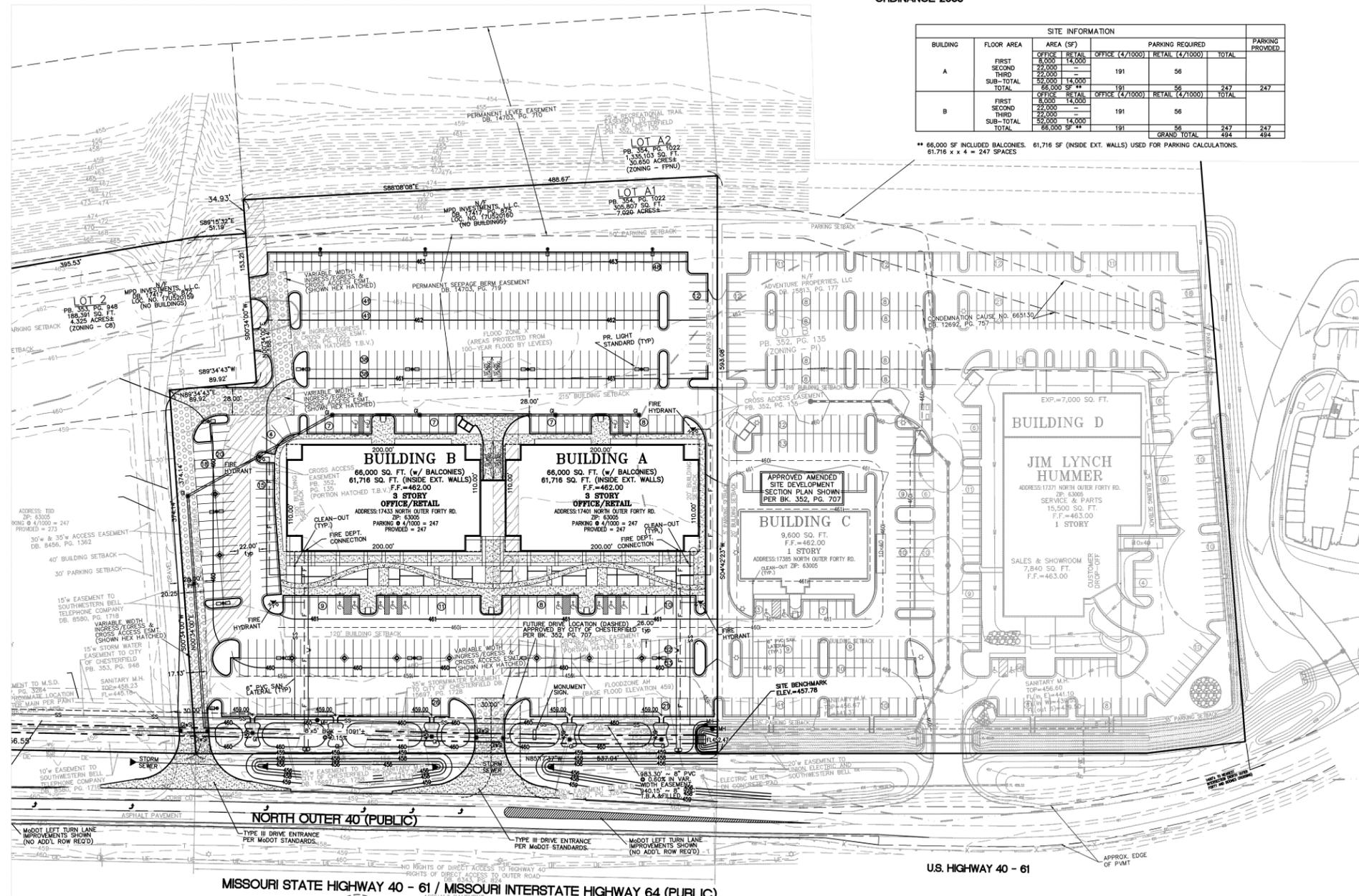
Stock & Associates
 Consulting Engineers, Inc.

257 Chesterfield Business Parkway
 St. Louis, MO 63005
 PH. (636) 530-9100
 FAX (636) 530-9130
 e-mail: general@stockassoc.com
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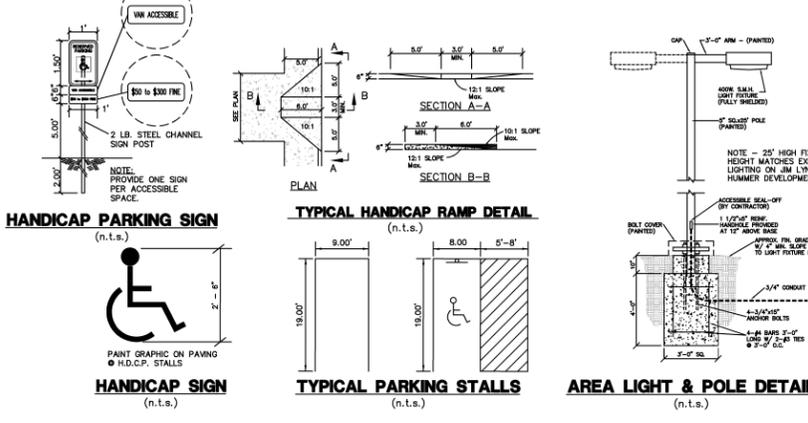
DRAM BY: J.P.W. DATE CHECKED BY: G.M.S. DATE JOB NUMBER: 03/27/07 206-3961.2 SHEET: 1 of 3

BUILDING	FLOOR AREA	SITE INFORMATION				PARKING PROVIDED
		OFFICE (4/1000)	RETAIL (4/1000)	PARKING REQUIRED	TOTAL	
A	FIRST	13,000	14,000	191	56	247
	SECOND	22,000	14,000	56	247	
	THIRD	22,000	14,000	56	247	
SUB-TOTAL		57,000	42,000	191	56	247
TOTAL		114,000	84,000	382	112	494
B	FIRST	8,000	14,000	191	56	247
	SECOND	22,000	14,000	56	247	
	THIRD	22,000	14,000	56	247	
SUB-TOTAL		52,000	42,000	191	56	247
TOTAL		104,000	84,000	382	112	494

** 66,000 SF INCLUDED BALCONIES. 61,716 SF (INSIDE EXT. WALLS) USED FOR PARKING CALCULATIONS.
 61,716 x 4 = 247 SPACES



LEGAL DESCRIPTION
 LOT A1
 Beginning at the intersection of the Northern line of Missouri State Highway 40-61 with the Eastern line of Share 1 of the Subdivision of the Estate of Peter Steffan, as recorded in Deed Book 86, Page 457 in the St. Louis County Records; thence Northwesterly along said Eastern line of Share 1 the following courses and distances: North 00 degrees 34 minutes 00 seconds East 374.14 feet, North 89 degrees 34 minutes, 43 seconds East 89.92 feet, and North 00 degrees 34 minutes 00 seconds East 188.00 feet; thence departing said Eastern, South 88 degrees 09 minutes 08 seconds East 488.67 feet to the Eastern line of Lot A of Larry Enterprises and Lynch Hummer Subdivision, a subdivision according to the plat thereof recorded in Plat Book 352, Page 135 of the St. Louis County Records; thence along the common line of Lots A and B of said subdivision, South 04 degrees 42 minutes 23 seconds West, 393.08 to the Northern line of Missouri State Highway 40-61; thence North 85 degrees 17 minutes 37 seconds West, 537.04 feet to the point of beginning and containing 305,807 square feet or 7.02 acres more or less.



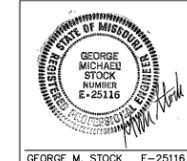
SITE BENCHMARK
 ELEV. 467.78
 IRON ROD BEING THE SOUTHEAST PROPERTY CORNER OF PROPERTY AT 17401 NORTH OUTER 40 DRIVE (U.S.G.S. (NGVD 1929))

ST. LOUIS COUNTY BENCHMARK
 12-171 STANDARD ALUMINUM DISK STAMPED SL-36, 1950. DISK IS SET AT THE NORTHWEST CORNER OF CHESTERFIELD AIRPORT ROAD AND CARRIAGE DRIVE. (U.S.G.S. (NGVD 1929))

GEOTECHNICAL ENGINEER'S STATEMENT
 Midwest Testing, at the request of Larry Enterprises, has performed a geotechnical exploration for the property of which the project proposed herein is a part thereof. Our findings indicated that the soil-related aspects are suitable for the development proposed herein pursuant to the geotechnical recommendations and considerations set forth in our December 5, 2002 report, titled Geotechnical Exploration - MT Job No. 3783 - Office Buildings - Chesterfield, Missouri.

SURVEYOR'S CERTIFICATION
 This is to certify that Stock and Associates Consulting Engineers, Inc. has prepared this Amended Site Development Concept Plan from an actual survey. The information shown is a correct representation of all existing and proposed land divisions.

STOCK AND ASSOCIATES CONSULTING ENGINEERS INC.
 L.S. No. 222-D
 DANIEL EHLMANN, Missouri L.S. No. 2215



PREPARED FOR:
 MPD INVESTMENTS, L.L.C.
 c/o BRETT PHILLIPS
 2445 DUNHEATH LANE
 O'FALLON, MO 63368
 PHONE: (314) 749-0000

OWNER (as of 1/9/07)
 MPD INVESTMENTS, L.L.C.
 c/o BRETT PHILLIPS
 2445 DUNHEATH LANE
 O'FALLON, MO 63368
 PHONE: (314) 749-0000

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LARRY ENTERPRISES AND LYNCH HUMMER SUBDIVISION

AMENDED SITE DEVELOPMENT CONCEPT PLAN

FOR MPD INVESTMENTS DEVELOPMENT FOR A
"PLANNED INDUSTRIAL" DISTRICT

LEGEND

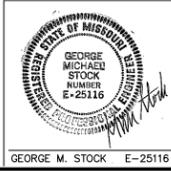
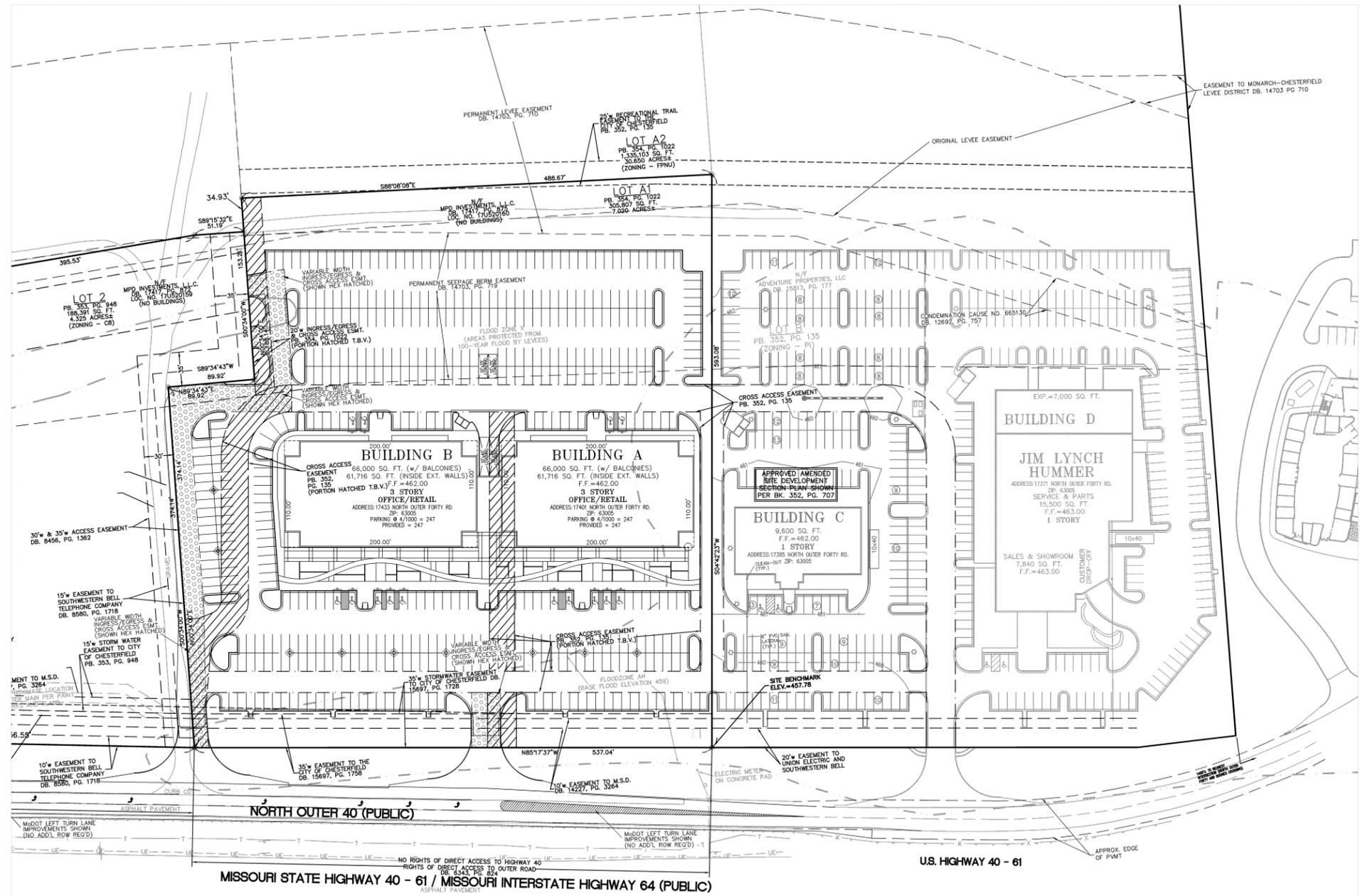
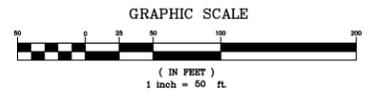
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SET 1/2" X 24" REBAR W/ 2" ALUMINUM CAP TO BE SET AFTER CONSTRUCTION AND/OR WITHIN 12 MONTHS AFTER RECORDING OF THIS PLAT

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- FOUND STONE
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ABBREVIATIONS

- A.T.G. - ADJUST TO GRADE
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04/30/07 - PER MONARCH FPD 4/24 AND CITY 4/26 COMMENTS
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MPD INVESTMENTS DEVELOPMENT
AMENDED SITE DEVELOPMENT CONCEPT PLAN-EASEMENTS

STOCK & ASSOCIATES
 Consulting Engineers, Inc.

257 Chesterfield Business Parkway
 St. Louis, MO 63005
 PH. (636) 530-9100
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DRAWN BY: J.P.W. DATE: 03/27/07 CHECKED BY: G.M.S. DATE: 03/27/07 JOB NUMBER: 206-3961.2 SHEET: 2 of 3

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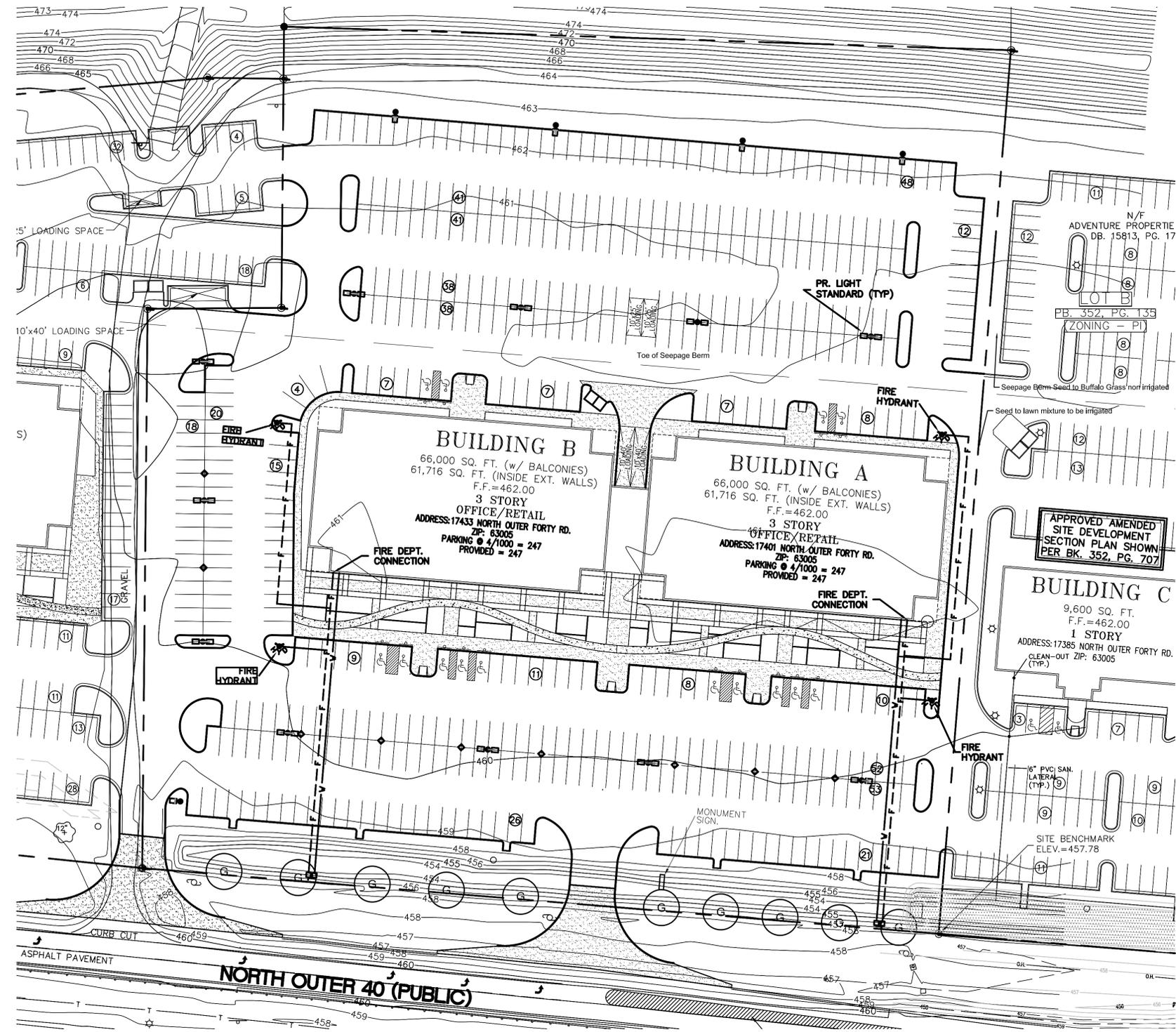


April 30, 2007

Consultants:

Larry Enterprises and Lynch Hummer Subdivision
 Site Development Concept Plan for MPD Investments, L.L.C.

Chesterfield, MO



Landscape Plan
 SCALE 1"=40'-0"

Landscape Requirements
 Street Trees Required = 10- 2 1/2" Trees
 Street Trees Provided = 10

STREET TREE SCHEDULE					
SYMBOL	QUANTITY	BOTANICAL NAME	COMMON NAME	SIZE	REMARKS
G	10	Zelkova serrata 'Green Vase'	Green Vase Zelkova	2 1/2"	45'+

Note:
 1) Lawn areas around building and parking islands to be sodded.
 2) Lawn areas to be seeded.
 3) In ground automatic irrigation system to be provided for landscape area south of the Seepage Berm.

Revisions:		
Date	Description	No.
3-28-07	Site Revisions	1
4-18-07	Site Revisions	2

Drawn: BD
 Checked: DD

loomisAssociates

landscapedesignplanners
 707 Spirit 40 Park Drive, Suite 135
 Chesterfield, Missouri 63005-1704
 E-mail: lhp@loomis.com

Sheet Title: **Landscape Plan**

Sheet No: **L-1**

Date: 3-13-07
 Job #: 687.006



VII. C.

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

May 9, 2007

Planning Commission
City of Chesterfield
690 Chesterfield Parkway West
Chesterfield, MO 63017

The Planning Commission agenda for **May 14, 2007** will include the following item for your consideration:

MPD Investments, LLC (Larry Enterprises/Lynch Hummer Lot A1): Site Development Section Plan, Landscape Plan, Lighting Plan, and Architectural Elevations for a 7.02 acre parcel zoned "PI" Planned Industrial District located west of Boone's Crossing on the north side of North Outer Forty Road.

Dear Planning Commission:

Stock & Associates, has submitted, on behalf of MPD Investments, L.L.C. a Site Development Section Plan, Landscape Plan, Lighting Plan and Architectural Elevations for the above referenced project. The Department of Planning has reviewed this request and submits the following report.

BACKGROUND

1. On October 3, 2001, the City of Chesterfield approved Ordinance 1790 which rezoned the property from "NU" Non Urban District and "FPNU" Non Urban Flood Plain District to "PI" Planned Industrial District.
2. On January 21, 2004, the City of Chesterfield approved Ordinance 2055 which repealed Ordinance 1790 to allow for outdoor storage and a change to building setbacks.
3. On May 24, 2004, the Planning Commission approved the Site Development Concept Plan with a vote of 9-0.

SUBMITTAL INFORMATION

1. The request is for two new 66,000 square foot office buildings with first floor retail located on Lot A1 of the Larry Enterprises/ Lynch Hummer development. Exterior materials include architectural precast, brick accent panels and glass. Roof materials will be a flat membrane.

ARB RECOMMENDATIONS

This project was reviewed by the Architectural Review Board on April 12, 2007. The ARB did not have a quorum however it forwarded the Site Development Section Plan, Architectural Elevations, Lighting Plan and Landscape Plan to the Planning Commission with no additional recommendations.

DEPARTMENTAL INPUT

The submittal was reviewed and is in compliance with all City of Chesterfield ordinances. The Department of Planning requests approval of the Site Development Section Plan, Landscape Plan, Lighting Plan and Architectural Elevations.

Respectfully submitted,



Mara M. Perry, AICP
Senior Planner of Plan Review

Cc: City Administrator
City Attorney
Department of Public Works

Attachments:
Site Development Section Plan
Landscape Plan
Lighting Plan
Architectural Elevations

LARRY ENTERPRISES AND LYNCH HUMMER SUBDIVISION

SITE DEVELOPMENT SECTION PLAN

FOR MPD INVESTMENTS DEVELOPMENT FOR A "PLANNED INDUSTRIAL" DISTRICT

LEGEND

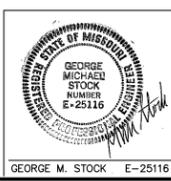
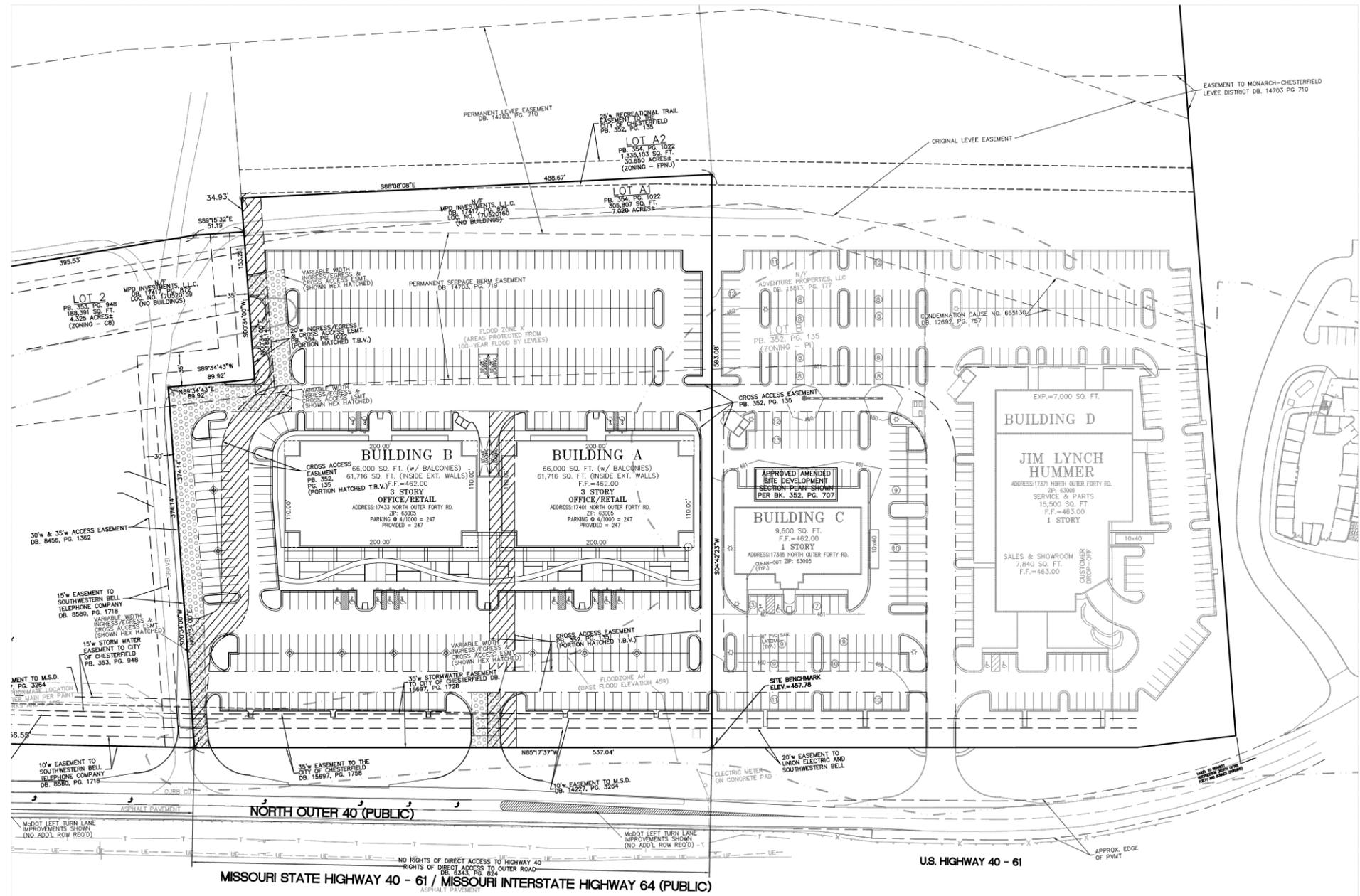
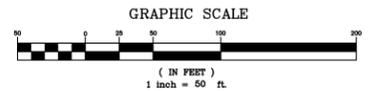
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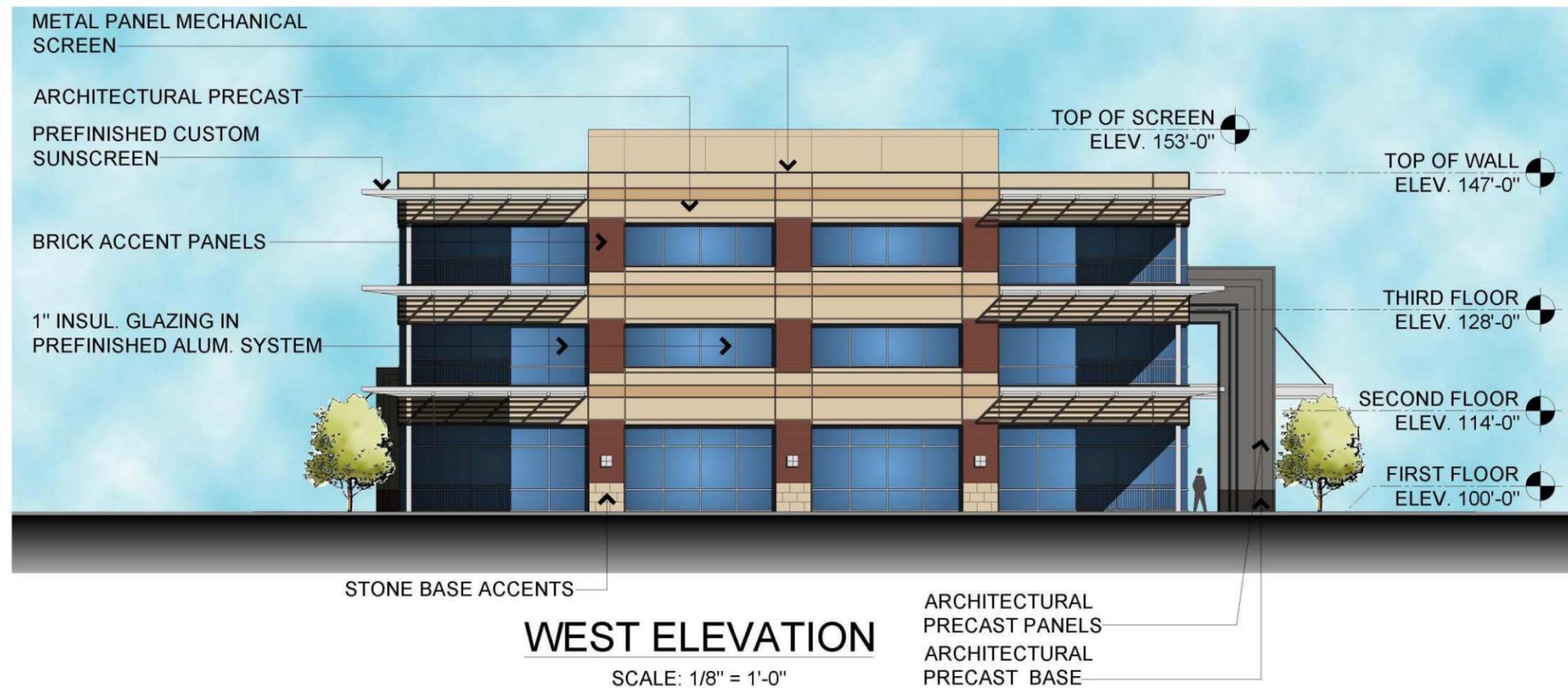
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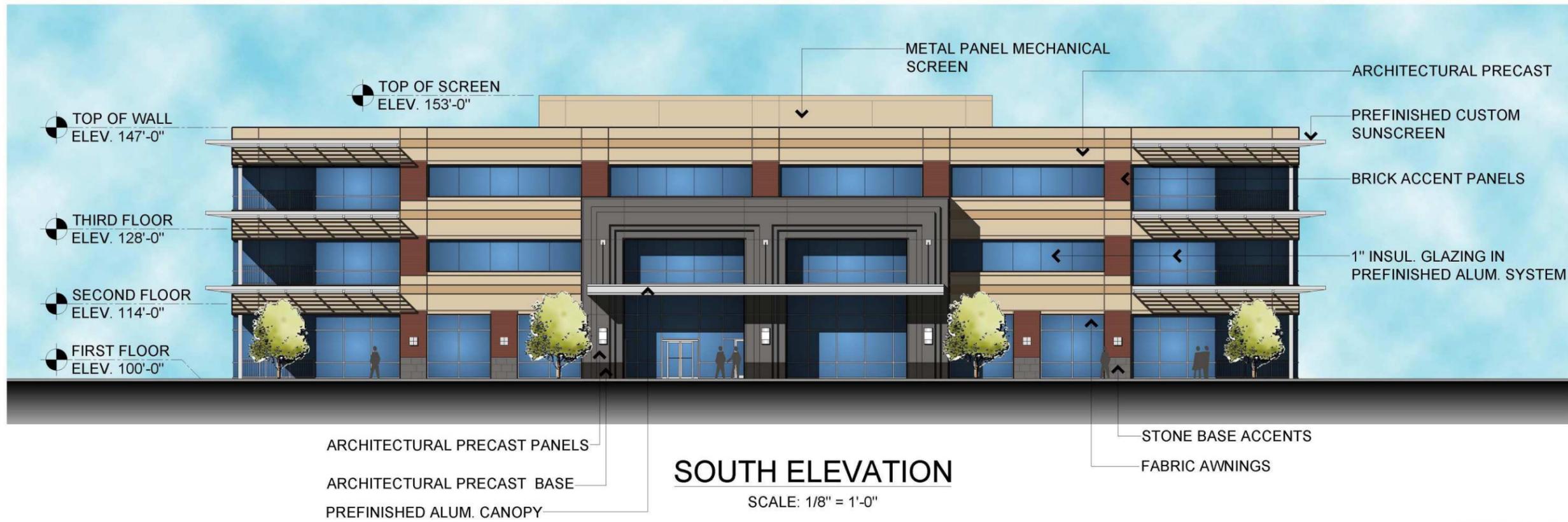
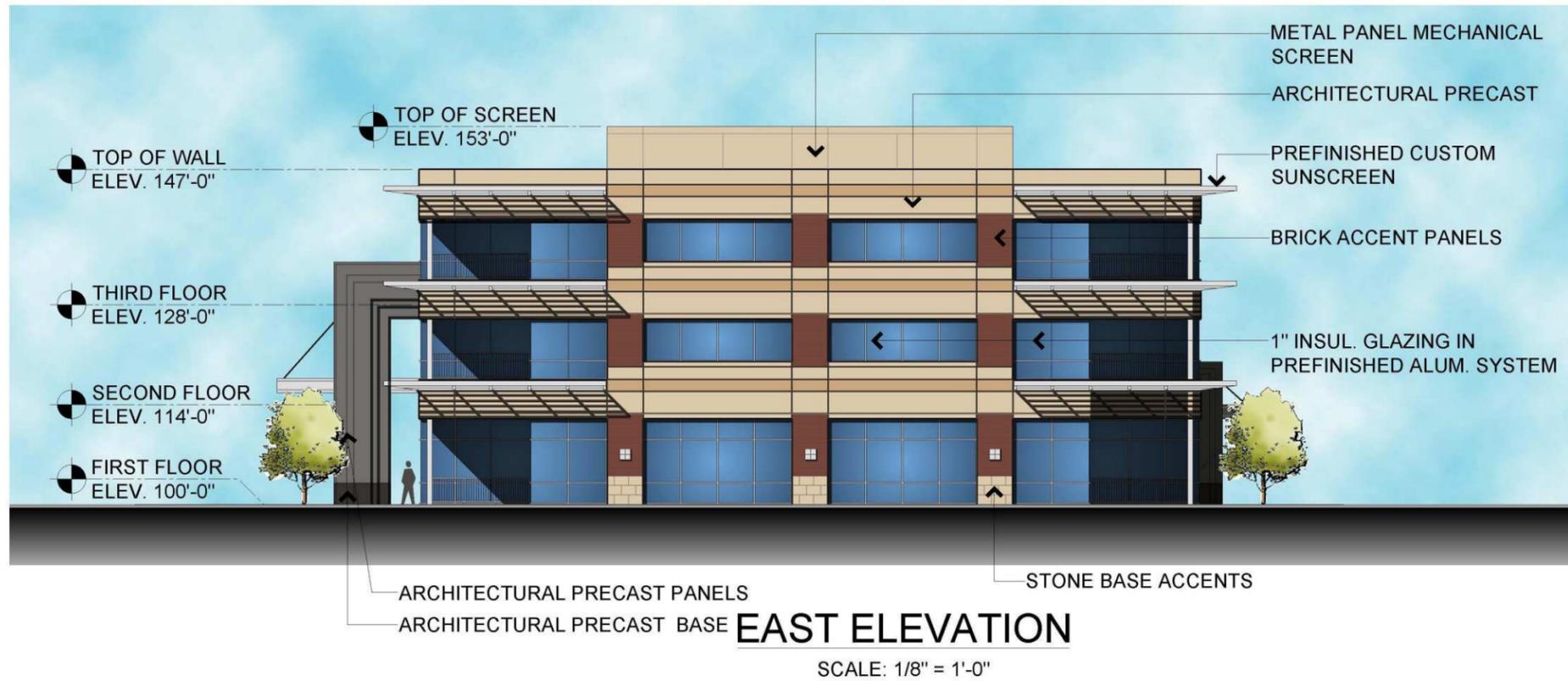
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207015 - March 29, 2007

MPD Investments
Chesterfield, Missouri



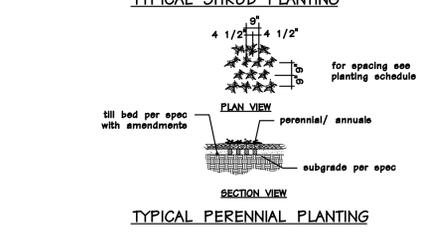
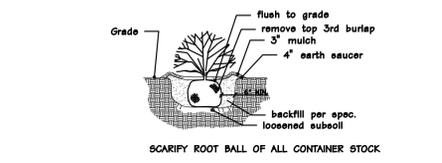
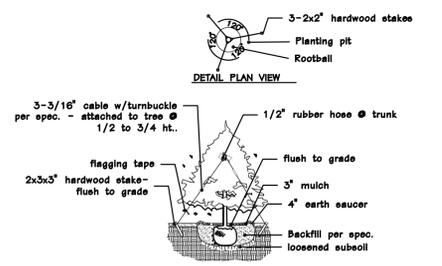
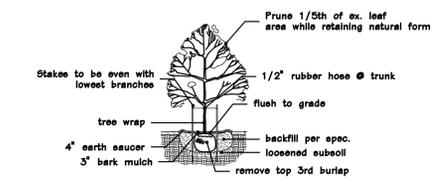
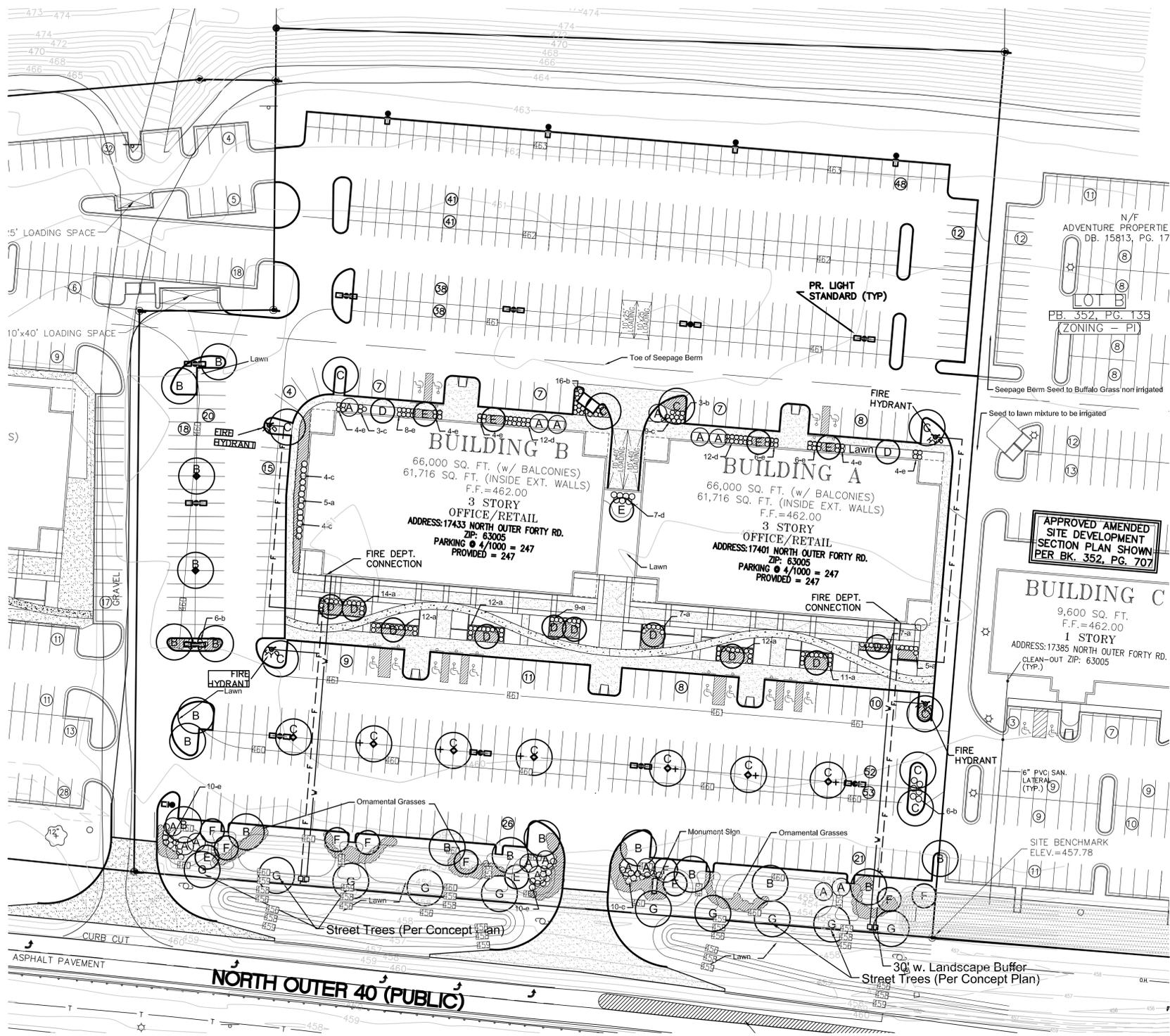
207015 - March 29, 2007

MPD Investments Chesterfield, Missouri



April 30, 2007

Consultants:



Landscape Plan
SCALE 1"=40'-0"

Landscape Requirements
Parking Lot Trees Required = 67
Total Trees Provided= 79 (Not including Street Trees)

STREET TREE SCHEDULE (Per Concept Plan)				
SYMBOL	QUANTITY	BOTANICAL NAME	COMMON NAME	REMARKS
G	10	Zelkova serrata 'Green Vase'	Green Vase Zelkova	2 1/2" 45'+

PLANTING SCHEDULE- SITE						
SYMBOL	QUANTITY	BOTANICAL NAME	COMMON NAME	SIZE	REMARKS	TREE TYPE
A	16	Pinus strobus	White Pine	8'	45-60'	Evergreen
B	18	Acer rubrum inermis	Red Maple	2 1/2"	50-75'	Canopy
C	16	Gleditsia triacanthos 'Inermis Skyline'	Skyline Honeylocust	2 1/2"	45'+	Canopy
D	12	Magnolia virginiana	Sweetbay Magnolia	2 1/2"	15'-25'	Ornamental
E	8	Cercis Canadensis	Eastern Redbud	2 1/2"	25-30'	Ornamental
F	8	Taxodium distichum	Bald Cypress	2 1/2"	45'+	Canopy
a	97	Ilex x meserveae 'Blue Girl'	Blue Holly	18"	3' O.C.	
b	31	Rhus aromatica 'Grow-low'	Grow-low Sumac	18"	3' O.C.	
c	21	Spirea x bumalda 'Anthony Waterer'	Anthony Waterer Spirea	18"	30" O.C.	
d	31	Euonymus alatas 'Compacta'	Dwarf Burning Bush	2-3'	3' O.C.	
e	71	Taxus cuspidata 'Densa'	Dense Yew	18"	30" O.C.	
-	-	Annuals/Perennials/Ground cover	To Be Selected			

Note:
1) Lawn areas around building and parking islands to be sodded.
2) Lawn areas to be seeded.
3) In ground automatic irrigation system to be provided for landscape area south of the Seepage Berm.

Larry Enterprises and Lynch Hummer Subdivision
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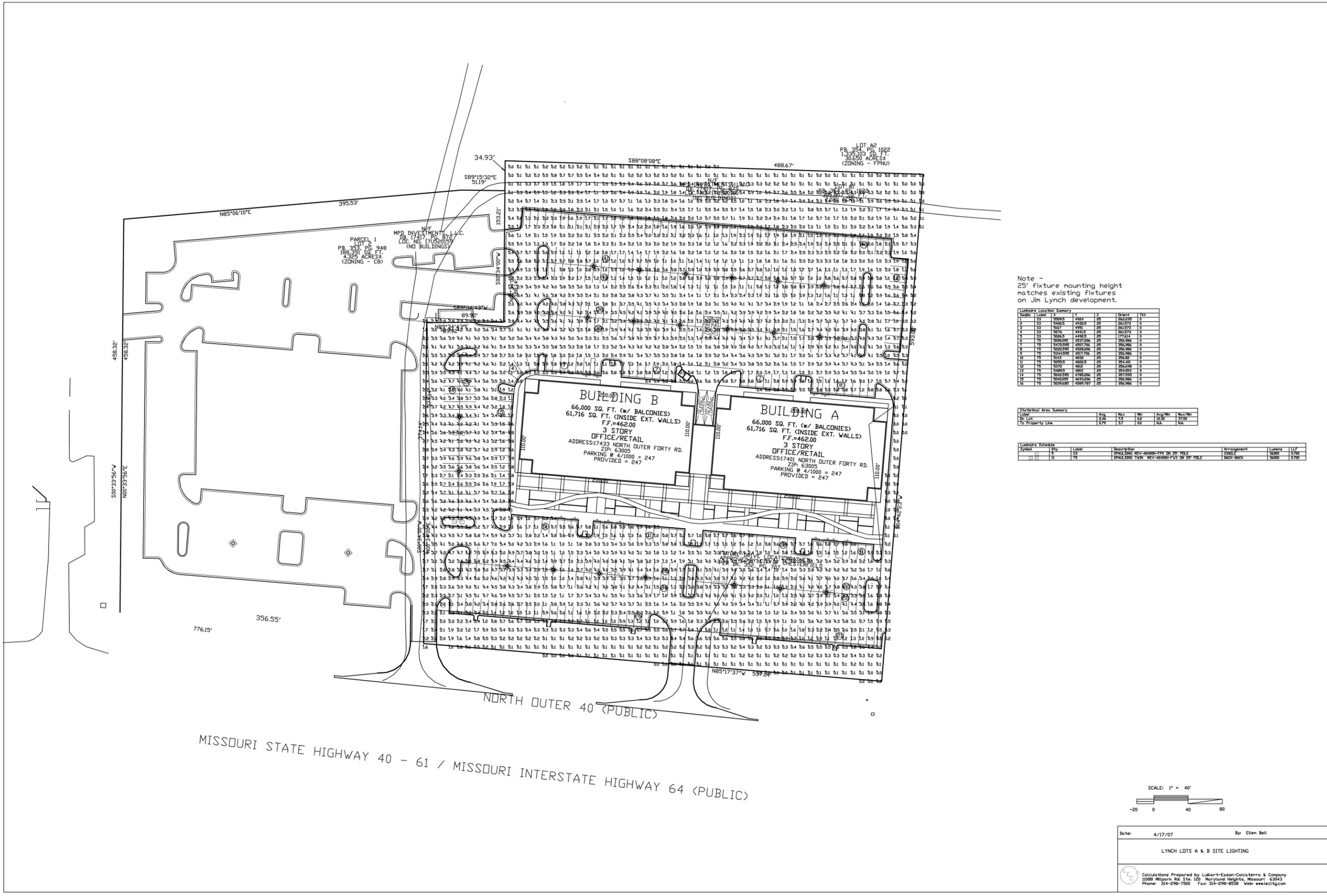
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Revisions:		
Date	Description	No.
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4-30-07	Site Revisions	3

Drawn: BD
Checked: DD

loomisAssociates
landscape architects/planners
707 Spirit-40 Park Drive, Suite 104
Chesterfield, Missouri 63005-104
E-mail: ltr@loomis.com

Sheet Title: **Landscape Plan**
Sheet No: **L-1**
Date: 3-13-07
Job #: 687.006

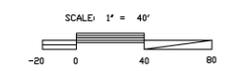


Note -
25' fixture mounting height
matches existing fixtures
on Jim Lynch development.

Symbol	Label	X	Y	Height	Orientation	YEL
1	54825	4461	28	25	262230	0
2	54825	4462	28	25	261570	0
3	54825	4463	28	25	261570	0
4	54825	4464	28	25	261570	0
5	54825	4465	28	25	177414	0
6	54825	4466	28	25	262966	0
7	54825	4467	28	25	262966	0
8	54825	4468	28	25	262966	0
9	54825	4469	28	25	262966	0
10	54825	4470	28	25	262966	0
11	54825	4471	28	25	261401	0
12	54825	4472	28	25	265500	0
13	54825	4473	28	25	261401	0
14	54825	4474	28	25	267595	0
15	54825	4475	28	25	262966	0
16	54825	4476	28	25	262966	0

Statistical Area Summary						
Lot	Area	Avg	Max	Min	Avg/Hr	Max/Hr
In Lot	2.46	7.5	13.3	15.30	2750	2750
To Property Line	0.79	3.7	8.8	N/A	N/A	N/A

Luminaire Schedule						
Symbol	Qty	Label	Description	Arrangement	Lumens	LFY
1	15	54	SPALLING TWIN REV-46000-FX IN BP PILE	BACK-TO-BACK	36000	5700
2	11	15	SPALLING TWIN REV-46000-FX IN BP PILE	BACK-TO-BACK	36000	5700



Date: 4/17/07 By: Ellen Bell

LYNCH LOTS A & B SITE LIGHTING

Calculations Prepared by Lukert-Enson-Calcaterra & Company
11088 Millpark Rd. Ste. 100 Maryland Heights, Missouri 63043
Phone: 314-298-7500 Fax: 314-298-8550 Web: www.lec.com



VII. D.

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

May 4, 2007

Planning Commission
City of Chesterfield
690 Chesterfield Parkway West
Chesterfield, MO 63017

The Planning Commission agenda for May 14, 2007 will include the following item for your consideration:

Re: **Mobil Mart (Clayton and Baxter):** Amended Site Development Plan, Landscape Plan, Lighting Plan and Architectural Elevations for a gas station/convenience store in a "PC" Planned Commercial District located on the Northwest Corner of Baxter and Clayton Roads

Dear Planning Commission:

Clayton Engineering, on behalf of Arch Energy, has submitted for your review the above-referenced project for a gas station/convenience store on the northwest corner of Baxter and Clayton Roads. The Department of Planning has reviewed this request and submits the following report:

BACKGROUND INFORMATION

- On June 4, 2001, the City of Chesterfield approved Ordinance 1750, which zoned the subject site from a "C8" Planned Commercial District to a "PC" Planned Commercial District. Said ordinance authorized the change of zoning with a filling station, including emergency towing and repair services, a fast food restaurant, and a vehicle washing facility.
- On October 15, 2001, Ordinance 1793 was approved. This provided for an increase in the square footage of the convenience store and in the height of the canopy.
- On December 3, 2001, the City approved Ordinance 1803, which provided the current standards of development of the site.
- Please note, each of the Ordinances was approved during discourse of litigation against the City by Arch Energy. A Site Development Plan was approved as an Exhibit with each.

SUBMITTAL INFORMATION

- The request involves a 3,925 square-foot convenience store/gas station with 1,920 square foot car wash on a 1.75 acre-parcel.

- Exterior materials are proposed to consist of split face block, brick veneer and E.I.F.S. Per the Architect's Statement of Design, "the materials are designed to keep the scale of the building more human and to add interest to the customer. The materials are non-maintenance providing a durable, long lasting appearance."
- The landscape plan utilizes a mixture of deciduous shade and ornamental trees and shrubs, evergreen trees and shrubs, perennial flowers and ornamental grass.
- A median will be constructed on Baxter Road by St. Louis County per direction to address ingress and egress concerns.

DEPARTMENTAL INPUT

Action is requested by the Planning Commission regarding approval of the Amended Site Development Plan, Landscape Plan, Lighting Plan and Architectural Elevations for Mobil Mart at Clayton and Baxter.

Respectfully Submitted,

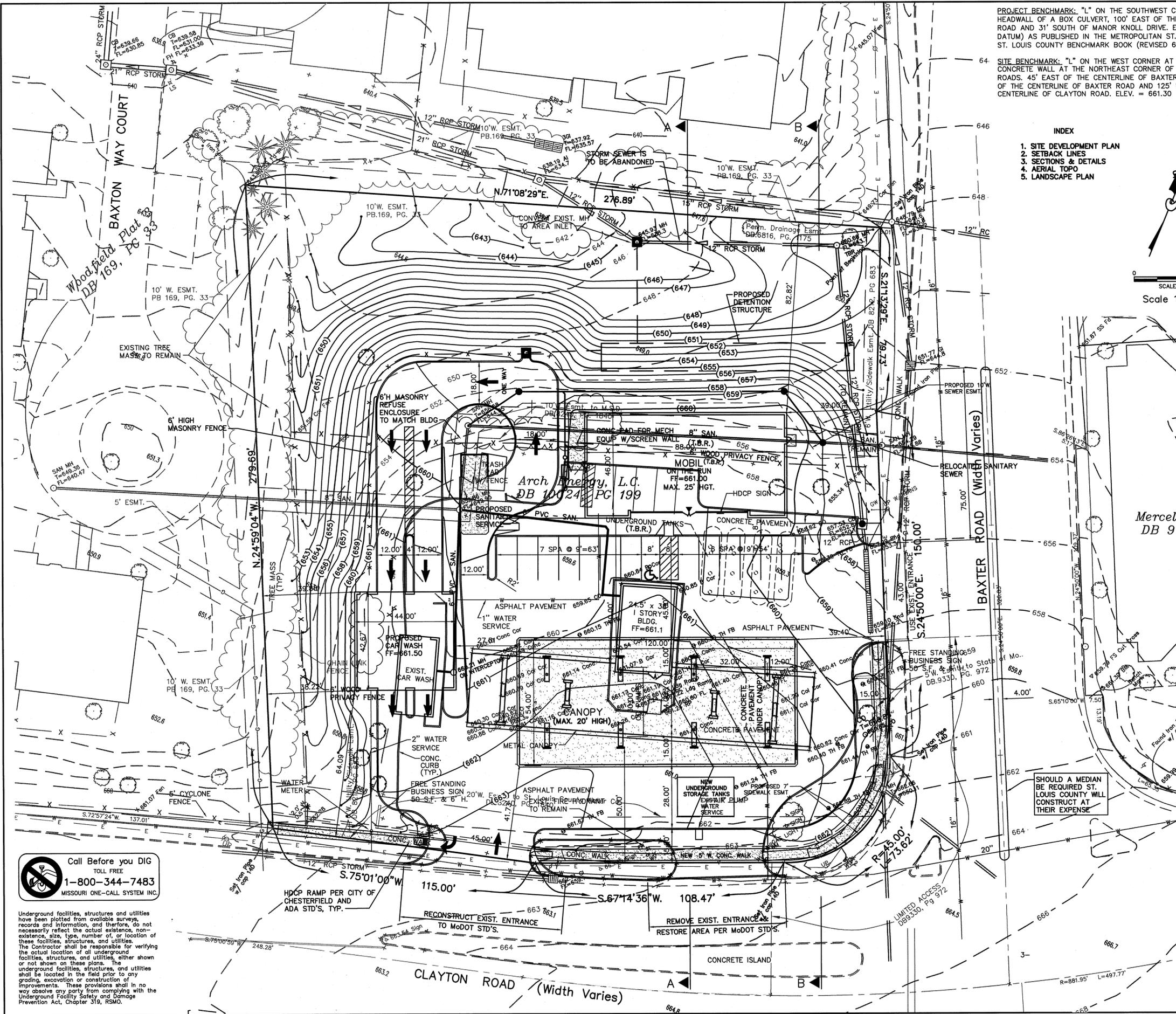
Annissa McCaskill-Clay

Annissa G. McCaskill-Clay, AICP
Assistant Director of Planning

Cc: City Administrator
City Attorney
Department of Public Works

Attachments:

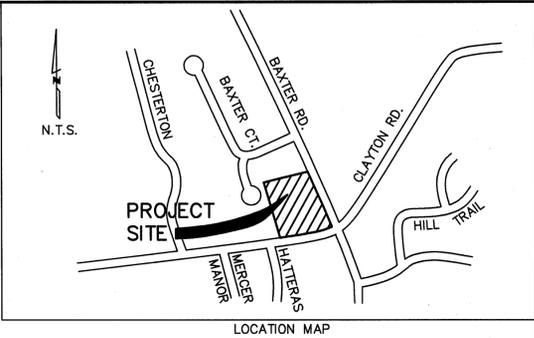
Amended Site Development Plan
Landscape Plan
Lighting Plan



PROJECT BENCHMARK: "L" ON THE SOUTHWEST CORNER OF THE SOUTH HEADWALL OF A BOX CULVERT, 100' EAST OF THE CENTERLINE OF BAXTER ROAD AND 31' SOUTH OF MANOR KNOLL DRIVE. ELEV. = 584.94 (USGS DATUM) AS PUBLISHED IN THE METROPOLITAN ST. LOUIS SEWER DISTRICT ST. LOUIS COUNTY BENCHMARK BOOK (REVISED 6/97) BM# 12-89.

64. SITE BENCHMARK: "L" ON THE WEST CORNER AT NORTH END OF 10' CONCRETE WALL AT THE NORTHEAST CORNER OF BAXTER AND CLAYTON ROADS. 45' EAST OF THE CENTERLINE OF BAXTER ROAD AND 125' NORTH OF THE CENTERLINE OF BAXTER ROAD AND 125' NORTH OF THE CENTERLINE OF CLAYTON ROAD. ELEV. = 661.30

- INDEX
1. SITE DEVELOPMENT PLAN
 2. SETBACK LINES
 3. SECTIONS & DETAILS
 4. AERIAL TOPO
 5. LANDSCAPE PLAN



SITE DATA:

1.) TOTAL AREA OF THIS SITE IS 1.75 ACRES	76,055 S.F.	
BUILDING AREA	3,925 S.F.	5%
(RESTAURANT PORTION SHALL NOT EXCEED	981 S.F.)	
CAR WASH	1,920 S.F.	2%
PAVEMENT AREA	32,662 S.F.	44%
GREEN AREA	37,465 S.F.	49%

- SITE DATA:**
- 1.) EXISTING ZONING IS C-8, PROPOSED ZONING IS PC PLANNED COMMERCIAL.
 - 2.) EXISTING USE IS MOBIL MART.
 - 3.) THE SITE IS SERVED BY THE FOLLOWING UTILITIES:
 LACLEDE GAS COMPANY
 METROPOLITAN ST. LOUIS SEWER DISTRICT (MSD)
 ST. LOUIS COUNTY WATER COMPANY
 SOUTHWESTERN BELL TELEPHONE
 UNION ELECTRIC COMPANY
 - 4.) EXISTING SITE LOCATOR NUMBER IS 21R41-0960.
 - 5.) SLOPES SHALL NOT EXCEED 3 HORIZONTAL TO 1 VERTICAL.
 - 6.) DRAINAGE NOTES:
 A. SITE GRADING AND DRAINAGE PER CITY OF CHESTERFIELD STANDARDS.
 B. ALL STORMWATER SHALL BE DISCHARGED AT AN ADEQUATE NATURAL DISCHARGE POINT. SINK HOLES ARE NOT ADEQUATE NATURAL DISCHARGE POINTS.
 C. SITE DRAINAGE SHALL CONFORM TO THE MISSOURI HIGHWAYS AND TRANSPORTATION DEPARTMENT, THE METROPOLITAN ST. LOUIS SEWER DISTRICT AND CITY OF CHESTERFIELD STANDARDS.
 - 7.) NO STEPS AT HANDICAPPED ACCESSIBLE DOORS.
 - 8.) ALL SIDEWALKS TO BE CONSTRUCTED TO CITY OF CHESTERFIELD AND ADA STANDARDS.
 - 9.) ALL PROPOSED IMPROVEMENTS SHALL BE IN CONFORMANCE WITH THE CITY OF CHESTERFIELD AND MSD STANDARDS.
 - 10.) TO SURVEYORS KNOWLEDGE EXISTING AND PROPOSED EASEMENTS ARE SHOWN ON THIS PLAN.
 - 11.) NO ONSITE LIGHT STANDARD SHALL EXCEED SIXTEEN (16) FEET IN HEIGHT AND BE SITUATED THAT LIGHT IS CAST DIRECTLY ON ADJOINING PROPERTIES OR PUBLIC ROADWAYS. LIGHT STANDARDS NORTH AND WEST OF THE NORTH AND WEST BUILDING SHALL NOT EXCEED EIGHT (8) FEET IN HEIGHT AND SHALL BE A SHOEBOX DESIGN. THE LIGHT STANDARD AT THE WEST ENTRANCE ON CLAYTON ROAD SHALL NOT EXCEED SIXTEEN (16) FEET IN HEIGHT AND SHALL BE A BOX DESIGN.

PARKING CALCULATIONS

C-STORE

3- 1/3 per 1,000 S.F.	14 Spaces Required
4,132 S.F. =	14 Spaces Provided

THIS PLAN IS A CORRECT REPRESENTATION OF ALL EXISTING AND PROPOSED LAND DIVISIONS.



The signed and sealed original of this drawing is on file at the offices of The Clayton Engineering Company, Inc. The signed and sealed original is the official document and shall take precedence over any digital version.

NO.	DATE	BY	DESCRIPTION
4	04/10/07	SLH	ADDED MEDIAN
3	03/07/07	JAK	ADDED NOTE ABOUT MEDIAN
2	5/25/05	DSB	REVISIONS PER CITY OF CHESTERFIELD COMMENTS
1	3/14/05	MRW	REVISED CAR WASH ENTRANCE

SITE DEVELOPMENT PLAN
MOBIL "ON THE RUN"

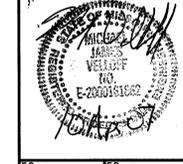
14905 Clayton Rd. Chesterfield, MO 63017
Prepared for:

ARCH ENERGY
106 East Washington
Cuba, MO. 65453
(573) 885-2277

Call Before you DIG
TOLL FREE
1-800-344-7483
MISSOURI ONE-CALL SYSTEM INC.

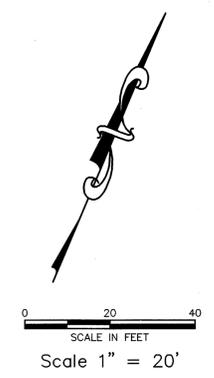
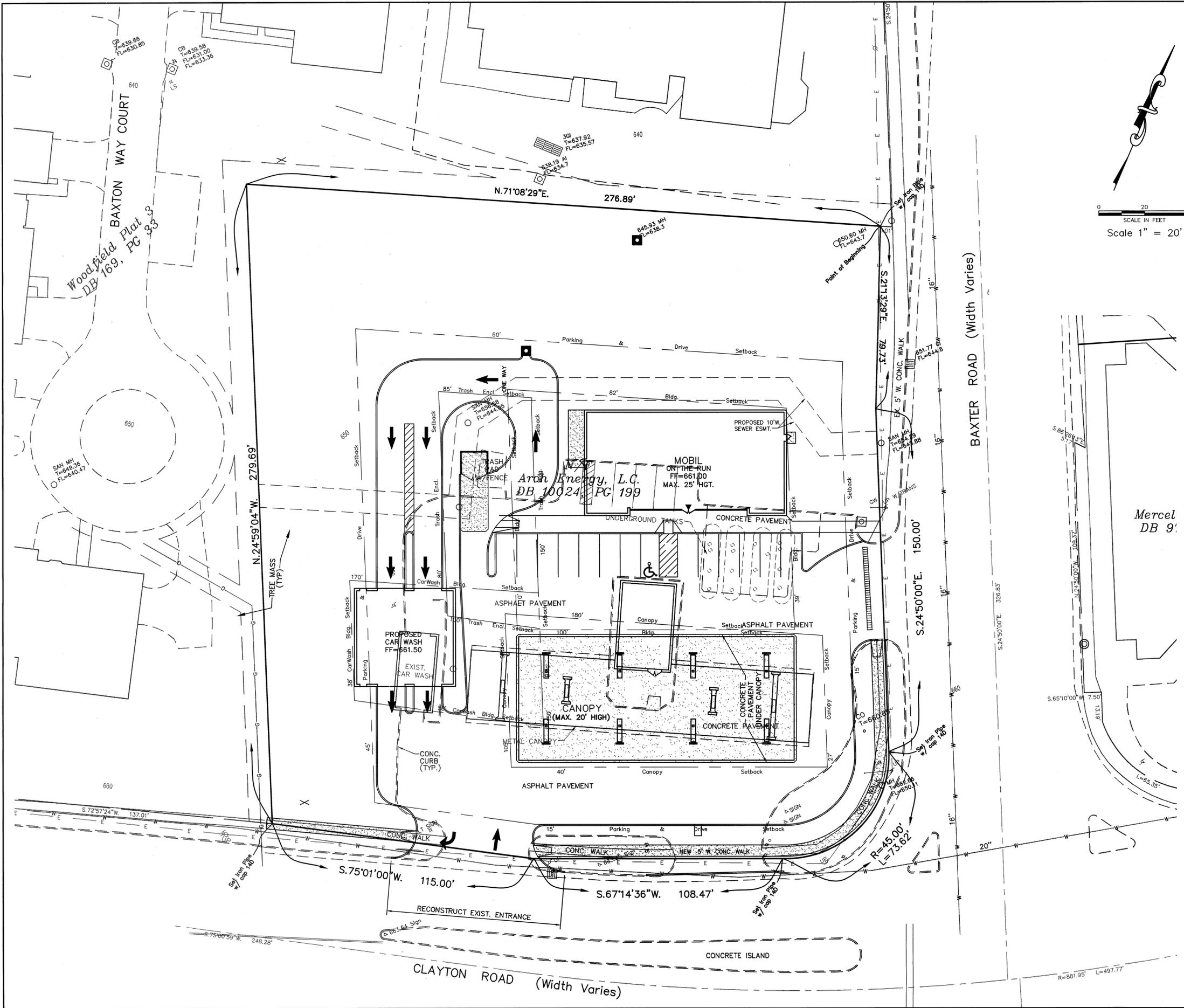
Underground facilities, structures and utilities have been plotted from available surveys, records and information, and therefore, do not necessarily reflect the actual existence, non-existence, size, type, number or location of these facilities, structures, and utilities. The Contractor shall be responsible for verifying the actual location of all underground facilities, structures, and utilities, either shown or not shown on these plans. The underground facilities, structures, and utilities shall be located in the field prior to any grading, excavation or construction of improvements. These provisions shall in no way absolve any party from complying with the Underground Facility Safety and Damage Prevention Act, Chapter 319, RSMO.

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the clayton engineering company, inc.
ENGINEERS • SURVEYORS • PLANNERS
11920 WESTLINE INDUSTRIAL DRIVE
ST. LOUIS, MISSOURI 63146
(314) 692-8888 FAX: (314) 692-8888
clayton-engineering.com

Designed	SWQ
Drawn	HRL, MRW
Checked	SWQ
Date	03/10/05
Project Number	97318
Sheet Number	1 of 5



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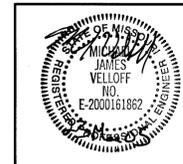
NO.	DATE	BY	REVISIONS
1	3/14/05	MRW	REVISED CAR WASH ENTRANCE

SETBACK LINES
MOBIL "ON THE RUN"

14905 Clayton Rd. Chesterfield, MO 63017
Prepared for:

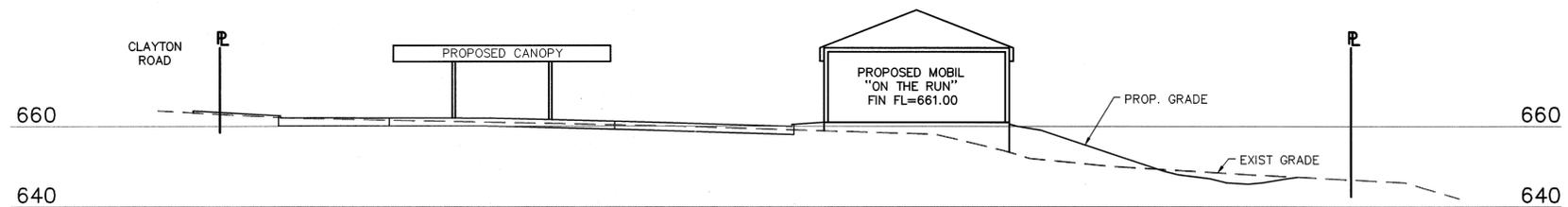
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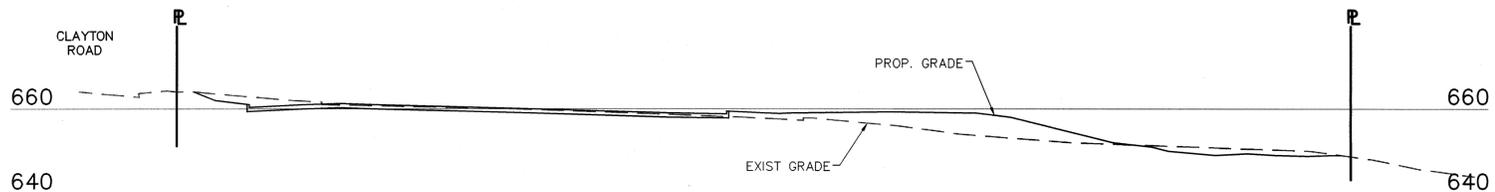


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clayton-engineering.com

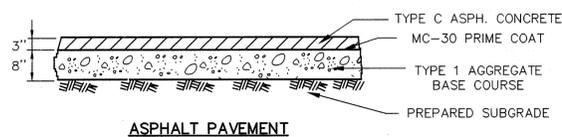
Designed	SWQ
Drawn	HRW, MRW
Checked	SWQ
Date	03/10/05
Project Number	97318
Sheet Number	2 of 5



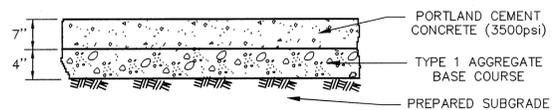
SECTION A-A



SECTION B-B
SCALE: 1"=20' HORIZ. & VERT.



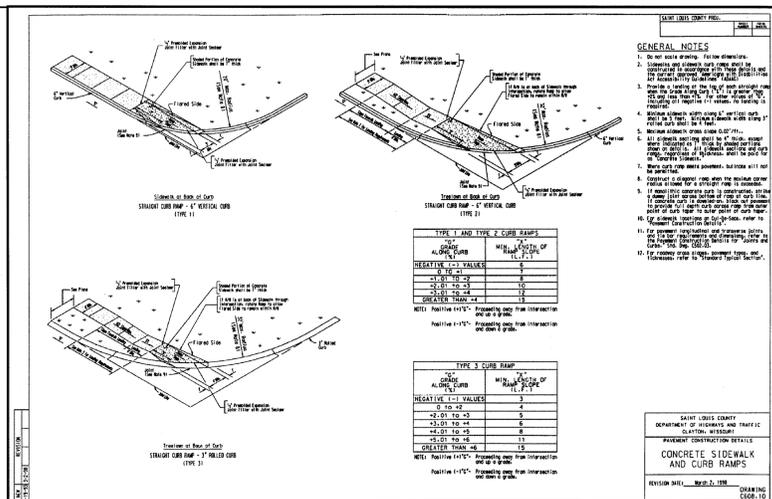
ASPHALT PAVEMENT



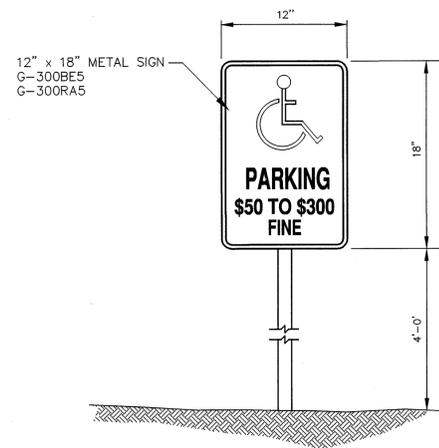
CONCRETE

PAVEMENT DETAILS

N.T.S.

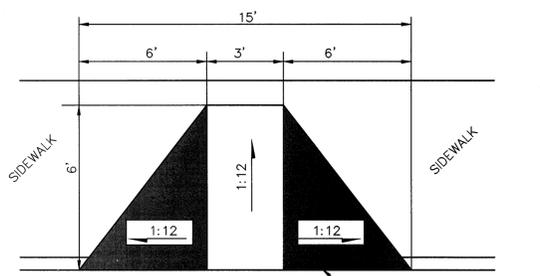


GENERAL NOTES:
 1. Do not use existing curb structures.
 2. Subgrade and concrete curb shall be constructed to meet the minimum 1% slope for drainage.
 3. Provide a 1/4" slope for the 1/2" curb height and 1/2" curb width. The curb shall be 1/2" high and 1/2" wide.
 4. Provide a 1/4" slope for the 1/2" curb height and 1/2" curb width. The curb shall be 1/2" high and 1/2" wide.
 5. Provide a 1/4" slope for the 1/2" curb height and 1/2" curb width. The curb shall be 1/2" high and 1/2" wide.
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 9. Provide a 1/4" slope for the 1/2" curb height and 1/2" curb width. The curb shall be 1/2" high and 1/2" wide.
 10. Provide a 1/4" slope for the 1/2" curb height and 1/2" curb width. The curb shall be 1/2" high and 1/2" wide.



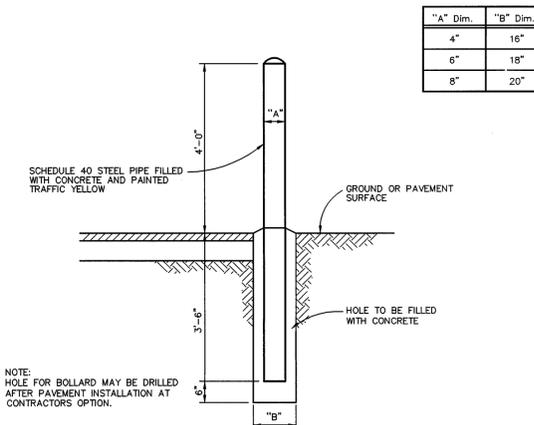
HANDICAPPED SIGN

N.T.S.



Typ. Handicap Ramp Plan
On Site

AREA TO BE PAINTED "SAFETY BLUE", OR APPROVED EQUAL (TYPICAL)



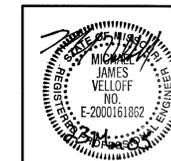
PIPE BOLLARD

REVISIONS	

SECTIONS & DETAILS
MOBIL "ON THE RUN"

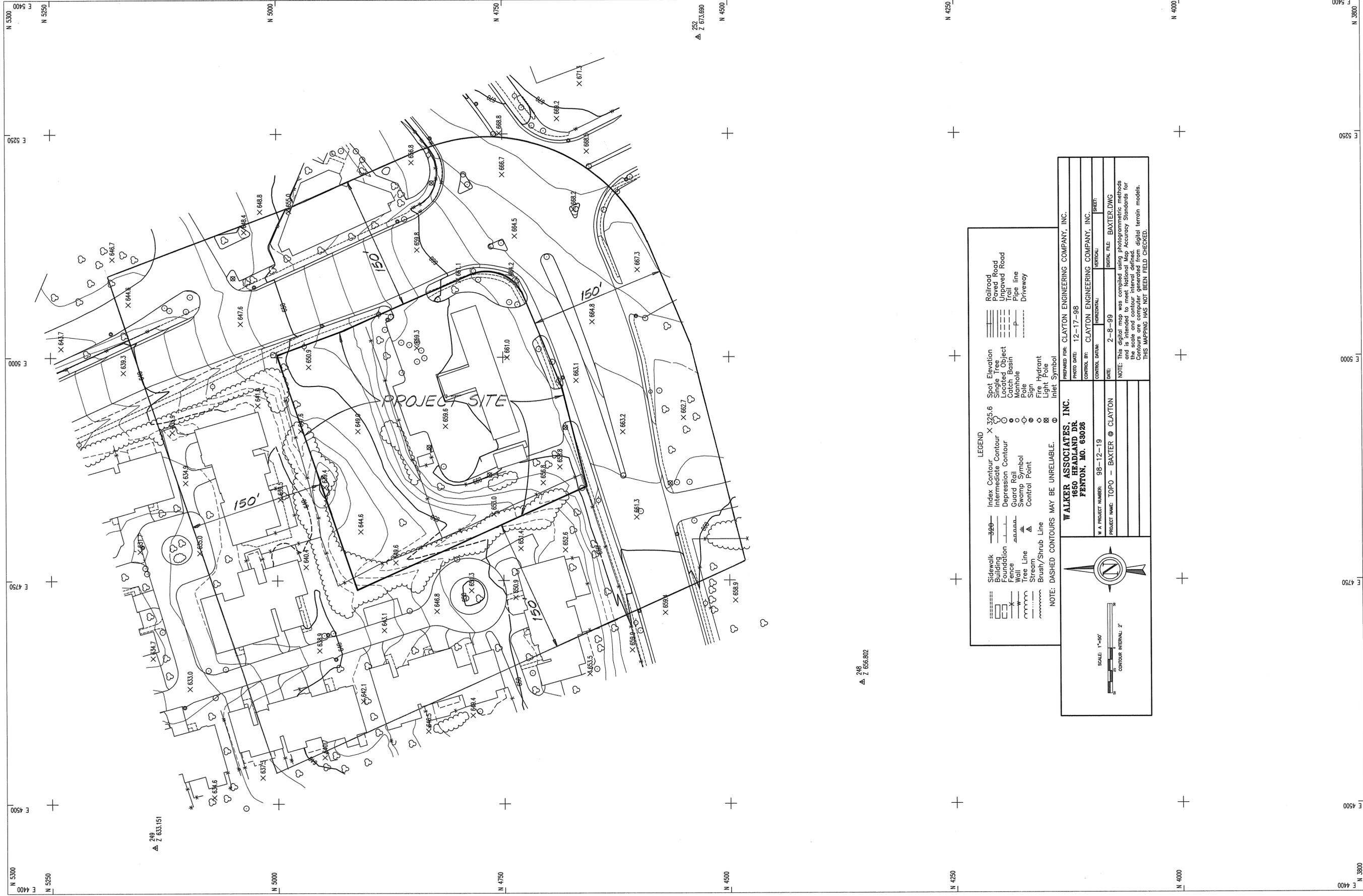
14905 Clayton Road, Chesterfield, MO 63017

Prepared for:
ARCH ENERGY
106 East Washington
Cuba, Mo. 65453
(573) 885-2277



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clayton-engineering.com

Designed	SWQ
Drawn	HRL, MRW
Checked	SWQ
Date	03/10/05
Project Number	97318
Sheet Number	3 of 5



248
Z 633.151

252
Z 673.690

248
Z 656.802

LEGEND

=====	Sidewalk	-----	Index Contour	X 325.6	Spot Elevation		Railroad
	Building	-----	Intermediate Contour	○	Single Tree		Paved Road
	Foundation	-----	Depression Contour	○	Cluster		Unpaved Road
	Fence	-----	Guard Rail	○	Canals		Trail
	Wall	-----	Swamp Symbol	○	Manhole		Pipe line
	Tree Line	-----	Control Point	○	Sign		Driveway
	Stream	-----	NOTE: DASHED CONTOURS MAY BE UNRELIABLE.	○	Fire Hydrant		
	Brush/Shrub Line	-----		○	Light Pole		
		-----		○	Inlet Symbol		

SCALE 1"=50'

CONTOUR INTERVAL 2'

WALKER ASSOCIATES, INC.
 1650 HEADLAND DR
 PENTON, MO. 63026

PREPARED FOR: CLAYTON ENGINEERING COMPANY, INC.

PHOTO DATE: 12-17-98

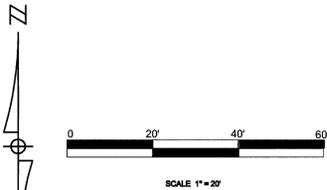
CONTROL BY: CLAYTON ENGINEERING COMPANY, INC.

DATE: 2-8-99

SHEET: BAXTER.DWG

PROJECT NAME: TOPO - BAXTER @ CLAYTON

NOTE: This digital map was compiled using photogrammetric methods and is intended to meet National Map Accuracy Standards for contour mapping. Contour elevations were derived from digital terrain models. THIS MAPPING HAS NOT BEEN FIELD CHECKED.



ABBREVIATIONS

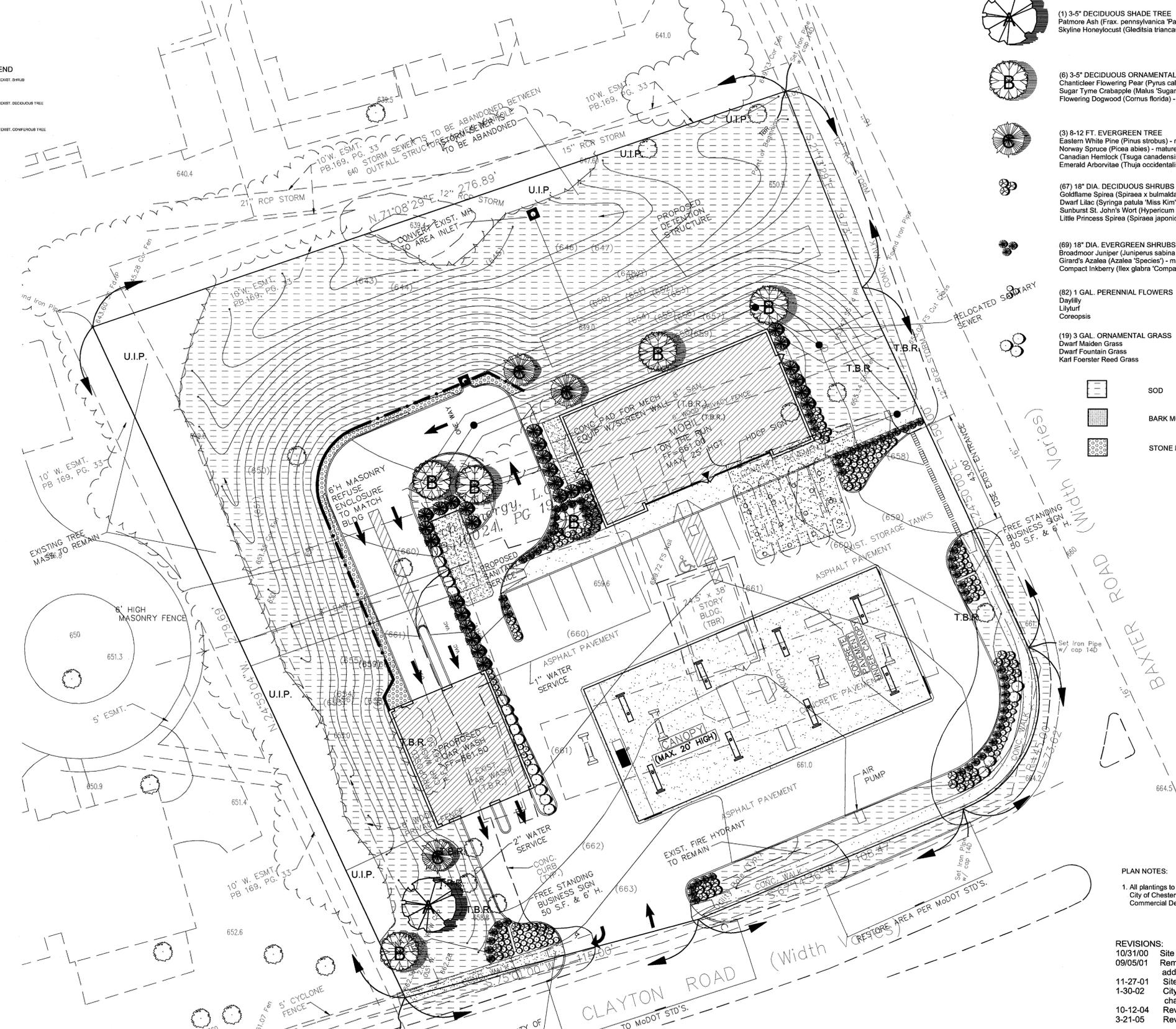
- CL - Centerline
- C.O. - Cleanout
- D.S. - Downspout
- E.P. - Edge Of Pavement
- F.F. - Finished Floor
- F.G. - Finished Grade
- FL - Flowline
- GL - Gutterline
- L.S. - Light Standard
- (T.B.R.) - To Be Removed
- T.O.C. - Top Of Curb
- T.P. - Top Of Pavement
- T.O.W. - Top Of Wall
- C.M. - Concrete Monument
- T.W. - Top Of Walk
- PL - Property Line
- A.I. - Area Inlet
- R.W. - Right Of Way
- G.I. - Gate Inlet
- M.H. - Manhole
- O.I.P. - Old Iron Pipe
- R. - Riser
- P.B. - Plat Book
- D.B. - Dead Block
- T.B.C. - Top Of Block
- T.S. - Top Of Slope
- C.M.P. - Corrugated Metal Pipe
- P.V.C. - Polyvinyl Chloride Pipe
- V.C.P. - Ventilated Clay Pipe
- C.P. - Non-Reinforced Concrete Pipe
- R.C.P. - Reinforced Concrete Pipe
- D.I.P. - Ductile Iron Pipe
- FF - Face to Face
- BB - Back to Back
- (U.I.P.) - Use In Place
- B.W. - Bottom Of Wall
- T.I. - Top Of Island
- T.G. - Top Of Grade
- (Typ.) - Typical
- Conc. - Concrete
- RE - Refer
- DI - Detail

LEGEND

- EXIST. SHRUB
- EXIST. DECIDUOUS TREE
- EXIST. EVERGREEN TREE

LANDSCAPE LEGEND

- (1) 3-5' DECIDUOUS SHADE TREE
Palmrose Ash (*Frax. pennsylvanica* 'Palmrose') - mature height 45'+
Skyline Honeylocust (*Gleditsia triacanthos* 'Skyline') - mature height 45'+
- (6) 3-5' DECIDUOUS ORNAMENTAL TREE
Chanticleer Flowering Pear (*Pyrus calleryana* 'Chanticleer') - mature height 40'
Sugar Tyme Crabapple (*Malus Sugar Tyme*) - mature height 18'
Flowering Dogwood (*Comus florida*) - mature height 15-25'
- (3) 8-12 FT. EVERGREEN TREE
Eastern White Pine (*Pinus strobus*) - mature height 50'
Norway Spruce (*Picea abies*) - mature height 60'
Canadian Hemlock (*Tsuga canadensis*) - mature height 45'+
Emerald Arborvitae (*Thuja occidentalis* 'Emerald') - mature height 15'
- (67) 18" DIA. DECIDUOUS SHRUBS
Goldflame Spirea (*Spiraea x buimaida* 'Goldflame') - mature height 3'
Dwarf Lilac (*Syringa patula* 'Miss Kim') - mature height 3'
Sunburst St. John's Wort (*Hypericum frondosum* 'Sunburst') - mature height 3'
Little Princess Spirea (*Spiraea japonica* 'Little Princess') - mature height 3'
- (69) 18" DIA. EVERGREEN SHRUBS
Broadmoor Juniper (*Juniperus sabina* 'Broadmoor') - mature height 2'
Giards Azalea (*Azalea* 'Species') - mature height 3'
Compact Intberry (*Ilex glabra* 'Compacta') - mature height 4'
- (82) 1 GAL. PERENNIAL FLOWERS
Daylily
Lilylurf
Coreopsis
- (19) 3 GAL. ORNAMENTAL GRASS
Dwarf Maiden Grass
Dwarf Fountain Grass
Karl Foerster Reed Grass
- SOD
- BARK MULCH
- STONE MULCH



PLAN NOTES:
1. All plantings to be done according to the City of Chesterfield Landscape Guidelines for Commercial Development.

REVISIONS:
10/31/00 Site plan changes, add pines
09/05/01 Remove detention and wall, size changes add site proof fence
11-27-01 Site plan changes, add trees
1-30-02 City comments, increase tree sizes, change sign note
10-12-04 Revised Site Plan per Civil
3-21-05 Revised Site Plan per Civil

A record original of this document is on file at the office of LAND DESIGN SERVICES, Inc. Any unauthorized alterations or changes made without the expressed consent from LAND DESIGN SERVICES, Inc. shall deem us not responsible for any said alterations and changes.

Not for Construction



LDS, Inc.
Land Design Services, Inc.

Land Planning
Civil Engineering
Landscape Architecture

MAIN OFFICE:
41 First Missouri Center, Suite 218
12 Louis, Missouri 63141
Phone: (314) 34-1888
Fax: (314) 576-1281

PREPARED FOR:
Arch Energy
106 East Washington
Cuba, MO 65453

MOBIL ON THE RUN
14905 Clayton Road, Chesterfield, MO

File: C:\00proj\00023\PL1-032105 OptionC

Sheet No.
PL-1

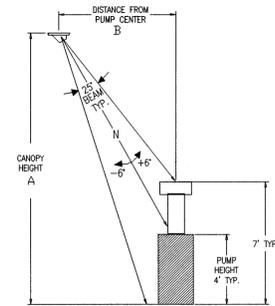
Date: 7/18/00
Ckd. By: RPW

Project No: 00023



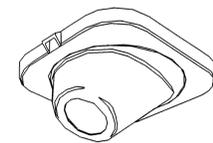
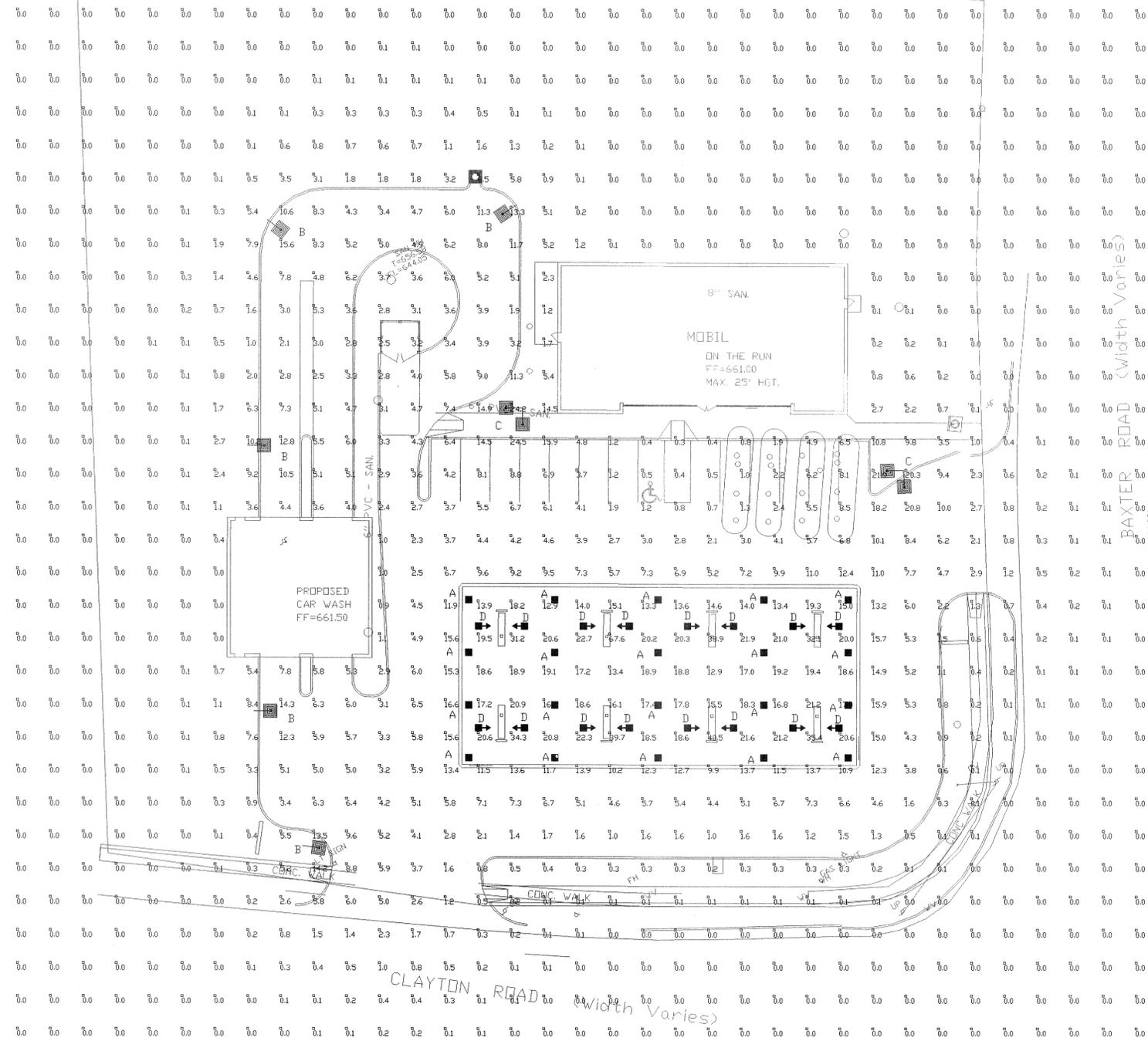
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PRELIMINARY LANDSCAPE PLAN



LAMP LAYOUT FOR 25° FLOOD

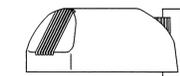
A	B								
VERTICAL HEIGHT OF CANOPY	HORIZONTAL DISTANCE FROM CENTER OF DISPENSER TO CENTER OF FIXTURE								
	4'8"	5'4"	6'	6'8"	7'4"	8'	8'8"	9'4"	10'
16'	-6	-6	N	N	N	N	N	+6	+6
15 1/2'	-6	-6	N	N	N	N	N	+6	+6
15'	-6	-6	N	N	N	N	N	+6	+6
14 1/2'	-6	-6	N	N	N	N	N	+6	+6
14'	-6	-6	N	N	N	N	N	+6	+6



FOCUS



FLAT LENS ENCORE



CHALLENGER

LAYOUT PREPARED TO ACHIEVE A MAX AVG OF 22 UNDER CANOPY

Numeric Summary							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
DISPENSER	Illuminance	Fc	39.72	63.9	22.3	1.78	2.87

Statistical Area Summary					
Label	Avg	Max	Min	Avg/Min	Max/Min
CANOPY	19.43	67.6	9.9	1.96	6.83
INSIDE CURB	5.92	20.8	0.4	14.80	52.00

MAINTAINED FOOTCANDLE VALUES AT GRADE, USING A .72 MAINTENANCE FACTOR.

Luminaire Schedule							
Symbol	Qty	Label	Arrangement	Lumens	LLF	Description	Total Watts
■	20	A	SINGLE	16000	0.720	ECTA-S-175-PSMV-F	215
■	5	B	SINGLE	32000	0.720	CHH-FT-320-PSMV-F 16' POLE SINGLE	370
■	2	C	2 @ 90 DEGREES	32000	0.720	CHH-FT-320-PSMV-F 16' POLE D90	740
■	16	D	SINGLE	6000	0.720	ECTA-SP-100-P-38-FD	129

Based on the information provided, all dimensions and luminaire locations shown represent recommended positions. The engineer and/or architect must determine applicability of the layout to existing or future field conditions.

This lighting pattern represents illumination levels calculated from laboratory data taken under controlled conditions utilizing current industry standard lamp ratings in accordance with Illuminating Engineering Society approved methods. Actual performance of any manufacturer's luminaire may vary due to variation in electrical voltage, tolerance in lamps and other variable field conditions.

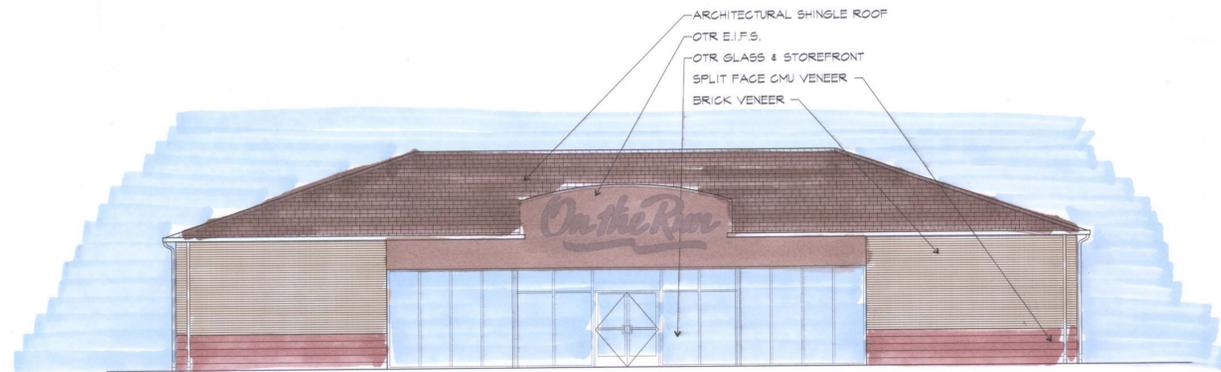
Rev.	Date	By
Revisions		

Lei INDUSTRIES™
1000 ALABAMA DR. P.O. BOX 4728 CHESTERFIELD, MO 63024
 (314) 733-2200 • FAX (314) 733-4023

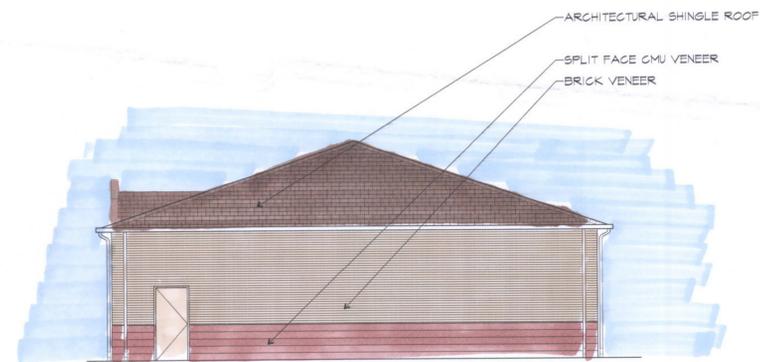
LIGHTING PROPOSAL FOR
 MOBIL (CLAYTON & BAXTER)
 CHESTERFIELD, MO

SCALE: 1"=20'
 DATE: 12-14-05
 BY: BDB

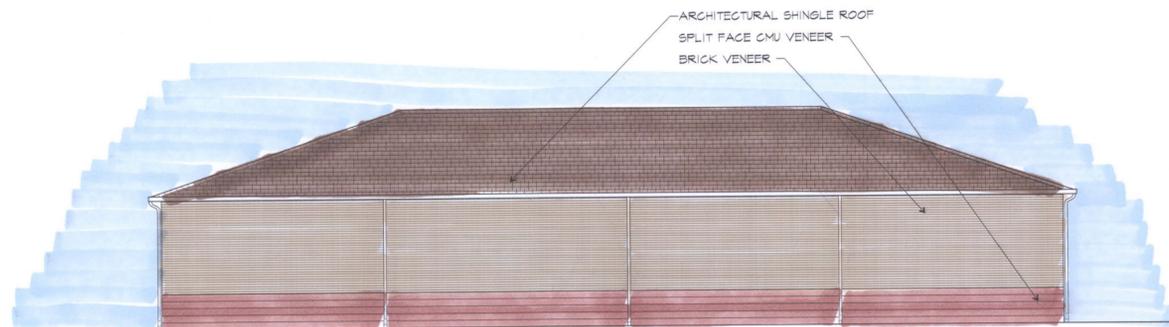
LO-73897B-1
 SHEET 1 OF 1



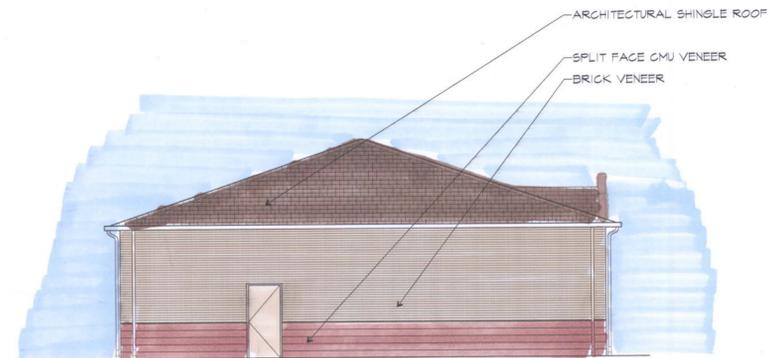
SOUTH ELEVATION - CLAYTON
SCALE: 1/8" = 1'-0"



EAST ELEVATION - BAXTER
SCALE: 1/8" = 1'-0"



NORTH ELEVATION
SCALE: 1/8" = 1'-0"

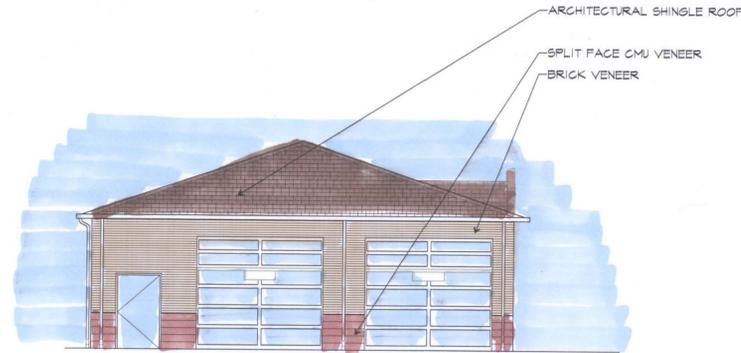


WEST ELEVATION
SCALE: 1/8" = 1'-0"

BAXTER & CLAYTON ON THE RUN

CONVENIENCE STORE





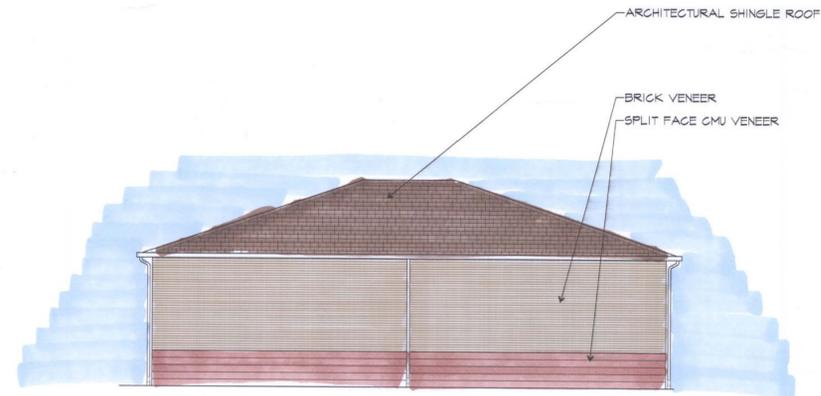
SOUTH ELEVATION - CLAYTON
SCALE: 1/8" = 1'-0"



EAST ELEVATION - BAXTER
SCALE: 1/8" = 1'-0"



NORTH ELEVATION
SCALE: 1/8" = 1'-0"



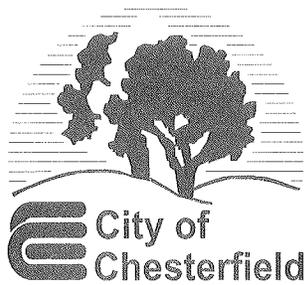
WEST ELEVATION
SCALE: 1/8" = 1'-0"

BAXTER & CLAYTON ON THE RUN

CAR WASH



1001 CRAIG ROAD
SUITE 280
ST. LOUIS, MISSOURI 63146
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FAX (314) 236-5046
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12-30-05



VII. E.

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May 9, 2007

Planning Commission
City of Chesterfield
690 Chesterfield Parkway West
Chesterfield, MO 63017

The Planning Commission agenda for **May 14, 2007** will include the following item for your consideration:

Pfizer (Monsanto): A Site Development Concept Plan for a 200.51 acre lot of land zoned “C8” and “FPC8” Planned Commercial District located at the Northeast corner of 700 Chesterfield Parkway West.

Dear Planning Commission:

Flad and Associates and Picket Ray and Silver, on behalf of Pharmacia Corp., have submitted a Site Development Concept Plan. The Department of Planning has reviewed this submittal and submits the following report.

BACKGROUND

1. Development of the tract was originally authorized by St. Louis County Ordinance Number 9002 in 1979.
2. It was subsequently amended by Ordinance Number 10,986 in 1983.

SUBMITTAL INFORMATION

1. The request is for the location of the proposed PGM Biopharma Building and the future expansion of building “JJ”.
2. Additional changes include the location of a proposed backup generator yard and amendments to existing temporary construction parking lots and access roads.

DEPARTMENTAL INPUT

The submittal was reviewed for compliance with all City of Chesterfield ordinances. The Department of Planning requests action on the Site Development Concept Plan.

Respectfully submitted,



Charlie Campo
Project Planner

Cc: City Administrator
City Attorney
Department of Public Works

Attachments: Site Development Concept Plan

Respectfully submitted,



Mara M. Perry, AICP
Senior Planner of Plan Review

DESCRIPTION
MONSANTO BOUNDARY
200.51 ACRES

Adjusted Tract One of Boundary Adjustment Tract of "tract of land in U.S. Surveys 123 and 154 and part of Lots 5, 6, 7, and 8 of the subdivision of Hugo Essen Farm, Township 45 North, Range 4 East, St. Louis County, Missouri," according to the plat thereof recorded in Plat Book 317, Page 45 of the St. Louis County, Missouri Records excepting therefrom that part subdivided and now known as "Chesterfield Government Center" according to the plat recorded in Plat Book 348, Page 135 of the St. Louis County, Missouri Records and being more particularly described as follows:

BEGINNING at the intersection of the western line of West Drive (30' wide) and the southern line of the Chicago Rock Island and Pacific Railroad; thence along the western of said West Drive the following courses and distances: South 21'38'46" East a distance of 362.24 feet to a point; thence South 34'51'50" East a distance of 200.22 feet to a point; thence South 42'05'20" East a distance of 115.05 feet to a point; thence South 29'22'36" East a distance of 156.07 feet to a point; thence South 75'29'44" East a distance of 76.79 feet to a point; thence North 57'12'19" East a distance of 106.10 feet to a point; thence South 58'52'26" East a distance of 105.84 feet to a point; thence South 58'52'26" East a distance of 392.52 feet to a point; thence South 27'38'56" East a distance of 175.15 feet to a point; thence South 17'15'51" East a distance of 175.17 feet to a point; thence South 30'03'05" East a distance of 2.60 feet to a point in the northwest line of the aforementioned Lot 6 of Hugo Essen Farm, said point also being along the northwestern line of a tract now or formerly of Rosalie Bergh as recorded in Deed Book 9414, Page 715 of the St. Louis County, Missouri Recorder's Office; thence leaving the western line of said West Drive along the northwestern line of said Bergh tract South 56'25'23" West a distance of 592.52 feet to a point on the southwestern line of said Lot 6, said point also being the southwestern line of said Bergh tract; thence leaving the northwestern line of said Lot 6 along the southwestern line of said Lot 6 South 33'14'36" East a distance of 423.50 feet to a point in the southeastern line of a tract of land now or formerly of Christine Porzeinski as recorded in Deed Book 8474, Page 1082 of the St. Louis County, Missouri Records; thence leaving the southwestern line of said Lot 6 along the southeastern line of said Porzeinski tract North 56'15'38" East a distance of 200.00 feet to a point being the northwest corner of Adjusted Tract Two as recorded in Plat Book 317, Page 45 of the St. Louis County, Missouri Records; thence leaving the southeastern line of said Porzeinski tract along the southwestern line of Adjusted Tract Two South 33'44'22" East a distance of 368.04 feet to a point; thence North 56'15'38" East a distance of 405.30 feet to a point in the western line of the aforementioned West Drive (30' wide); thence along the western line of said West Drive South 23'32'37" East a distance of 366.94 feet to a point; thence South 26'58'15" East a distance of 170.31 feet to a point on the northern line of Wellesley Place Addition Amended as recorded in Plat Book 319, Page 4 of the St. Louis County, Missouri Recorder's Office; thence along the northern line of said Wellesley Place South 63'12'06" West a distance of 268.84 feet to a point; thence South 26'10'01" East a distance of 117.92 feet to a point on the northern line of Wellesley Place as recorded in Plat as recorded in Plat Book 310, Page 85 of the St. Louis County, Missouri Recorder's Office; thence along the northern line of said Wellesley Place South 68'28'50" West a distance of 123.10 feet to a point; thence North 80'25'10" West a distance of 543.80 feet to a point; thence South 66'24'56" West a distance of 299.96 feet to a point on the southwestern line of said

Wellesley Place; thence along the southwestern line of said Wellesley Place South 38'20'34" East a distance of 245.37 feet to a point on the northern line of a tract of land now or formerly of Apple Sweets as recorded in Deed Book 13114, Page 1172 of the St. Louis County, Missouri Recorder's Office; thence along the northern line of said Apple Sweets tract South 51'39'26" West a distance of 456.05 feet to a point in the northern line of Chesterfield Village Parkway (80' wide); thence leaving the northern line of Apple Sweets tract along the northern line of said Chesterfield Village Parkway the following courses and distances: along a curve to the left having a radius of 994.93 feet, an arc length of 541.79 feet, a chord of which bears North 53'56'35" West a chord distance of 535.12 feet to a point; thence North 69'32'36" West a distance of 1010.75 feet to a point; thence along a curve to the left having a radius of 994.93 feet, an arc length of 763.05 feet, a chord of which bears South 88'29'06" West a chord distance of 744.49 feet to a point, said point being the intersection of the northern line of said Chesterfield Village Parkway and the northeastern line of City Center Drive (variable width) as shown on Lot 1 of the Chesterfield Government Center as recorded in Plat Book 348, Page 135 of the St. Louis County, Missouri Records; thence leaving the northern line of Chesterfield Village Parkway along the northeastern line of said City Center Drive North 45'25'51" West a distance of 194.57 feet to a point; thence North 52'04'00" West a distance of 98.32 feet to a point; thence North 14'25'18" East a distance of 14.97 feet to a point; thence North 76'24'38" West a distance of 20.00 feet to a point; thence North 81'57'01" West a distance of 161.04 feet to a point; thence North 54'43'29" West a distance of 88.49 feet to a point; thence along a curve to the left having a radius of 101.50 feet, an arc length of 134.48 feet, a chord of which bears North 44'37'52" West a chord distance of 124.86 feet to a point; thence South 04'35'19" West a distance of 25.04 feet to a point; thence along a curve to the left having a radius of 76.50 feet, an arc length of 159.90 feet, a chord of which bears South 38'27'25" West a chord distance of 132.34 feet to a point; thence along a curve to the right having a radius of 212.50 feet, an arc length of 59.40 feet to a point; thence South 53'59'15" East a chord distance of 59.40 feet to a point; thence South 62'01'22" West a distance of 152.80 feet to a point; thence along a curve to the left having a radius of 245.00 feet, an arc length of 101.83 feet, a chord of which bears South 50'06'57" West a chord distance of 101.10 feet to a point; thence South 38'12'32" West a distance of 84.52 feet to a point; thence along a curve to the right having a radius of 54.00 feet, an arc length of 76.45 feet, a chord of which bears South 78'46'05" West a chord distance of 70.23 feet to a point in the northern line of Swingley Ridge Road (49' wide); thence along the northern line of said Swingley Ridge Road along a curve to the left having a radius of 424.50 feet, an arc length of 119.91 feet, a chord of which bears North 68'45'52" West a chord distance of 119.51 feet to a point; thence North 76'50'46" West a distance of 134.59 feet to a point on the eastern line of a tract of land now or formerly of Silohot Hotels, L.P. as recorded in Deed Book 15619, Page 1280 of the St. Louis County, Missouri Recorder's Office; thence leaving the northern line of Swingley Ridge Road along the eastern line of said Silohot Hotels, L.P. tract North 13'30'39" West a distance of 1287.29 feet to a point in the southern line of the aforementioned Chicago Rock Island and Pacific Railroad; thence along the southern line of said Chicago Rock Island and Pacific Railroad North 68'20'48" East a distance of 3535.49 feet to the POINT OF BEGINNING and containing 8,734,374 square feet or 200.51 acres more or less.

PICKETT RAY & SILVER
 CIVIL ENGINEERS
 2110 N. GRAND AVENUE
 ST. LOUIS, MISSOURI 63103
 PHONE: 314-241-7100
 FAX: 314-241-7101
 WWW.PRS-LLP.COM



REV.	DATE	DESCRIPTION OF REVISION
1	05/27/07	PLANNING BOARD
2	05/28/07	PLANNING BOARD
3	05/28/07	PLANNING BOARD
4	05/28/07	PLANNING BOARD
5	05/28/07	PLANNING BOARD

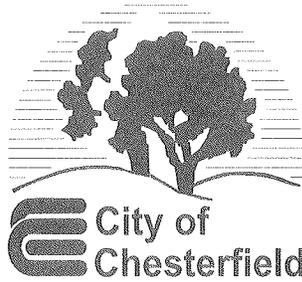


MM BUILDING

Project File:	CONSTRUCTION DOCUMENTS
Date:	05/28/07
Project Number:	05199-00
Drawn By:	Flad
Checked By:	Flad

AMMENDED SITE DEVELOPMENT PLAN

Sheet Number:	A-001-B
Rev. No.:	E
File:	c-002



VII. F.

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

May 9, 2007

Planning Commission
City of Chesterfield
690 Chesterfield Parkway West
Chesterfield, MO 63017

The Planning Commission agenda for **May 14, 2007** will include the following item for your consideration:

Pfizer (Monsanto) PGM Biopharma Building: A Site Development Section Plan, Landscape Plan and Architectural Elevations for a 200.51 acre lot of land zoned “C8” and “FPC8” Planned Commercial District located at the Northeast corner of 700 Chesterfield Parkway West.

Dear Planning Commission:

Picket Ray and Silver, on behalf of Pharmacia Corp., have submitted a Site Development Section Plan, Landscape Plan and Architectural Elevations. The Department of Planning has reviewed this submittal and submits the following report.

BACKGROUND

1. Development of the tract was originally authorized by St. Louis County Ordinance Number 9002 in 1979.
2. It was subsequently amended by Ordinance Number 10,986 in 1983.

SUBMITTAL INFORMATION

1. The request is for a 46,498 sq.ft. office/laboratory facility, located on a parcel zoned “C-8” Planned Commercial District.
2. The exterior building materials will be comprised of brick, metal panels and glass. The roof is proposed to be a flat/low slope, modified bituminous membrane.
3. The Landscape Plan is in compliance with the City of Chesterfield Tree Manual.

ARB RECOMMENDATIONS

This project was reviewed by the Architectural Review Board on February 15, 2007. The ARB voted unanimously that the project should be forwarded for approval of the Site Development Section Plan, Architectural Elevations, and Landscape Plan to the Planning Commission.

DEPARTMENTAL INPUT

The submittal was reviewed for compliance with all City of Chesterfield ordinances. The Department of Planning requests action on the Site Development Section Plan.

Respectfully submitted,



Charlie Campo
Project Planner

Respectfully submitted,



Mara M. Perry, AICP
Senior Planner of Plan Review

Cc: City Administrator
City Attorney
Department of Public Works

Attachments: Site Development Section Plan
Landscape Plan
Architectural Elevations



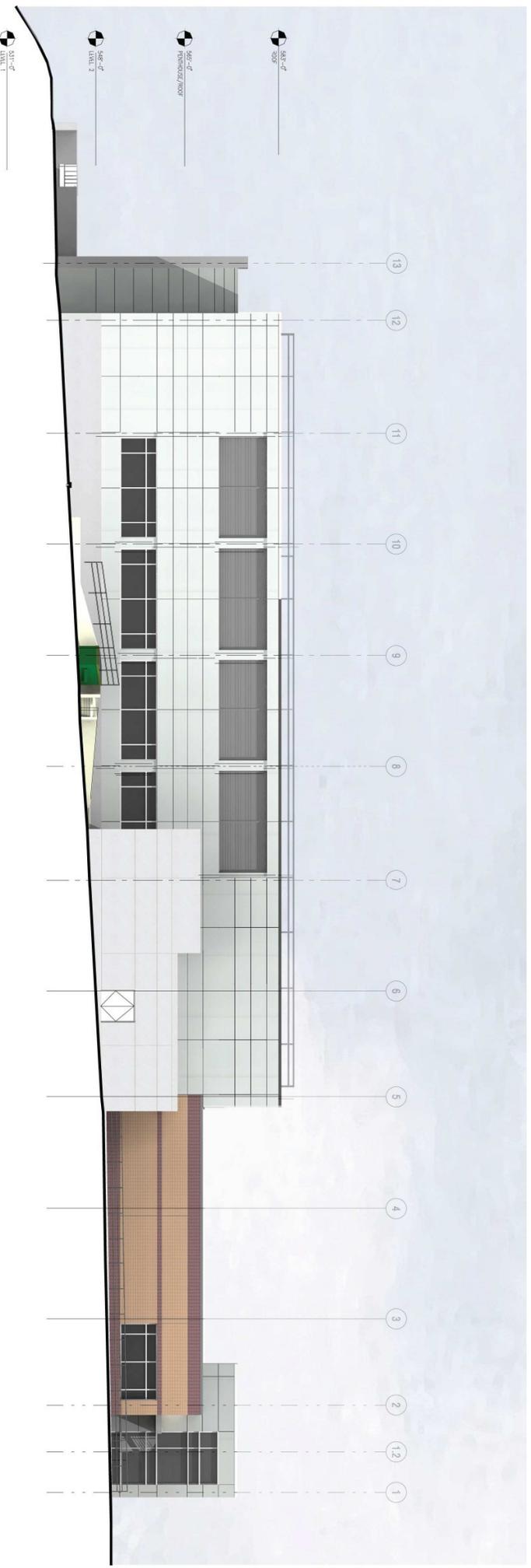
VIEW FROM ACCESS ROAD



PGM BIOPHARMA
NOT TO SCALE

Flad & Associates

PROJECT NO.: 05199-00
FEBRUARY 5 2007

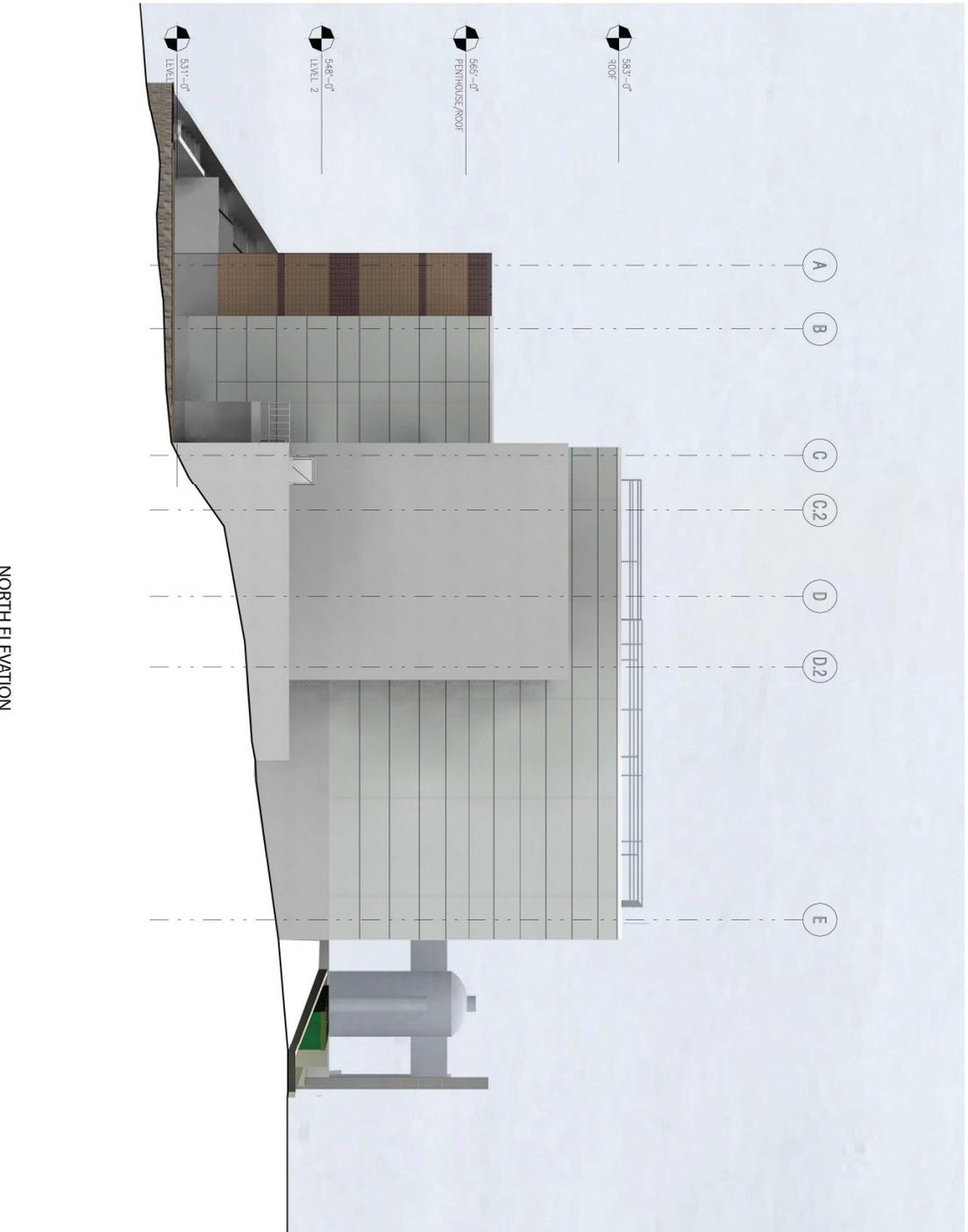


PGM BIOPHARMA
NOT TO SCALE

WEST ELEVATION

Flad & Associates

PROJECT NO.: 05199-00
FEBRUARY 5 2007



NORTH ELEVATION



PGM BIOPHARMA
NOT TO SCALE

Flad & Associates

PROJECT NO.: 05199-00
FEBRUARY 5 2007

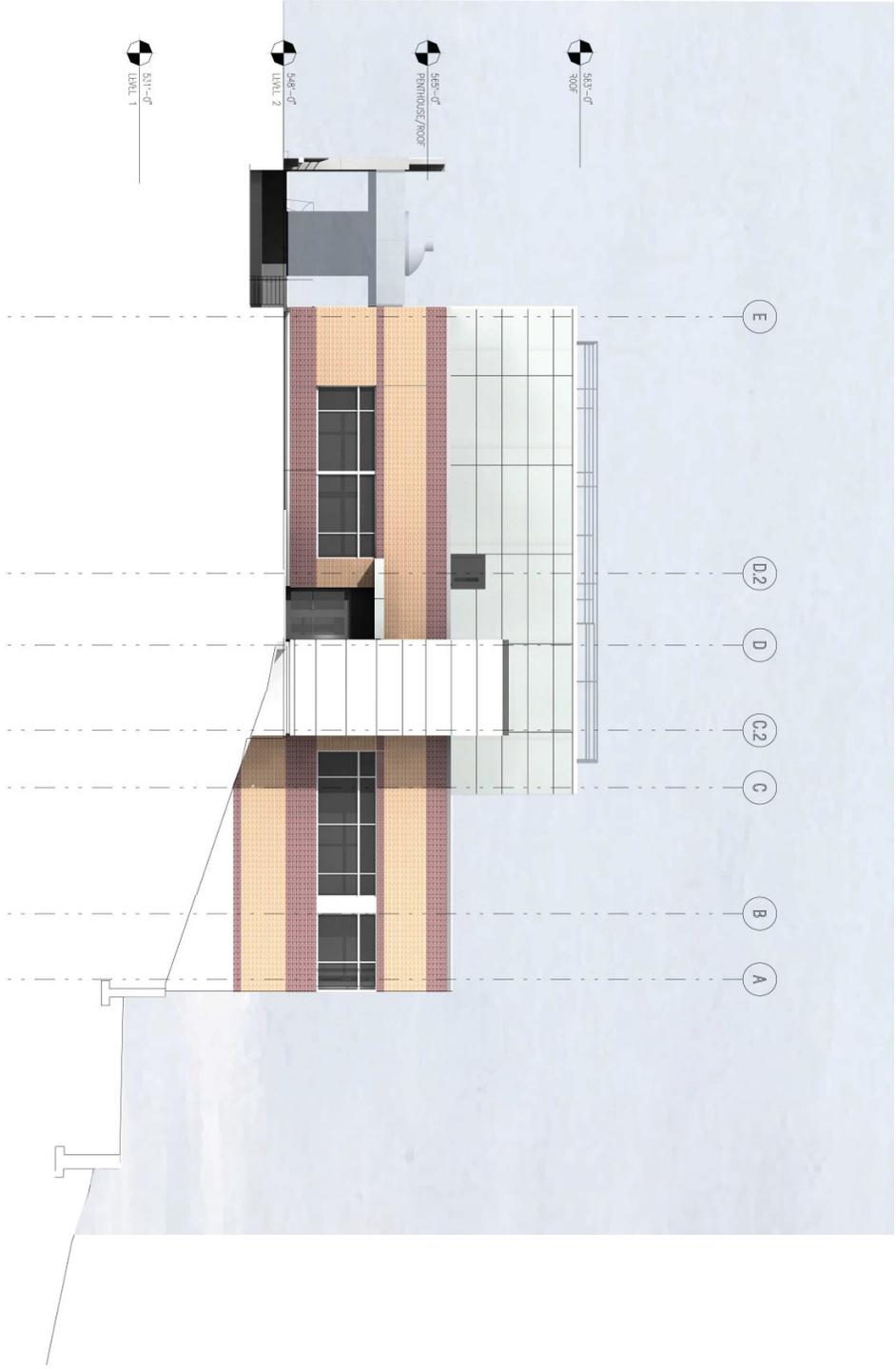


PGM BIOPHARMA
NOT TO SCALE

VIEW FROM BUILDING JJ

Flad & Associates

PROJECT NO.: 05199-00
FEBRUARY 5 2007



SOUTH ELEVATION



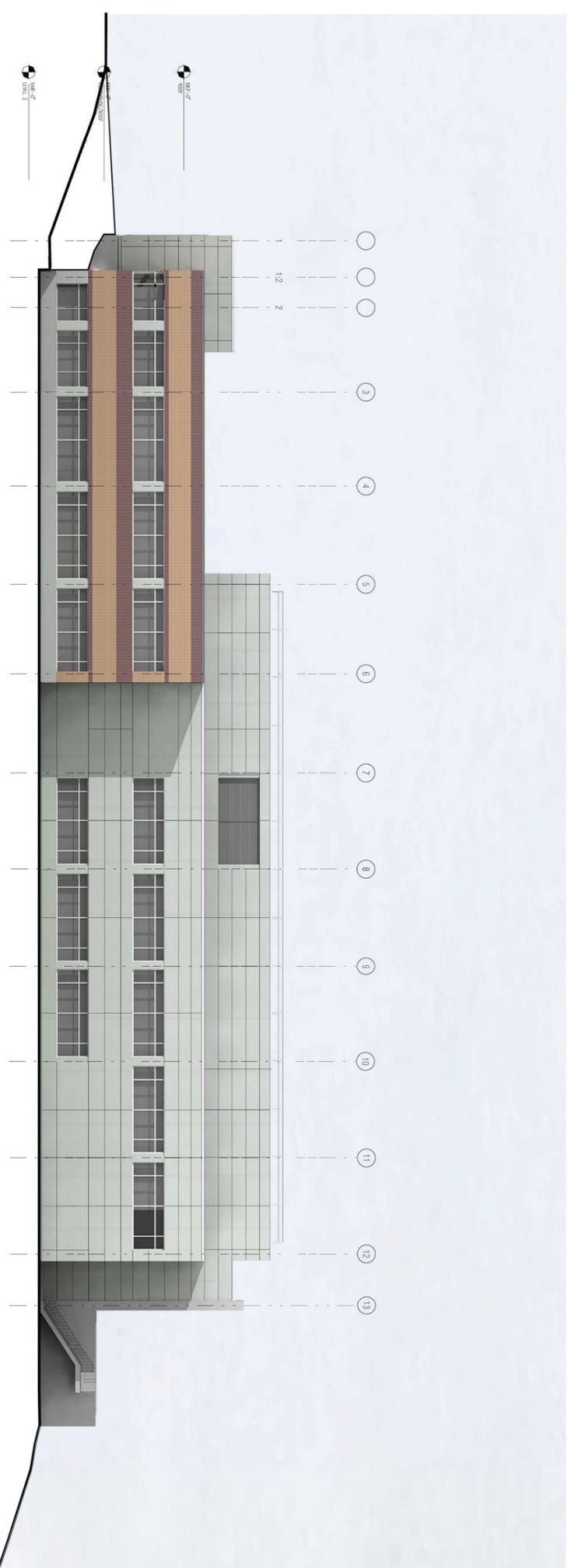
VIEW FROM NORTH WOODS



PGM BIOPHARMA
NOT TO SCALE

Flad & Associates

PROJECT NO.: 05199-00
FEBRUARY 5 2007

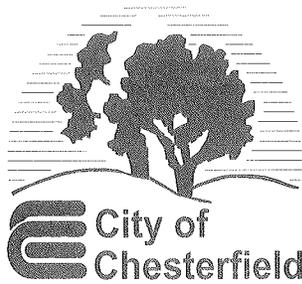


PGM BIOPHARMA
NOT TO SCALE

EAST ELEVATION

Flad & Associates

PROJECT NO.: 05199-00
FEBRUARY 5 2007



VII. G.

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

May 9, 2007

Planning Commission
City of Chesterfield
690 Chesterfield Parkway West
Chesterfield, MO 63017

The Planning Commission agenda for **May 14, 2007** will include the following item for your consideration:

The Village at Olde Baxter Square: A Record Plat for a 2.0 acre lot of land zoned “R-4” Residence District, under a Planned Environment Unit Procedure, located at the Southwest corner of Old Baxter Road and Century Lake Dr.

Dear Planning Commission:

Cornerstone Inc, on behalf of Flower Homes Inc., has submitted a Record Plat for your review. The Department of Planning has reviewed this submittal and submits the following report.

BACKGROUND

1. On May 1st, 2006, The City of Chesterfield approved Ordinance 2261 which amended the City of Chesterfield Zoning Ordinance by changing the boundaries of an “NU” Non-Urban to “R-4” 7,500 square foot residential district for 2.00 acres of land located south of Century Lake and west of Old Baxter Road.
2. On May 1st, 2006, The City of Chesterfield approved Ordinance 2262 which established a Planned Environment Unit (PEU) Special Procedure in the “R-4” 7,500 square foot residential district for 2.00 acres of land located south of Century Lake and west of Old Baxter Road.

PURPOSE

The purpose of the Record Plat is to establish 8 lots on a 2.0 acre tract of land for a single family attached and detached residential development.

DEPARTMENTAL INPUT

The submittal was reviewed for compliance with all City of Chesterfield ordinances. The Department of Planning requests approval of the Record Plat.

Respectfully submitted,



Charlie Campo
Project Planner

Respectfully submitted,



Mara M. Perry, AICP
Senior Planner of Plan Review

Cc: City Administrator
City Attorney
Department of Public Works

Attachments:
Record Plat



VIL.H

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

May 9, 2007

Planning Commission
City of Chesterfield
690 Chesterfield Parkway West
Chesterfield, MO 63017

The Planning Commission agenda for **May 14, 2007** will include the following item for your consideration:

Tuscany Reserve: A Site Development Plan, Tree Stand Delineation, Tree Preservation Plan, Landscape Plan, and Architectural Elevations for a 58.15 acre lot of land zoned “E-One Acre” Estate Residence District, and “E-Two Acre” Estate Residence District, located north of Strecker Road and east of Church Road.

Dear Planning Commission:

Stock and Associates on behalf of Simon Development Inc., has submitted a Site Development Plan, Tree Stand Delineation, Tree Preservation Plan, Landscape Plan, and Architectural Elevations for your review. The Department of Planning has reviewed this submittal and submits the following report.

BACKGROUND

1. On November 16, 1998, The City of Chesterfield approved Ordinance 1472 which amended the City of Chesterfield Zoning Ordinance by changing the boundaries of an “NU” Non-Urban to “LLR” Large Lot Residential District for 68.59 acres of land located north of Strecker Road and East of Church Road.
2. On November 20, 2007, The City of Chesterfield approved Ordinance 2322 which amended the City of Chesterfield Zoning Ordinance by changing the boundaries of an “LLR” Large Lot Residential District to “E-One Acre” Estate District for 17.06 acres and “E-Two Acre” Estate District for a 40.5 acre of land located north of Strecker Road and East of Church Road.

SUBMITTAL INFORMATION

1. The request is for a total of 36 single family detached homes constructed on both sides of a public street on a cul-de-sac. There are 16 single family detached Homes in the “E-One Acre” Estate Residential District and 20 single family detached home in the “E-Two Acre” Estate Residential District

2. The exterior building materials will be comprised of brick, stone and vinyl siding. The roof is proposed to be comprised of 20 year architectural shingles.
3. The landscape plan is in compliance with the City of Chesterfield Tree Manual.

DEPARTMENTAL INPUT

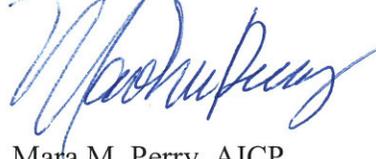
The submittal was reviewed for compliance with all City of Chesterfield ordinances. The Department of Planning requests action on the Site Development Plan.

Respectfully submitted,



Jarvis Myers
Project Planner

Respectfully submitted,



Mara M. Perry, AICP
Senior Planner of Plan Review

Cc: City Administrator
City Attorney
Department of Public Works

Attachments:

Site Development Plan
Tree Stand Delineation
Tree Preservation Plan
Landscape Plan
Architectural Elevations

SITE DEVELOPMENT PLAN

A TRACT OF LAND LYING PARTLY IN U.S. SURVEY 124, TOWNSHIP 45 NORTH - RANGE 9 EAST, AND TOWNSHIP 45 NORTH - RANGE 4 EAST, IN U.S. SURVEY 886 AND FRACTIONAL SECTION 18, TOWNSHIP 45 NORTH - RANGE 5 EAST, CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI

INDEX of SHEETS

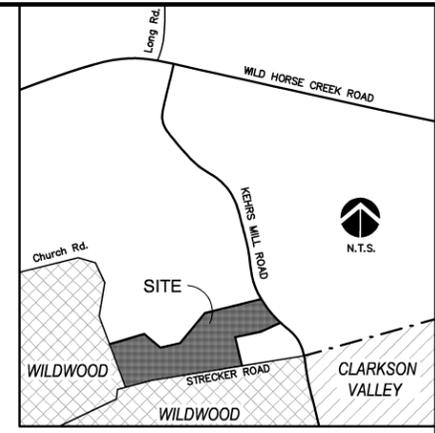
- 1 SITE DEVELOPMENT PLAN
- 2 GEOMETRIC PLAN
- 3 EXISTING CONDITIONS PLAN
- 4 DETAILS AND NOTES
- 5 DETAILS
- 6 ORDINANCE
- 7 ORDINANCE
- 8 ORDINANCE
- 9 ORDINANCE
- 10 ORDINANCE
- 11 SITE DISTANCE - KEHRS MILL ROAD
- 12 SITE DISTANCE - STRECKER ROAD

LEGEND

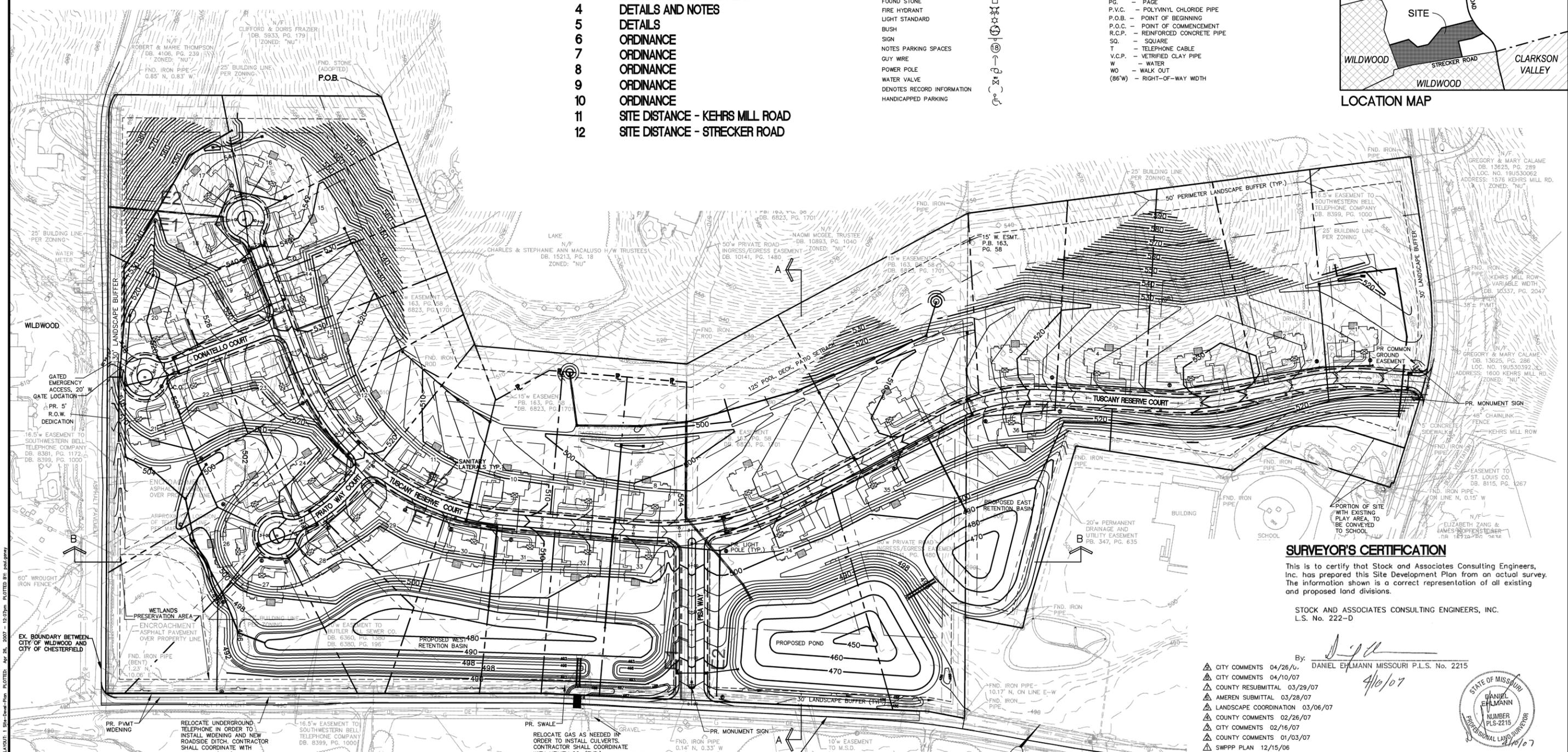
- EXISTING SANITARY SEWER
- EXISTING STORM SEWER
- EXISTING TREE
- EXISTING BUILDING
- EXISTING CONTOUR
- SPOT ELEVATION
- EXISTING UTILITIES
- FOUND 1/2" IRON PIPE
- SET IRON PIPE
- FOUND CROSS
- FOUND STONE
- FIRE HYDRANT
- LIGHT STANDARD
- BUSH
- SIGN
- NOTES PARKING SPACES
- GUY WIRE
- POWER POLE
- WATER VALVE
- DENOTES RECORD INFORMATION
- HANDICAPPED PARKING

ABBREVIATIONS

- C.O. - CLEANOUT
- DB. - DEED BOOK
- E - ELECTRIC
- FL - FLOWLINE
- FT - FEET
- FND. - FOUND
- G - GAS
- M.H. - MANHOLE
- N/F - NOW OR FORMERLY
- PB. - PLAT BOOK
- PG. - PAGE
- P.V.C. - POLYVINYL CHLORIDE PIPE
- P.O.B. - POINT OF BEGINNING
- P.O.C. - POINT OF COMMENCEMENT
- R.C.P. - REINFORCED CONCRETE PIPE
- SQ. - SQUARE
- T - TELEPHONE CABLE
- V.C.P. - VETRIFIED CLAY PIPE
- W - WATER
- WO - WALK OUT
- (86'W) - RIGHT-OF-WAY WIDTH



LOCATION MAP



SURVEYOR'S CERTIFICATION

This is to certify that Stock and Associates Consulting Engineers, Inc. has prepared this Site Development Plan from an actual survey. The information shown is a correct representation of all existing and proposed land divisions.

STOCK AND ASSOCIATES CONSULTING ENGINEERS, INC.
L.S. No. 222-D

- ▲ CITY COMMENTS 04/26/07
- ▲ CITY COMMENTS 04/10/07
- ▲ COUNTY RESUBMITTAL 03/29/07
- ▲ AMEREN SUBMITTAL 03/28/07
- ▲ LANDSCAPE COORDINATION 03/06/07
- ▲ COUNTY COMMENTS 02/26/07
- ▲ CITY COMMENTS 02/16/07
- ▲ COUNTY COMMENTS 01/03/07
- ▲ SWPPP PLAN 12/15/06



FLOODPLAIN NOTE

Portion of subject property lies within flood zones AE & X per the National Flood Insurance Program, Flood Insurance Rate Map for St. Louis County, Missouri & Incorporated Areas. The map is identified as map No. 2918K0138 H with an effective date of August 2, 1995. 100 year HW elevations vary from 485 ft in the southwest corner to 496 ft in the southeast corner.

GEOTECHNICAL ENGINEER'S STATEMENT

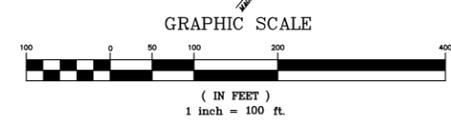
Neither SCI Engineering, Inc. (SCI) nor the undersigned has prepared any part of these plans. The signature and seal are intended to confirm our review and professional opinion that these plans and revisions, through the date given below, comply with the Geotechnical Report for the project, dated 12/08/06, and are compatible with the soil and geologic conditions at the site, as anticipated from the exploration data.

Conditions may vary from those encountered during the exploration or can change due to construction activities, weather, or other conditions. Therefore, SCI must be involved during construction of this project to observe the actual subsurface conditions and implement recommendations relative to construction. Construction means and methods shall be approved by Contractor.

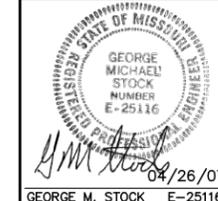
SCI ENGINEERING, INC.
Date: 4/10/07
WILLIAM J. GREEN
NUMBER E-18904

ST. LOUIS COUNTY BENCHMARK
12-170: STANDARD ALUMINUM DISK STAMPED "SL-37", 1990. DISK IS SET ALONG THE NORTH SIDE OF STRECKER ROAD NEAR THE SOUTHERLY CORNER OF THE ATHLETIC FIELD OF LINDA VISTA MONTESSORI CATHOLIC SCHOOL; APPROXIMATELY 0.1 MILE WEST OF THE INTERSECTION OF KEHRS MILL ROAD AND STRECKER ROAD.
ELEV: 483.60 (U.S.G.S. - NGVD 1929 DATUM)

PREPARED FOR:
SIMON DEVELOPMENT, INC.
632 TRADE CENTER BLVD.
SUITE A
CHESTERFIELD, MO 63005
636/537-8700
636/537-8705 FAX
ATTN: MR. BARRY SIMON, PRESIDENT



M.S.D. P# 27551-00
BASE MAP # 19U



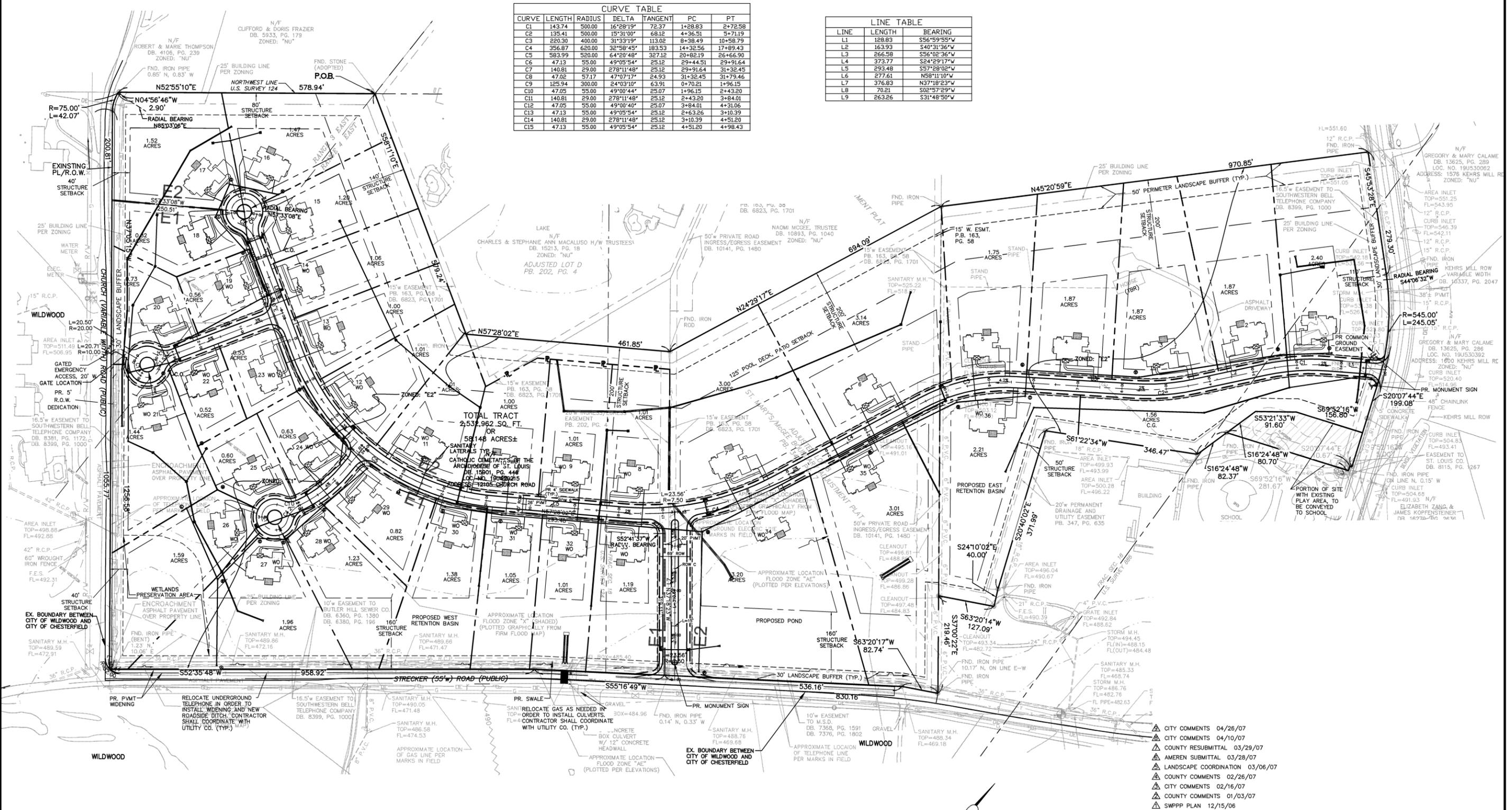
**TUSCANY RESERVE
SITE DEVELOPMENT PLAN**

Stock & Associates
Consulting Engineers, Inc.

257 Chesterfield Business Parkway
St. Louis, MO 63005
PH. (636) 530-9100
FAX (636) 530-9130
e-mail: general@stockassoc.com
Web: www.stockassoc.com

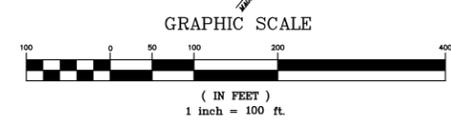
CURVE TABLE						
CURVE	LENGTH	RADIUS	DELTA	TANGENT	PC	PT
C1	143.74	500.00	161°28'19"	72.37	1+28.83	2+72.58
C2	135.41	500.00	151°31'00"	68.12	4+36.51	5+71.19
C3	220.30	400.00	31°33'19"	113.02	8+38.49	10+58.79
C4	356.87	620.00	32°58'45"	183.53	14+32.56	17+89.43
C5	583.99	520.00	64°20'48"	327.12	20+82.19	26+66.90
C6	47.13	55.00	49°05'54"	25.12	29+44.51	29+91.64
C7	140.81	29.00	278°11'48"	25.12	29+91.64	31+32.45
C8	47.02	57.17	47°07'17"	24.93	31+32.45	31+79.46
C9	125.94	300.00	24°03'10"	63.91	0+70.21	1+96.15
C10	47.05	55.00	49°00'44"	25.07	1+96.15	2+43.20
C11	140.81	29.00	278°11'48"	25.12	2+43.20	3+84.01
C12	47.05	55.00	49°00'40"	25.07	3+84.01	4+31.06
C13	47.13	55.00	49°05'54"	25.12	4+31.06	4+78.19
C14	140.81	29.00	278°11'48"	25.12	3+10.39	4+51.20
C15	47.13	55.00	49°05'54"	25.12	4+51.20	4+98.43

LINE TABLE		
LINE	LENGTH	BEARING
L1	128.83	S56°59'55"W
L2	163.93	S40°31'36"W
L3	266.58	S56°02'36"W
L4	373.77	S24°29'17"W
L5	293.48	S57°28'02"W
L6	277.61	N58°11'10"W
L7	376.83	N37°18'23"W
L8	70.21	S02°57'29"W
L9	263.26	S31°48'50"W



- ▲ CITY COMMENTS 04/26/07
- ▲ CITY COMMENTS 04/10/07
- ▲ COUNTY RESUBMITTAL 03/29/07
- ▲ AMEREN SUBMITTAL 03/28/07
- ▲ LANDSCAPE COORDINATION 03/06/07
- ▲ COUNTY COMMENTS 02/26/07
- ▲ CITY COMMENTS 02/16/07
- ▲ COUNTY COMMENTS 01/03/07
- ▲ SWPPP PLAN 12/15/06

M.S.D. P# 27551-00
BASE MAP # 19U



GEORGE M. STOCK
E-25116

04/26/07

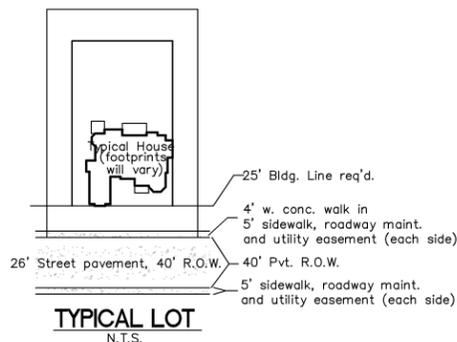
GEORGE M. STOCK E-25116

TUSCANY RESERVE
GEOMETRIC PLAN

Stock & Associates
Consulting Engineers, Inc.

257 Chesterfield Business Parkway
St. Louis, MO 63005
PH. (636) 530-9100
FAX (636) 530-9130
e-mail: general@stockassoc.com
Web: www.stockassoc.com

DRAWN BY: P.R.G. DATE: 12/12/06 CHECKED BY: C.A.M. DATE: 12/12/06 JOB NUMBER: 206-3933 SHEET: 2 OF 12



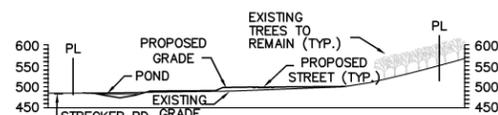
GREEN SPACE CALCULATIONS

Green Area:	58.15 ac. site
	- 0.52 ac. play area site

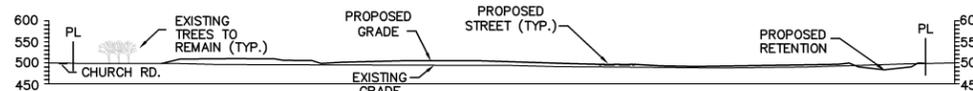
	57.63 ac. site - net
	- 3.05 ac. streets
	- 0.74 ac. sidewalks
	- 4.82 ac. typ. house, drive & patio (0.134 ac.) x 36 lots

	49.02 ac. green area and all non-paved surfaces

49.02 ac. green area and all non-paved surfaces	= 0.86 or 86% Greenspace
57.63 ac. net site - 0.74 ac. sidewalks	(percentage includes sidewalks)



A-A
Scale: 1" = 200' (horiz. & vert.)
(SECTIONS COURTESY OF HALL AND HALSEY)



B-B
Scale: 1" = 200' (horiz. & vert.)

TUSCANY RESERVE E1 ZONING DESCRIPTION

A tract of land being part of U.S. Survey 124 in Township 45 North Range 3 East and Township 45 North Range 4 East and part of Fractional Section 18 and U. S. Survey 886 in Township 45 North Range 4 East St. Louis County, Missouri and being more particularly described as follows:

COMMENCING at the point of intersection of the Northwest line of U.S. Survey 124 with the Range line between Township 45 North, Range 3 East and Township 45 North, Range 4 East, said point being on the northwestern line of a tract of land described in a deed to St. Mary's Institute as recorded in Deed Book 5212 Page 35 of the St. Louis County, Missouri Records; thence along the northwestern line of said St. Mary's Institute tract South 52 degrees 55 minutes 10 seconds West 578.94 feet to a point on the northeastern line of Church Road (variable width); thence along said northeastern line the following courses and distances; South 04 degrees 56 minutes 48 seconds East, 2.90 feet to a point of curvature; thence along a curve to the left having a radius of 75.00 feet and an arc length of 42.07 feet to the point of tangency; thence South 37 degrees 05 minutes 15 seconds East, 200.81 feet to the POINT OF BEGINNING of the tract herein described; thence leaving said northeastern line, through the aforesaid St. Mary's Institute tract along the line between proposed lots 17 and 18 North 57 degrees 33 minutes 08 seconds East, a distance of 250.51 feet to a point on the southeastern line of a proposed cul-de-sac; thence along said southeastern line along a curve to the left having a radius of 49.00 feet, an arc length of 64.00 feet, and a chord which bears South 69 degrees 51 minutes 58 seconds East, a chord distance of 59.55 feet to a point of reverse curvature; thence along a curve right having a radius of 35.00 feet, an arc length of 29.99 feet, and a chord which bears South 82 degrees 44 minutes 07 seconds East, a chord distance of 29.08 feet to a point on the southeastern line of a proposed street (40 feet wide); thence along the southeastern line of said proposed street the following courses and distances; South 58 degrees 11 minutes 10 seconds East, a distance of 277.61 feet to a point; thence along a curve to the left having a radius of 540.00 feet, an arc length of 606.45 feet, and a chord which bears North 89 degrees 38 minutes 26 seconds East, a chord distance of 575.08 feet to a point; thence North 57 degrees 28 minutes 02 seconds East, a distance of 293.48 feet to a point; thence along a curve to the left having a radius of 640.00 feet, an arc length of 69.63 feet, and a chord which bears North 54 degrees 21 minutes 02 seconds East, a chord distance of 69.60 feet to a point; thence along a curve to the right having a radius of 25.00 feet, an arc length of 39.91 feet, and a chord which bears South 83 degrees 02 minutes 11 seconds East, a chord distance of 35.80 feet to a point on the southwestern line of a proposed street (69 feet wide); thence along said southwestern line South 37 degrees 18 minutes 23 seconds East, a distance of 319.11 feet to a point on the northeastern line of Strecker Road; thence along the northeastern line of Strecker Road South 55 degrees 16 minutes 49 seconds West, a distance of 294.00 feet to a point; thence South 52 degrees 35 minutes 48 seconds West, a distance of 958.92 feet to the point of intersection with the northeastern line of the aforesaid Church Road; thence along said northeastern line North 37 degrees 05 minutes 15 seconds West, a distance of 1055.76 feet to the Point of Beginning and containing (743,427 sq. ft. 17.066 acres).

TUSCANY RESERVE E2 ZONING DESCRIPTION

A tract of land being part of U.S. Survey 124 in Township 45 North Range 3 East and Township 45 North Range 4 East and part of Fractional Section 18 and U. S. Survey 886 in Township 45 North Range 4 East St. Louis County, Missouri and being more particularly described as follows:

BEGINNING at the point of intersection of the northwestern line of U.S. Survey 124 with the Range line between Township 45 North, Range 3 East and Township 45 North, Range 4 East, said point being on the northwestern line of a tract of land described in a deed to St. Mary's Institute as recorded in Deed Book 5212 Page 35 of the St. Louis County, Missouri Records; thence along said northwestern line the following; South 58 degrees 11 minutes 10 seconds East, a distance of 579.24 feet to a point; thence North 57 degrees 28 minutes 02 seconds East, a distance of 461.85 feet to a point; thence North 24 degrees 29 minutes 17 seconds East, a distance of 694.09 feet to a point; thence North 45 degrees 20 minutes 59 seconds East, a distance of 970.85 feet to a point on the southwestern line of Kehrs Mill Road (variable width); thence along said southwestern line the following courses and distances; South 45 degrees 53 minutes 28 seconds East, a distance of 279.30 feet to a point; thence along a curve to the right having a radius of 545.00 feet, an arc length of 245.05 feet, and a chord which bears South 33 degrees 00 minutes 36 seconds East, a chord distance of 242.99 feet to a point; thence South 20 degrees 07 minutes 24 seconds East, a distance of 118.88 feet to a point; thence leaving said southwestern line, through the aforesaid St. Mary's Institute tract South 69 degrees 52 minutes 16 seconds West, a distance of 156.80 feet to a point; thence South 53 degrees 21 minutes 33 seconds West, a distance of 91.60 feet to a point; thence South 16 degrees 24 minutes 48 seconds West, a distance of 80.70 feet to a point; thence South 16 degrees 24 minutes 48 seconds West, a distance of 82.37 feet to a point; thence South 61 degrees 22 minutes 34 seconds West, a distance of 346.47 feet to a point; thence South 20 degrees 40 minutes 02 seconds East, a distance of 371.99 feet to a point; thence South 24 degrees 10 minutes 02 seconds East, a distance of 40.00 feet to a point; thence South 63 degrees 20 minutes 14 seconds West, a distance of 127.09 feet to a point; thence South 37 degrees 00 minutes 22 seconds East, a distance of 219.46 feet to a point on the northwestern line of Strecker Road; thence along said northwestern line of Strecker Road the following courses and distances; South 63 degrees 20 minutes 17 seconds West, a distance of 82.74 feet to a point; thence South 55 degrees 16 minutes 49 seconds West, a distance of 536.16 feet to a point on the southwestern line of a proposed street (69 feet wide); thence leaving the northwestern line of Strecker Road along said southwestern line of the proposed street North 37 degrees 18 minutes 23 seconds West, a distance of 319.11 feet to a point of curvature; thence along a curve to the left having a radius of 25.00 feet, an arc length of 39.91 feet to a point, and a chord which bears North 83 degrees 02 minutes 11 seconds West, a chord distance of 35.80 feet to a point of reverse curvature on the southeastern line of a proposed street (40 feet wide); thence along said southeastern line along a curve to the right having a radius of 640.00 feet, an arc length of 69.63 feet, and a chord which bears South 54 degrees 21 minutes 02 seconds West, a chord distance of 69.60 feet to a point; thence South 57 degrees 28 minutes 02 seconds West, a distance of 293.48 feet to a point; thence along a curve to the right having a radius of 540.00 feet, an arc length of 606.45 feet, and a chord which bears South 89 degrees 38 minutes 26 seconds West, a chord distance of 575.08 feet to a point; thence North 58 degrees 11 minutes 10 seconds West, a distance of 277.61 feet to a point of curvature; thence along a curve to the left having a radius of 35.00 feet, an arc length of 29.99 feet, and a chord which bears North 82 degrees 44 minutes 07 seconds West, a chord distance of 29.08 feet to a point of reverse curvature on the southeastern line of a proposed cul-de-sac; thence along a curve to the right having a radius of 49.00 feet, an arc length of 64.00 feet, and a chord which bears North 69 degrees 51 minutes 58 seconds West, a chord distance of 59.55 feet to a point; thence leaving the southeastern line of the cul-de-sac along the proposed lot line between proposed lots 17 and 18 South 57 degrees 33 minutes 08 seconds West, a distance of 250.51 feet to a point on the northeastern line of Church Road (variable width); thence along said northeastern line North 37 degrees 05 minutes 15 seconds West, a distance of 200.81 feet to a point; thence along a curve to the right having a radius of 75.00 feet, an arc length of 42.07 feet, and a chord which bears North 21 degrees 01 minutes 01 seconds West, a chord distance of 41.52 feet to a point; thence North 04 degrees 56 minutes 48 seconds West, a distance of 2.90 feet to a point; thence leaving said northeastern line along the northwestern line of the aforesaid St. Mary's Institute tract; North 52 degrees 55 minutes 10 seconds East, a distance of 578.94 feet to the Point of Beginning and containing (1,766,940 sq. ft. 40.563 acres).

GENERAL NOTES

- Size of Tract: 58.15 acres - 0.52 ac. play area** = 57.63 ac. net (+/-) [**play area site to be conveyed to the Linda Vista School]
- Existing Zoning: Large Lot Residential (LLR) District
- Proposed Zoning: "E-One Acre" Estate District, and "E-Two Acre" Estate District
- Density: E-1: 17.07 acres/(1ac/lot) = 17 lots max allowed vs. 16 lots proposed.
E-2: 40.56 acres/(2ac/lot) = 20 lots max allowed vs. 20 lots proposed.
*NOTE: 0.52 acres to Linda Vista (41.08-0.52)=40.56Ac./2Ac=20 Lots -OK (per Ordinance 2275, 3.B)
- Development Proposed: 36 single family detached lots
- Development Criteria: (See Typical Lot)
Lots 6, 7, 34 & 35 are 3 ac. min., as that land area is encumbered by Paoland Place indentures requiring 3 ac. lots.
- Partinent Information:
Metropolitan St. Louis Sewer District Rockwood R-6 School District Ameren Electric Company
Laclede Gas Company SBC Telephone Company Missouri-American Water Company
Charter Communication Company Monarch FPD
- Proposed streets shall be public and built to City of Chesterfield standards. ROW shall be 40' width, with 5' roadway maintenance, sidewalk and utility easements on each side. Pavement shall be 26' width. Sidewalks are required on one side of the street only. Sidewalks will be provided on both sides of street (as shown) if required by the City of Chesterfield.
- Fire Access: An "Emergency Vehicle Only" access shall be provided on the western side of the development, connecting a cul-de-sac street to Church Road. This access shall be gated at the property line (or other location as approved) to prohibit unauthorized use. A Knox lock shall be provided, as required by Fire District.
- Fire Protection: Each basement in required to have at least one egress window, unless it is a walkout with a door. If a window well is used it will count as part of the structure and must be within the structure setbacks.
- Stormwater: Stormwater detention is proposed, in basins on the west and east sides of the development. Detention shall be constructed as required by MSD and City of Chesterfield.
- All proposed utilities shall be placed underground. The development of this parcel will coordinate the installation of all utilities in conjunction with the construction of any roadway on site.
- Sign approval is a separate process.
- Plans subject to change during improvement plan design and agency review.

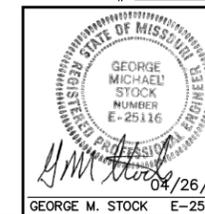
COUNTY NOTES

- ALL SIDEWALKS IN KEHRS MILL ROAD R.O.W. TO BE CONSTRUCTED TO SAINT LOUIS COUNTY ADA STANDARDS.
- ALL PROPOSED IMPROVEMENTS IN KEHRS MILL R.O.W. SHALL BE CONSTRUCTED TO SAINT LOUIS COUNTY STANDARDS.
- ALL GRADING AND DRAINAGE IN KEHRS MILL R.O.W. TO BE IN CONFORMANCE WITH SAINT LOUIS COUNTY AND MSD STANDARDS
- NO SLOPES WITHIN SAINT LOUIS COUNTY RIGHT-OF-WAY SHALL EXCEED 3 (HORIZONTAL) TO 1 (VERTICAL)
- STORM WATER SHALL BE DISCHARGED AT AN ADEQUATE NATURAL DISCHARGE POINT. SINKHOLES ARE NOT ADEQUATE DISCHARGE POINTS.
- PRIOR TO SPECIAL USE PERMIT ISSUANCE BY THE SAINT LOUIS COUNTY DEPARTMENT OF HIGHWAYS AND TRAFFIC, A SPECIAL CASH ESCROW OR A SPECIAL ESCROW SUPPORTED BY AN IRREVOCABLE LETTER OF CREDIT, MUST BE ESTABLISHED WITH THE SAINT LOUIS COUNTY DEPARTMENT OF HIGHWAYS AND TRAFFIC TO GUARANTEE COMPLETION OF THE REQUIRED ROADWAY IMPROVEMENTS ALONG KEHRS MILL ROAD.
- ACCESS TO THIS DEVELOPMENT FROM KEHRS MILL ROAD SHALL BE VIA ONE ENTRANCE LOCATED TO PROVIDE REQUIRED SITE DISTANCE AND CONSTRUCTED TO SAINT LOUIS COUNTY STANDARDS AS DIRECTED BY THE SAINT LOUIS COUNTY DEPARTMENT OF HIGHWAYS AND TRAFFIC.

- ▲ CITY COMMENTS 04/26/07
- ▲ CITY COMMENTS 04/10/07
- ▲ COUNTY RESUBMITTAL 03/29/07
- ▲ AMEREN SUBMITTAL 03/28/07
- ▲ LANDSCAPE COORDINATION 03/06/07
- ▲ CITY COMMENTS 02/26/07
- ▲ CITY COMMENTS 02/16/07
- ▲ COUNTY COMMENTS 01/03/07
- ▲ SWPPP PLAN 12/15/06

TUSCANY RESERVE DETAILS AND NOTES

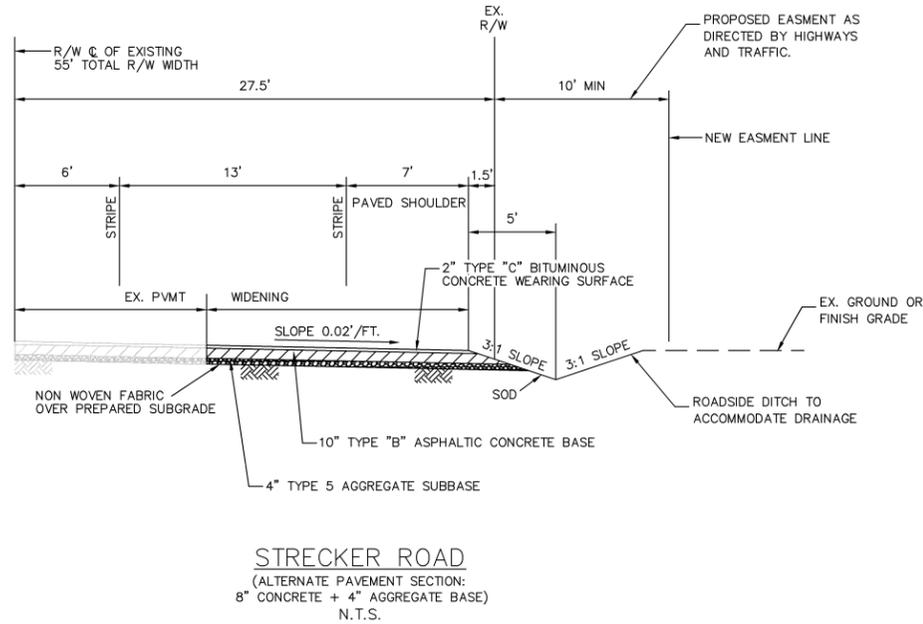
M.S.D. P# 27551-00
BASE MAP # 19U



STOCK & ASSOCIATES
Consulting Engineers, Inc.

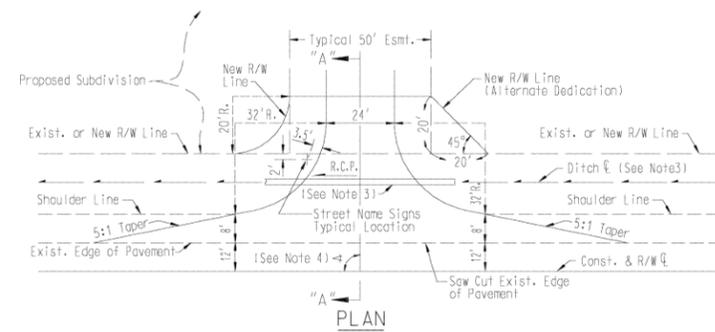
257 Chesterfield Business Parkway
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PH. (636) 530-9100
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e-mail: general@stockassoc.com
Web: www.stockassoc.com

DATE: 12/12/06
CHECKED BY: C.A.M.
JOB NUMBER: 206-3933
SHEET: 4 OF 12

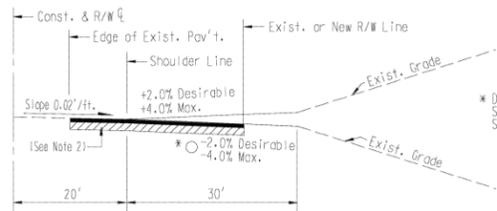


STRECKER ROAD
(ALTERNATE PAVEMENT SECTION:
8" CONCRETE + 4" AGGREGATE BASE)
N.T.S.

NEW	REVISIONS			
7-18-88	1-11-90	4-19-93	10-11-97	3-2-98



"A" PLAN



SECTION "A"- "A"

- GENERAL NOTES**
1. Do not scale drawing. Follow dimensions.
 2. Payment within Public Right-of-Way to consist of 1/2" Type 'C' Bituminous Concrete Base and 2" Type 'C' Bituminous Concrete Wearing Surface.
 3. Ditch and culvert pipe with Flared End Sections if required.
 4. Angle of entrance as shown on record plat. (70 Max. -90 Min.) per Saint Louis County Subdivision Ordinance.

SAINT LOUIS COUNTY
DEPARTMENT OF HIGHWAYS AND TRAFFIC
CLAYTON, MISSOURI

PAVEMENT CONSTRUCTION DETAILS
STREET INTERSECTION APPROACH
(LARGE LOT SUBDIVISION)

REVISION DATE: March 2, 1998

DRAWING
C608.34

STRECKER ENTRANCE DETAIL
N.T.S.

Missouri Dusk-To-Dawn Basic Monthly Rates

Type of Luminaire	Lamp Wattage	Lumen Output	Monthly Rate
Open-Type	100 H.P.S.	9,500	\$ 6.90
Enclosed-Type	100 H.P.S.	9,500	\$ 7.70
Enclosed-Type	250 H.P.S.	23,500	\$11.27
Enclosed-Type	400 H.P.S.	50,000	\$20.00
Directional	250 H.P.S.	23,500	\$14.30
Directional	400 H.P.S.	50,000	\$22.61
Directional	400 M.H.	36,000	\$14.30
Directional	1000 M.H.	100,000	\$45.21

Total cost for complete lighting service using high pressure sodium (H.P.S.) lamps on an existing pole. There are extra charges if additional facilities are required.

Open-Type Luminaire H.P.S. Enclosed-Type Luminaire H.P.S. Directional Luminaire H.P.S., M.H.

Post Top Luminaires
All units have photoelectric control. They turn "on" at dusk and "off" at dawn - automatically.

Lamp Wattage	Lumen Output	Monthly Rate*
100 H.P.S.	9,500	\$14.44

*Plus municipal tax where applicable.
All post top luminaires require underground wiring (cost of which is not included in the above).
Monthly rate includes fiberglass pole, luminaire, high pressure sodium (H.P.S.) lamp, electricity and maintenance. 10-year contracts only.

Dusk-To-Dawn Outdoor Lighting

The Safety and Security of Outdoor Lighting for Your Home or Business

- Provides optimum security over a large area
- Comes on and shuts off automatically
- Cost can be added to your monthly AmerenUE bill

Three Types Available

- Protect-O-Lite - wall-mounted flood lights
- Post Top Luminaire - freestanding pole lamp
- Area Lighting - mounted to existing utility pole

To find out more about Dusk-To-Dawn Outdoor Lighting, contact AmerenUE at 554-4500, or toll-free at 1-800-552-7989.

Ameren UE

LIGHTING DETAIL
N.T.S.

- ▲ CITY COMMENTS 04/26/07
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- ▲ COUNTY COMMENTS 01/03/07
- ▲ SWPPP PLAN 12/15/06

TUSCANY RESERVE
DETAILS

M.S.D. P# 27551-00
BASE MAP # 19U

STATE OF MISSOURI
REGISTERED PROFESSIONAL ENGINEER
GEORGE MICHAEL STOCK
NUMBER E-25116
04/26/07
GEORGE M. STOCK E-25116

STOCK & ASSOCIATES
Consulting Engineers, Inc.

257 Chesterfield Business Parkway
St. Louis, MO 63005
PH. (636) 530-9100
FAX (636) 530-9130
e-mail: general@stockassoc.com
Web: www.stockassoc.com

DRAWN BY: P.R.G. DATE: 12/12/06
CHECKED BY: C.A.M. DATE: 12/12/06
JOB NUMBER: 206-3933
SHEET: 5 OF 12

DRAWING FILE: C:\VIA\WORK\2063933\2063933.dwg (2063933.dwg) PLOTTED: 04/26/07 12:16pm PLOTTED BY: paul.gary

BILL NO. 2495

ORDINANCE NO. 2322

AN ORDINANCE AMENDING THE ZONING ORDINANCE OF THE CITY OF CHESTERFIELD BY CHANGING THE BOUNDARIES OF AN "LLR" LARGE LOT RESIDENTIAL DISTRICT TO AN "E-ONE ACRE" ESTATE DISTRICT, FOR A 17.06 ACRE TRACT OF LAND, AND TO AN "E-TWO ACRE" ESTATE DISTRICT, FOR A 40.5 ACRE TRACT OF LAND, LOCATED NORTH OF STRECKER ROAD AND EAST OF CHURCH ROAD (P.Z. 05-2006, BARRY SIMON DEVELOPMENT (TUSCANY RESERVE))

WHEREAS, the Petitioner, Michael Doster, requested a change in zoning from "LLR" Large Lot Residential District to "E-One Acre" Estate District and "E-Two Acre" Estate District; and,

WHEREAS, the Planning Commission held a public hearing regarding the said request on March 13, 2006; and,

WHEREAS, the Planning Commission upon review of said request recommended approval by a vote of 5-3 on August 28, 2006 with conditions as written in the Attachment A; and,

WHEREAS, the City Council upon review of said request, recommended approval with an amendment to the structure setbacks, landscape requirements, and zoning district requirements.

NOW THEREFORE BE IT ORDAINED BY THE CITY OF COUNCIL OF THE CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI, AS FOLLOWS:

Section 1. The City of Chesterfield Zoning Ordinance and Official Zoning District Maps, which are part thereof, are hereby amended by approving preliminary plans for a change in zoning of a "LLR" Large Lot Residential District to "E-One Acre" Estate District for 17.06 acre tract of land and "E-Two Acre" Estate District for 40.5 acre tract of land located north of Strecker Road and east of Church Road in the City of Chesterfield as follows:

LAND DESCRIPTION-"E-One Acre" Estate District

A tract of land being part of U.S. Survey 124 in Township 45 North Range 3 East and Township 45 North Range 4 East and part of Fractional Section 18 and U. S. Survey 886 in Township 45 North Range 4 East St. Louis County, Missouri and being more particularly described as follows:

COMMENCING at the point of intersection of the Northwest line of U.S. Survey 124 with the Range line between Township 45 North, Range 3 East and Township 45 North, Range 4 East, said point being on the northwestern line of a tract of land described in a deed to St. Mary's Institute as recorded in Deed Book 5212 Page 35 of the St. Louis County, Missouri Records; thence along the northwestern line of said St. Mary's Institute tract South 52 degrees 55 minutes 10 seconds West 578.94 feet to a point on the northeastern line of Church Road (variable width); thence along said northeastern line the following courses and distances: South 04 degrees 56 minutes 46 seconds East, 2.90 feet to a point of curvature; thence along a curve to the left having

a radius of 75.00 feet and an arc length of 42.07 feet to the point of tangency; thence South 37 degrees 05 minutes 15 seconds East, 200.81 feet to the POINT OF BEGINNING of the tract herein described; thence leaving said northeastern line, through the aforesaid St. Mary's Institute tract along the line between proposed lots 17 and 18 North 57 degrees 33 minutes 08 seconds East, a distance of 250.51 feet to a point on the southeastern line of a proposed cul-de-sac; thence along said southeastern line along a curve to the left having a radius of 49.00 feet, an arc length of 64.00 feet, and a chord which bears South 69 degrees 51 minutes 58 seconds East, a chord distance of 59.55 feet to a point of reverse curvature; thence along a curve right having a radius of 35.00 feet, an arc length of 29.99 feet, and a chord which bears South 82 degrees 44 minutes 07 seconds East, a chord distance of 29.08 feet to a point on the southeastern line of a proposed street (40 feet wide); thence along the southeastern line of said proposed street the following courses and distances: South 58 degrees 11 minutes 10 seconds East, a distance of 277.61 feet to a point; thence along a curve to the left having a radius of 540.00 feet, an arc length of 606.45 feet, and a chord which bears North 89 degrees 38 minutes 26 seconds East, a chord distance of 575.08 feet to a point; thence North 57 degrees 28 minutes 02 seconds East, a distance of 293.48 feet to a point; thence along a curve to the left having a radius of 640.00 feet, an arc length of 69.63 feet, and a chord which bears North 54 degrees 21 minutes 02 seconds East, a chord distance of 69.60 feet to a point; thence along a curve to the right having a radius of 25.00 feet, an arc length of 39.91 feet, and a chord which bears South 83 degrees 02 minutes 11 seconds East, a chord distance of 35.80 feet to a point on the southwestern line of a proposed street (69 feet wide); thence along said southwestern line South 37 degrees 18 minutes 23 seconds East, a distance of 319.11 feet to a point on the northeastern line of Strecker Road; thence along the northeastern line of Strecker Road South 53 degrees 16 minutes 49 seconds West, a distance of 294.00 feet to a point; thence South 52 degrees 35 minutes 48 seconds West, a distance of 958.92 feet to the point of intersection with the northeastern line of the aforesaid Church Road; thence along said northeastern line North 37 degrees 05 minutes 15 seconds West, a distance of 1055.76 feet to the Point of Beginning and containing (743,427 sq. ft. 17.066 acres).

LAND DESCRIPTION-"E-Two Acre" Estate District

A tract of land being part of U.S. Survey 124 in Township 45 North Range 3 East and Township 45 North Range 4 East and part of Fractional Section 18 and U. S. Survey 886 in Township 45 North Range 4 East St. Louis County, Missouri and being more particularly described as follows:

BEGINNING at the point of intersection of the northwestern line of U.S. Survey 124 with the Range line between Township 45 North, Range 3 East and Township 45 North, Range 4 East, said point being on the northwestern line of a tract of land described in a deed to St. Mary's Institute as recorded in Deed Book 5212 Page 35 of the St. Louis County, Missouri Records; thence along said northwestern line the following courses and distances: South 58 degrees 11 minutes 10 seconds East, a distance of 579.24 feet to a point; thence North 57 degrees 28 minutes 02 seconds East, a distance of 461.85 feet to a point; thence North 24 degrees 29 minutes 17 seconds East, a distance of 694.09 feet to a point; thence North 45 degrees 20 minutes 59 seconds East, a distance of 970.85 feet to a point on the southwestern line of Kehrs Mill Road (variable width); thence along said southwestern line the following courses and distances: South 45 degrees 53 minutes 28 seconds East, a distance of 279.30 feet to a point; thence along a curve to the right having a radius of 545.00 feet, an arc length of 245.05 feet, and a chord which bears South 33 degrees 00 minutes 36 seconds East, a chord distance of 242.99 feet to a point; thence South 20 degrees 07 minutes 44 seconds East, a distance of 118.88 feet to a point; thence leaving said southwestern line, through the aforesaid St. Mary's Institute tract South 69 degrees 52 minutes

16 seconds West, a distance of 156.80 feet to a point; thence South 53 degrees 21 minutes 33 seconds West, a distance of 91.60 feet to a point; thence South 16 degrees 24 minutes 48 seconds West, a distance of 80.70 feet to a point; thence South 16 degrees 24 minutes 48 seconds West, a distance of 82.37 feet to a point; thence South 61 degrees 22 minutes 34 seconds West, a distance of 346.47 feet to a point; thence South 20 degrees 40 minutes 02 seconds East, a distance of 371.99 feet to a point; thence South 24 degrees 10 minutes 02 seconds East, a distance of 40.00 feet to a point; thence South 63 degrees 20 minutes 14 seconds West, a distance of 127.09 feet to a point; thence South 37 degrees 00 minutes 22 seconds East, a distance of 219.46 feet to a point on the northwestern line of Strecker Road; thence along said northwestern line of Strecker Road the following courses and distances: South 63 degrees 20 minutes 17 seconds West, a distance of 82.74 feet to a point; thence South 55 degrees 16 minutes 49 seconds West, a distance of 536.16 feet to a point on the southwestern line of a proposed street (69 feet wide); thence leaving the northwestern line of Strecker Road along said southwestern line of the proposed street North 37 degrees 18 minutes 23 seconds West, a distance of 319.11 feet to a point of curvature; thence along a curve to the left having a radius of 25.00 feet, an arc length of 39.91 feet to a point, and a chord which bears North 83 degrees 02 minutes 11 seconds West, a chord distance of 35.80 feet to a point of reverse curvature on the southeastern line of a proposed street (40 feet wide); thence along said southeastern line along a curve to the right having a radius of 640.00 feet, an arc length of 69.63 feet, and a chord which bears South 54 degrees 21 minutes 02 seconds West, a chord distance of 69.60 feet to a point; thence South 57 degrees 28 minutes 02 seconds West, a distance of 293.48 feet to a point; thence along a curve to the right having a radius of 540.00 feet, an arc length of 606.45 feet, and a chord which bears South 89 degrees 38 minutes 26 seconds West, a chord distance of 575.08 feet to a point; thence North 58 degrees 11 minutes 10 seconds West, a distance of 277.61 feet to a point of curvature; thence along a curve to the left having a radius of 35.00 feet, an arc length of 29.99 feet, and a chord which bears North 82 degrees 44 minutes 07 seconds West, a chord distance of 29.08 feet to a point of reverse curvature on the southeastern line of a proposed cul-de-sac; thence along a curve to the right having a radius of 49.00 feet, an arc length of 64.00 feet, and a chord which bears North 69 degrees 51 minutes 58 seconds West, a chord distance of 59.55 feet to a point; thence leaving the southeastern line of the cul-de-sac along the proposed lot line between proposed lots 17 and 18 South 57 degrees 33 minutes 08 seconds West, a distance of 250.51 feet to a point on the northeastern line of Church Road (variable width); thence along said northeastern line North 37 degrees 05 minutes 15 seconds West, a distance of 200.81 feet to a point; thence along a curve to the right having a radius of 75.00 feet, an arc length of 42.07 feet, and a chord which bears North 21 degrees 01 minutes 01 seconds West, a chord distance of 41.52 feet to a point; thence North 04 degrees 56 minutes 46 seconds West, a distance of 2.90 feet to a point; thence leaving said northeastern line along the northwestern line of the aforesaid St. Mary's Institute tract; North 52 degrees 55 minutes 10 seconds East, a distance of 578.94 feet to the Point of Beginning and containing (1,766,940 sq. ft. 40.563 acres).

Section 2. The preliminary approval, pursuant to the City of Chesterfield Zoning Ordinance is granted subject to all of the ordinances, rules and regulations and the specific conditions as recommended by the Planning Commission in its recommendations to the City Council.

Section 3. The City Council, pursuant to the petition filed by Michael Doster in P.Z. 05-2006 requesting the amendment embodied in this ordinance, and pursuant to the recommendations of the City of Chesterfield Planning Commission that said petition be granted and after public hearing, held by the Planning Commission on the 13th day of March 2006, does hereby adopt this ordinance pursuant to the power granted to the City of Chesterfield under Chapter 89 of the Revised Statutes of the State of Missouri authorizing the City Council to exercise legislative power pertaining to planning and zoning.

Section 4. This ordinance and the requirements thereof are exempt from the warnings and summons for violations as set out in Section 1003.410 of the zoning Ordinance of the City of Chesterfield.

Section 5. This ordinance shall be in full force and effect from and after its passage and approval.

Passed and approved this 20th day of November 2006.

MAYOR

ATTEST:

CITY CLERK

First Reading Held: 11/6/06

Planning Commission July 10, 2006 P.Z. 05-2006 Barry Simon Development
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ATTACHMENT A

In keeping with the following Comprehensive Plan policies, these conditions have been developed:

- 1.4 Quality New Development
2.1 Quality Residential Development
2.1.1 Conservation of Existing Quality of Life
2.1.5 Provide Buffer for Existing Residential Development
2.1.6 Reinforce Existing Residential Development Pattern
2.1.9 Encourage Planned Residential Development
2.1.12 Residential Subdivision Access

I. SPECIFIC CRITERIA

A. Information to be shown on the Site Development Concept Plan shall adhere to conditions specified under General Criteria-Concept Plan. Site Development Plans and Site Development Section Plans shall adhere to specific design criteria.

B. Definitions

- 1. A Site Development Concept Plan is a conceptual plan for development in a planned district being done in phases.
2. A Site Development Section Plan is a plan for development for sections of the overall concept plan.
3. A Site Development Plan is a plan for development in planned districts that is being done in one phase.

C. PERMITTED USES

- 1. The regulations of the "E-One Acre" Estate District shall be as follows:
a. Lots 18-33 as shown on Exhibit A shall be zoned "E-One Acre".
b. There shall be a maximum of sixteen (16) lots in this "E-One Acre" Estate District.
c. The minimum lot size shall be one half acre.

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2. The regulations of the "E-Two Acre" Estate District shall be as follows:

- a. Lots 1-17, 34, 35, and 36 as shown on Exhibit A shall be zoned "E-Two Acre"
b. There shall be a maximum of twenty (20) lots in this "E-Two Acre" District.
c. The minimum lot size shall be one (1) acre.
d. Lots 6, 7, 34, and 35 shall be a minimum of three (3) acres and shall have no more than one (1) home on each lot.

3. Non Single family uses, other than home occupations shall be prohibited in this development.

4. The minimum lot size for both the "E-One Acre" and "E-Two Acre" sections of this development shall be no smaller than the lot sizes provided on the attached Exhibit A.

D. FLOOR AREA, HEIGHT, BUILDING AND PARKING STRUCTURE REQUIREMENTS

1. HEIGHT

a. The maximum height of the single-family dwelling shall not exceed 50 feet.

2. BUILDING REQUIREMENTS

a. Openspace: Openspace includes all areas excluding the building or areas for vehicular circulation

A minimum of 84% openspace is required for this development.

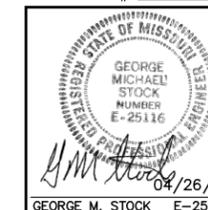
E. SETBACKS

1. STRUCTURE SETBACKS

No building or structure, other than a freestanding project identification sign, boundary and retaining walls, light standards, flag poles or fences will be located within the following setbacks:

- a. One hundred and sixty (160) feet from the right-of-way of Strecker Road on the southern boundary of the E-1 Acre District.
b. One hundred and ten (110) feet from the right-of-way of Kehrs Mill Road on the eastern boundary of the E-2 Acre District.

M.S.D. P# 27551-00
BASE MAP # 19U



- CITY COMMENTS 04/26/07
CITY COMMENTS 04/10/07
COUNTY RESUBMITTAL 03/29/07
AMEREN SUBMITTAL 03/28/07
LANDSCAPE COORDINATION 03/06/07
COUNTY COMMENTS 02/26/07
CITY COMMENTS 02/16/07
COUNTY COMMENTS 01/03/07
SWPPP PLAN 12/15/06

TUSCANY RESERVE
ORDINANCE



257 Chesterfield Business Parkway
St. Louis, MO 63005
PH. (636) 530-9100
FAX (636) 530-9130
e-mail: general@stockassoc.com
Web: www.stockassoc.com

- c. Forty (40) feet from the right-of-way of Church Road on the western boundary of the E-1 Acre District.
- d. Eighty (80) feet from the northern boundary with bearing N52°55'10"E.
- e. One hundred and forty (140) feet from the northern boundary with bearing S58°11'10"E.
- f. Two hundred (200) feet from the northern boundary with bearing N57°28'02"E.
- g. Two hundred (200) feet from the northern boundary with bearing N24°29'17"E.
- h. Two hundred (200) feet from the northern boundary with bearing N45°20'59"E.
- i. Fifty (50) feet from the boundary of this E-2 Acre district adjacent to the property owned by St. Mary's Institute.

2. LOT CRITERIA

In addition to the above-referenced requirements, no building or structure (other than boundary and retaining walls) light standards, flag poles or fences, shall be within the following setbacks:

For the "E-One Acre" District:

- a. Front yard setback: Twenty five (25) feet from the internal public right-of-way.
- b. Side yard setback: Twenty (20) feet from the side property line.
- (i.) A minimum of forty (40) feet must be maintained between structures.

- c. Rear yard setback: Twenty five (25) feet from the rear property line.

For the "E-Two Acre" District:

- a. Front yard setback: Twenty five (25) feet from the internal public right-of-way.
- b. Side yard setback: Twenty five (25) feet from the side property line.

- (i.) For lots 12-17 in the "E-2" District portion of this development to maintain a side yard setback of twenty (20) feet in lieu of the twenty five (25) foot requirement.
 - (ii.) A minimum of fifty (50) feet must be maintained between structures.
 - (iii.) For lots 12-17 in the "E-2" District portion of this development to maintain a distance between structures of forty (40) feet in lieu of the fifty (50) foot requirement.
- c. Rear yard setback:
- (i.) The foundation of the residential structure shall maintain a setback of one hundred and fifty (150) feet from the rear property line with bearings N45°20'59"E, N24°29'17"E, N57°28'02"E, and S58°11'10"E.
 - (ii.) Other structures such as pools, decks, or patios shall maintain a setback of one hundred and twenty five (125) feet from the above mentioned rear property line.

F. PARKING AND LOADING REQUIREMENTS

1. Parking and loading spaces for this development will be as required in the City of Chesterfield Code.
2. Construction Parking
 - a. The streets surrounding this development and any street used for construction access thereto shall be cleaned throughout the day. The developer shall keep the road clear of mud and debris at all times.
 - b. Provide adequate off-street stabilized parking area(s) for construction employees and a washdown station for construction vehicles entering and leaving the site in order to eliminate the condition whereby mud from construction and employee vehicles is tracked onto the pavement causing hazardous roadway and driving conditions.
 - c. Construction parking shall not be permitted on public-maintained roadways. Adequate off-street stabilized parking area(s) shall be provided for construction employees.

- d. Parking on non-surfaced areas shall be prohibited in order to eliminate the condition whereby mud from construction and employee vehicles is tracked onto the pavement causing hazardous roadway and driving conditions.

G. LANDSCAPE AND TREE REQUIREMENTS

1. The developer shall submit a landscape plan, tree stand delineation, and tree preservation plan which adheres to the Tree Manual of the City of Chesterfield Code.
2. A minimum thirty (30) foot wide landscape buffer strip shall be required around the perimeter of this development in accordance with the City of Chesterfield Tree Manual.
 - a. A fifty (50) foot wide landscape buffer strip shall be required along the northern boundary of the development.
 - b. Lots 7-16 shall contain heavily buffered landscape screening along the boundary of this E-2 district as shown on Exhibit A.
4. Landscaping in the right of way, if proposed, shall be reviewed by the City of Chesterfield Department of Public Works, and/or the St. Louis County Department of Highways and Traffic.

H. SIGN REQUIREMENTS

1. Ornamental Entrance Monument construction, if proposed, shall be reviewed by the City of Chesterfield, and the St. Louis County Department of Highways and Traffic, for sight distance considerations prior to installation or construction.
2. Signs shall be permitted in accordance with the regulations of the City of Chesterfield Code.

I. LIGHT REQUIREMENTS

Provide a lighting plan and cut sheet in accordance with the City of Chesterfield Code.

J. ARCHITECTURAL

1. The developer shall submit architectural elevations, including but not limited to, colored renderings and building materials.

2. Building facades should be articulated by using color, arrangement or change in materials to emphasize the facade elements. The planes of the exterior walls may be varied in height, depth or direction. Extremely long facades shall be designed with sufficient building articulation and landscaping to avoid a monotonous or overpowering appearance.
3. Trash enclosures: The location and elevation of any trash enclosures will be as approved by the Planning Commission on the Site Development Plan. All exterior trash areas will be enclosed with a six (6) foot high sight-proof enclosure complimented by adequate landscaping approved by the Planning Commission on the Site Development Plan. The material will be as approved by the Planning Commission in conjunction with the Site Development Plan.
4. Mechanical equipment will be adequately screened by roofing or other material as approved by the Planning Commission.

K. ACCESS/ACCESS MANAGEMENT

1. Streets and drives related to this development shall be designed and located in conformance with the Chesterfield Driveway Access Location and Design Standards, as originally adopted by Ordinance No. 2103 and as may be amended from time to time.
2. Access to this development shall be no more than one (1) street approach onto Strecker Road and one (1) street approach onto Kehrs Mill Road and constructed to St. Louis County standards as directed by the St. Louis County Department of Highways and Traffic.
3. The cul-de-sac lengths for this development may exceed the eight hundred (800) feet maximum as stated in the City of Chesterfield Subdivision Ordinance provided that the following conditions are met:
 - a. Emergency access shall be provided to Church Road.
 - b. The developer shall provide to the City of Chesterfield the remote control devices that shall allow emergency personnel access to the interior streets from Church Road.
 - c. A minimum of four (4) remote control devices shall be provided to the City of Chesterfield as directed.
 - d. Special key access shall be provided to the Monarch Fire Protection District.
4. If required site distance cannot be provided at the access locations, acquisition of right-of-way, reconstruction of pavement including acquisition to the vertical alignment and other off-site improvements may

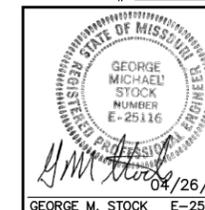
- be required to provide adequate sight distance as directed by the Saint Louis County Department of Highways and Traffic.
- 5. No direct access to Kehrs Mill Road, Strecker Road, or Church road will be permitted from any lot within the subdivision.

L. PUBLIC/PRIVATE ROAD IMPROVEMENTS, INCLUDING PEDESTRIAN CIRCULATION

1. Improve Church Road along the entire frontage of the site to 1/2 of a 60 foot right of way and up to a 32 foot wide pavements, including curb, required tapers, and storm drainage facilities, as directed by the Department of Public Works and/or the City of Wildwood.
2. Improve Strecker Road 1/2 of a 38 foot wide pavement along the entire frontage of the site, including curb, required tapers and storm drainage facilities, as directed by the Department of Public Works and/or the City of Wildwood.
3. Improve Kehrs Mill Road to one half of a eighty-four (84) foot right-of-way and a twenty-four (24) foot pavement with eight (8) foot shoulders and additional widening to provide a minimum thirty-three (33) foot wide road surface for a left turn lane with required tapers and including all storm drainage facilities as directed by the St. Louis County Department of Highways and Traffic.
4. Improvements along Church Road and Strecker Road shall be completed prior to the issuance of building permits exceeding sixty percent (60%) of the approved dwelling units. Delays due to utility relocation and/or adjustment, for which the developer is responsible monetarily, shall not constitute a cause to issue permits in excess of sixty percent (60%).
5. All roadway and related improvements in each plat or phase of the development shall be constructed prior to sixty percent (60%) occupancy of that plat or phase. All roadway and related improvements in the overall development shall be completed prior to eight five (85%) occupancy of the overall development.
6. Provide any additional right-of-way and construct any improvements to Kehrs Mill Road, as required by St. Louis County Highways and Traffic.
7. Provide a sidewalk conforming to St. Louis County ADA standards adjacent to Kehrs Mill Road or provide the finish grading therefore and required cash escrow, as directed by the St. Louis County Department of Highways and Traffic.

8. If roadways are designated to be private, these roadways within this development shall be private and remain private forever. Maintenance of private streets, including but not limited to, snow removal, shall be the responsibility of the developer/subdivision. In conformance with Section 1005.265 of the Subdivision Ordinance, a disclosure statement shall be provided to all potential buyers. In conformance with Section 1005.180 of the Subdivision Ordinance, the method for providing continuous maintenance of streets and appurtenant storm sewers shall be included in the trust indentures and the record plat.
9. Signage indicating that the streets are private and owners are responsible for maintenance shall be posted in conformance with Section 1005.180 of the Subdivision Ordinance. Said signage shall be posted within 30 days of the placement of the adjacent street pavements and maintained/replaced by the developer until such time as the subdivision trustees are residents of the subdivision, at which time the trustees shall be responsible.
10. Internal streets shall be constructed in accordance with Section 1005.180 of the Subdivision Ordinance of the City of Chesterfield.
11. If street grades in excess of 6 percent are desired, steep grade approval must be obtained. In no case shall slopes in excess of 12% be considered. Any request for steep street grades must include justification prepared, signed and sealed by a registered professional engineer and include plans, profiles, boring logs, cross-sections, etc. in accordance with the Street Grade Design Policy. The justification should clearly indicate site conditions and alternatives considered. If steep grades are approved for this site, a disclosure statement shall be provided to all potential buyers and a note indicating that priority snow removal will not be given to this site shall be included on the Site Development Plan and Record Plat.
12. Any request to install a gate at the entrance to this development must be approved by the City of Chesterfield and the St. Louis County Department of Highways and Traffic. No gate installation will be permitted on public right of way. A minimum stacking distance of 60 feet from any intersection and a turnaround for rejected vehicles designed to accommodate a single unit truck shall be provided in advance of the gate, as directed by the Department of Public Works and the St. Louis County Department of Highways and Traffic.
13. If a gate is installed on a street in this development, the streets within the development or that portion of the development that is gated shall be private and remain private forever. Maintenance of private streets, including snow removal, shall be the responsibility of the developer/subdivision. In conformance with Section 1005.265 of the Subdivision Ordinance, a disclosure statement shall be provided to all

M.S.D. P# 27551-00
 BASE MAP # 19U



- ▲ CITY COMMENTS 04/26/07
- ▲ CITY COMMENTS 04/10/07
- ▲ COUNTY RESUBMITTAL 03/29/07
- ▲ AMEREN SUBMITTAL 03/28/07
- ▲ LANDSCAPE COORDINATION 03/06/07
- ▲ COUNTY COMMENTS 02/26/07
- ▲ CITY COMMENTS 02/16/07
- ▲ COUNTY COMMENTS 01/03/07
- ▲ SWPPP PLAN 12/15/06

TUSCANY RESERVE
 ORDINANCE

STOCK & ASSOCIATES
Consulting Engineers, Inc.

257 Chesterfield Business Parkway
 St. Louis, MO 63005
 PH. (636) 530-9100
 FAX (636) 530-9130
 e-mail: general@stockassoc.com
 Web: www.stockassoc.com

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5. Provide open space percentage for overall development including separate percentage for each lot on the plan.
6. Provide Floor Area Ratio (F.A.R.).
7. A note indicating all utilities will be installed underground.
8. A note indicating signage approval is separate process.
9. Depict the location of all buildings, size, including height and distance from adjacent property lines and proposed use.
10. Specific structure and parking setbacks along all roadways and property lines.
11. Indicate location of all existing and proposed freestanding monument signs.
12. Zoning district lines, subdivision name, lot number, dimensions, and area, and zoning of adjacent parcels where different than site.
13. Floodplain boundaries.
14. Depict existing and proposed improvements within 150 feet of the site as directed. Improvements include, but are not limited to, roadways, driveways and walkways adjacent to and across the street from the site, and significant natural features, such as wooded areas and rock formations, that are to remain or be removed.
15. Depict all existing and proposed easements and rights-of-way within 150 feet of the site and all existing or proposed off-site easements and rights-of-way required for proposed improvements.
16. Indicate the location of proposed storm sewers, detention basins, sanitary sewers and connection(s) to the existing systems.
17. Depict existing and proposed contours at intervals of not more than 20 feet, and extending 150 feet beyond the limits of the site as directed.
18. Address trees and landscaping in accordance with the City of Chesterfield Code.
19. Provide a lighting plan in accordance with the City of Chesterfield Code.
20. Comply with all preliminary plat requirements of the City of Chesterfield Subdivision Ordinance.

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21. Signed and sealed in conformance with the State of Missouri Department of Economic Development, Division of Professional Registration, Missouri Board for Architects, Professional Engineers and Land Surveyors requirements.
 22. Provide comments/approvals from the appropriate Fire District, the Metropolitan St. Louis Sewer District, the St. Louis County Department of Highways and Traffic, and the Metropolitan St. Louis Sewer District.
 23. Compliance with Sky Exposure Plane.
- V. GRADING AND IMPROVEMENT PLAN REQUIREMENTS**
- A. A Site Development Plan and Tree Preservation Plan must be approved prior to issuance of a grading permit or approval of improvement plans.
 - B. Prior to approval of a grading permit or improvement plans, a Storm Water Pollution Prevention Plan (SWPPP) must be submitted and approved. The SWPPP shall address installation and maintenance of required erosion control practices specific to site conditions. The purpose of the SWPPP is to ensure the design, implementation, management and maintenance of Best Management Practices (BMPs) to control erosion and reduce the amount of sediment and other pollutants in storm water discharges associated with land disturbance activities, and ensure compliance with the terms and conditions stated in the Sediment and Erosion Control Manual.
 - C. No grading which results in a change in watersheds will be permitted.
 - D. If existing City maintained streets are to be used as construction access to this site, prior to approval of a grading permit or improvement plans, or any construction related traffic or delivery of any construction equipment to the site, the following items must be addressed:
 1. The travel route must be approved by the Department of Public Works. No deviation from the approved route will be permitted.
 2. An evaluation, including film record, of the current condition of the pavement on the approved travel route must be submitted.
 3. An appropriate bond must be submitted, as approved by the City of Chesterfield, to ensure that any damage to existing pavement is repaired. Repair of damage to existing streets will not be included in the subdivision escrow; a separate bond must be established.
 4. All plan sheets shall indicate that vehicle loads of construction traffic using this route are not to exceed 22,400 pounds axle load or 60,000

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- gross vehicle weight, and that no tri-axle trucks are to be used. Weight tickets may be used to determine conformance with this requirement.
5. Additional protective measures, as deemed necessary by the Department of Public Works, may also be required.
- E. Prior to grading permit or improvement plan approval, provide comments/approvals from the appropriate Fire District, St. Louis County Department of Highways and Traffic, and the Metropolitan St. Louis Sewer District.
 - F. Prior to approval of a grading permit or improvement plans, copies of recorded easements, including book and page of record, for all off-site work and off-site areas inundated by headwater from on-site improvements must be submitted.
- VI. TRUST FUND CONTRIBUTION**
- A. As this development is not subject to traffic generation assessment, the roadway improvements required herein represent the developer's road improvement obligation. These improvements will not exceed an amount established by multiplying the ordinance required parking spaces by the following applicable rates:
- | Type of Development | Required Contribution |
|---------------------|------------------------|
| Single Family | \$879.10/parking space |
- (Parking spaces as required by the City of Chesterfield Code.)
- If types of development differ from those listed, rates shall be provided by the St. Louis County Department of Highways and Traffic.
- As a portion of the improvements required herein are needed to provide for the safety of the traveling public, their completion as a part of this development is mandatory.
- Allowable credits for required roadway improvements will be awarded as directed by the St. Louis County Department of Highways and Traffic. Sidewalk construction and utility relocation, among other items, are not considered allowable credits.
- The amount of this required contribution, if not submitted by January 1, 2007 will be adjusted on that date and on the first day of January in each succeeding year thereafter in accordance with the construction cost index as determined by the St. Louis County Department of Highways and Traffic.
- VIII. RECORDING**

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 City Council November 6, 2006
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- Within 60 days of approval of any development plan by the City of Chesterfield, the approved Plan will be recorded with the St. Louis County Recorder of Deeds. Failure to do so will result in the expiration of approval of said plan and require re-approval of a plan by the Planning Commission.
- IX. VERIFICATION PRIOR TO SPECIAL USE PERMIT ISSUANCE
 Prior to any Special Use Permit issuance by the St. Louis County Department of Highways and Traffic, a special case escrow or special escrow supported by an Irrevocable Letter of Credit, must be established with the St. Louis County Department of Highways and Traffic to guarantee completion of the required roadway improvements.
 - X. VERIFICATION PRIOR TO RECORD PLAT APPROVAL
 The developer shall cause, at his expense and prior to the recording of any plat, the reestablishment, restoration or appropriate witnessing of all Corners of the United States Public Land Survey located within, or which define or lie upon, the outboundaries of the subject tract in accordance with the Missouri Minimum Standards relating to the preservation and maintenance of the United States Public Land Survey Corners.
 - XI. VERIFICATION PRIOR TO FOUNDATION OR BUILDING PERMITS
 - A. A grading permit or improvement plan approval is required prior to issuance of a building permit. In extenuating circumstances, an exception to this requirement may be granted.
 - B. All required subdivision improvements in each plat of a subdivision shall be completed prior to issuance of more than 85% of the building permits for all lots in the plat.
 - C. Prior to the issuance of foundation or building permits, all approvals from all applicable agencies and the Department of Public Works, as applicable, must be received by the City of Chesterfield Department of Planning.
 - D. Prior to issuance of foundation or building permits, all approvals from the City of Chesterfield, St. Louis County Department of Highways and Traffic and the Metropolitan St. Louis Sewer District must be received by the St. Louis County Department of Public Works.
 - E. Based on the preliminary plan, improvements to Kehrs Mill Road must be completed prior to issuance of the building permits in excess of 60% of the total. As previously noted, the delays due to utility relocation and adjustments will not constitute a cause to allow occupancy prior to completion of the required roadway improvements.

Planning Commission July 10, 2006 P.Z. 05-2006 Barry Simon Development
 Planning Commission August 28, 2006 Page 19 of 21
 City Council November 6, 2006
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XII. OCCUPANCY PERMIT/FINAL OCCUPANCY

- A. Prior to the issuance of any occupancy permit, floodplain management requirements shall be met.
- B. All lots shall be seeded and mulched or sodded before an occupancy permit shall be issued, except that a temporary occupancy permit may be issued in cases of undue hardship because of unfavorable ground conditions. Seed and mulch shall be applied at rates that meet or exceed the minimum requirements stated in the Sediment and Erosion Control Manual.
- C. Prior to final occupancy of any building, the developer shall provide certification by a registered land surveyor that all monumentation depicted on the record plat has been installed and United States Public Land Survey Corners have not been disturbed during construction activities or that they have been reestablished and the appropriate documents filed with the Missouri Department of Natural Resources Land Survey Program.

XIII. FINAL RELEASE OF SUBDIVISION DEPOSITS

Prior to final release of subdivision construction deposits, the developer shall provide certification by a registered land surveyor that all monumentation depicted on the record plat has been installed and United States Public Land Survey Corners have not been disturbed during construction activities or that they have been reestablished and the appropriate documents filed with the Missouri Department of Natural Resources Land Survey Program.

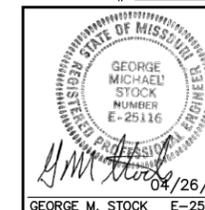
XIV. GENERAL DEVELOPMENT CONDITIONS

- A. Erosion and siltation control devices shall be installed prior to any clearing or grading and be maintained throughout the project until adequate vegetative growth insures no future erosion of the soil and work is accepted by the owner and controlling regulatory agency.
- B. General development conditions relating to the operation, construction, improvement and regulatory requirements to be adhered to by the developer are as follows:
 1. When clearing and/or grading operations are completed or will be suspended for more than five (5) days, all necessary precautions shall be taken to retain soil materials on site. Protective measures may include a combination of seeding, periodic wetting, mulching, or other suitable means.

Planning Commission July 10, 2006 P.Z. 05-2006 Barry Simon Development
 Planning Commission August 28, 2006 Page 20 of 21
 City Council November 6, 2006
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2. If cut and fill operations occur during a season not favorable for immediate establishment of permanent ground cover, unless alternate storm water detention and erosion control devices have been designed and established, a fast germinating annual, such as rye or sudan grasses, shall be utilized to retard erosion.
3. If cut or fill slopes in excess of the standard maximum of 3:1 horizontal run to vertical rise are desired, approval for the steeper slopes must be obtained from the Director of Public Works. Approval of steeper slopes is limited to individual and isolated slopes, rock dikes, undisturbed and stable natural slopes and slopes blending with the natural terrain. Design of the steep slopes must be performed by a registered professional engineer and include recommendations regarding construction methods and long-term maintenance of the slope. Any steep slope proposed on a Site Development Plan shall be labeled and referenced with the following note: *Approval of this plan does not constitute approval of slopes in excess of 3:1. Steep slopes are subject to the review and approval of the Director of Public Works. Review of the proposed steep slope will be concurrent with the review of the grading permit or improvement plans for the project.*
4. Soft soils in the bottom and banks of any existing or former pond sites or tributaries or any sediment basins or traps should be removed, spread out and permitted to dry sufficiently to be used as fill. This material shall not be placed in proposed public right-of-way locations or in any storm sewer location.
5. All fills placed under proposed storm and sanitary sewer lines and/or paved areas, including trench backfill within and off the road right-of-way, shall be compacted to 90% of maximum density as determined by the Modified AASHTO T-180 Compaction Test (ASTM D-1557) for the entire depth of the fill. Compacted granular backfill is required in all trench excavation within the street right-of-way and under all paved areas. All tests shall be performed concurrent with grading and backfilling operations under the direction of a geotechnical engineer who shall verify the test results.
6. Access/utility easements shall be required throughout the development. A continuous 15 foot wide rear yard easement shall be provided. At a minimum, a 10 foot wide utility/access easement shall be provided at every other lot line or break between structures, as directed.
7. Should the design of the subdivision include retaining walls that serve multiple properties, those walls shall be located within common ground or special easements, including easements needed for access to the walls.

M.S.D. P# 27551-00
 BASE MAP # 19U



- ▲ CITY COMMENTS 04/26/07
- ▲ CITY COMMENTS 04/10/07
- ▲ COUNTY RESUBMITTAL 03/29/07
- ▲ AMEREN SUBMITTAL 03/28/07
- ▲ LANDSCAPE COORDINATION 03/06/07
- ▲ COUNTY COMMENTS 02/26/07
- ▲ CITY COMMENTS 02/16/07
- ▲ COUNTY COMMENTS 01/03/07
- ▲ SWPPP PLAN 12/15/06

**TUSCANY RESERVE
 ORDINANCE**

STOCK & ASSOCIATES
Consulting Engineers, Inc.

257 Chesterfield Business Parkway
 St. Louis, MO 63005
 PH. (636) 530-9100
 FAX (636) 530-9130
 e-mail: general@stockassoc.com
 Web: www.stockassoc.com

Planning Commission July 10, 2006 P.Z. 05-2006 Barry Simon Development
 Planning Commission August 28, 2006 Page 21 of 21
 City Council November 6, 2006
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8. This development may require an NPDES permit from the Missouri Department of Natural Resources. NPDES permits are applicable to construction activities that disturb one or more acres.

XV. ENFORCEMENT

- A. The City of Chesterfield, Missouri will enforce the conditions of this ordinance in accordance with the Site Development Plan approved by the City of Chesterfield and the terms of this Attachment A.
- B. Failure to comply with any or all the conditions of this ordinance will be adequate cause for revocation of approvals/permits by reviewing Departments and Commissions.
- C. Non-compliance with the specific requirements and conditions set forth in this Ordinance and its attached conditions or other Ordinances of the City of Chesterfield shall constitute an ordinance violation, subject, but not limited to, the penalty provisions as set forth in the City of Chesterfield Code.
- D. Waiver of Notice of Violation per the City of Chesterfield Code.
- E. This document shall be read as a whole and any inconsistency to be integrated to carry out the overall intent of this Attachment A.

Name of Owner _____

In connection with a change of zoning for the following described property from _____

(prior zoning) to _____
 (present zoning)

PROPERTY DESCRIPTION

A tract of land being part of Lots 1 and 2 of the "Subdivision In Partition of the Schulz Estate" as made by Commissioners in Partition in the St. Louis Land Court Case No. 3, March Term 1860, a certified copy of which is recorded in Book 253 page 48 of the St. Louis City records; Being Adjusted Lot 8 of "St. Mary's/McGee Boundary Adjustment Plat", as recorded in Plat Book 342 page 7 of the St. Louis County records; being part of Lot D of "McKenna Pacland Lake", a subdivision according to the plat thereof recorded in Plat Book 187 page 70 of the St. Louis County records and shown on "Boundary Adjustment Plat and Easement Plat of Lot D of McKenna Pacland Lake and U.S. Survey 124 and Fractional Section 18, Township 45 North - Range 4 East" as recorded in Plat Book 202 page 4 of the St. Louis County records; lying partly in U.S. Survey 124, Township 45 North - Range 3 East and Township 45 North - Range 4 East, in U. S. Survey 886 and Fractional Section 18, Township 45 North - Range 4 East, St. Louis County, Missouri and being more particularly described as:

Beginning at the intersection of the Northwest line of U.S. Survey 124 with the Range line between Township 45 North - Range 3 East and Township 45 North - Range 4 East; said point being also the Southwest corner of Lot C of aforesaid "McKenna Pacland Lake"; thence Southeastwardly along the Southwest line of Lots C and D of "McKenna Pacland Lake" South 58 degrees 11 minutes 10 seconds East 579.24 feet to the most Southwestern point on the new parcel line shown on the "Boundary Adjustment Plat and Easement Plat of Lot D of McKenna Pacland Lake and U.S. Survey 124 and Fractional Section 18, Township 45 North - Range 4 East"; thence Northeastwardly along said last mentioned line North 57 degrees 28 minutes 02 seconds East 461.85 feet to the most Western corner of Adjusted Lot 8 of aforesaid "St. Mary's/McGee Boundary Adjustment Plat"; thence Northwardly along the West line of said Adjusted Lot 8 North 24 degrees 29 minutes 17 seconds East 694.09 feet to the most Southern corner of property now or formerly of Robert J. Liebe, Jr., Trustee and Linda C. Liebe, Trustee, as described in deed recorded in Book 10265 Page 921 of the St. Louis County Records; thence Northeastwardly along the Southeast line of said Liebe property North 45 degrees 20 minutes 59 seconds East 970.85 feet to a point in the Southwest line of Kehrs Mill Road, as widened; said point being also the most Western corner of Parcel No. 5 conveyed to St. Louis County by deed recorded in Book 11262 Page 2349 of the St. Louis County Records; thence in a Southeastwardly direction along the right-of-way line of Kehrs Mill Road the following courses and distances: South 45 degrees 53 minutes 28 seconds East 279.30 feet, along a curve to the right whose radius point bears South 44 degrees 06 minutes 32 seconds West 545.00 feet from the last mentioned point, a distance of 245.05 feet and South 20 degrees 07 minutes 44 seconds East 199.08 feet to a point in the North line of a parcel described in deed to St. Louis County by deed recorded in Book 10125 Page 1744 and re-recorded in Book 10337 Page 2047 of the St. Louis County Records; thence continuing along the Kehrs Mill Road right-of-way and said widening parcel South 69 degrees 52 minutes 16 seconds West 5.00 feet and South 20 degrees 07 minutes 44 seconds East 10.67 feet to the Northeast corner of a parcel conveyed to St. Louis County by deed recorded in Book 12356 Page 298 of the St. Louis County Records; thence Westwardly along the North line of said last mentioned parcel South 69 degrees 52 minutes 16 seconds West 6.00 feet to the Northwest corner thereof; thence leaving said right-of-way line South 69 degrees 52 minutes 16 seconds West 281.67 feet to a point; thence South 16 degrees 24 minutes 48 seconds West 82.77 feet to a point; thence South 61 degrees 22 minutes 24 seconds West 346.47 feet to a point; thence South 20 degrees 40 minutes 02 seconds East 371.99 feet to a point; thence South 24 degrees 10 minutes 02 seconds East 40.00 feet to a point; thence South 63 degrees 20 minutes 14 seconds West 127.09 feet to a point in the Northeast line of aforesaid Adjusted Lot 8 of "St. Mary's/McGee Boundary Adjustment Plat"; thence Southeastwardly along said Northeast line South 37 degrees 00 minutes 22 seconds East 219.46 feet to a point in the Northwest line of Strecker Road, varying width; thence Southwestwardly along said Northwest line the following courses and distances: South 63 degrees 20 minutes 17 seconds West 82.74 feet, South 55 degrees 16 minutes 49 seconds West 830.16 feet and South 52 degrees 35 minutes 48 seconds West 958.92 feet to its intersection with the Northeast line of Church Road, varying width; thence Northwestwardly along said Northeast line the following courses and distances: North 37 degrees 05 minutes 15 seconds West 1,256.58 feet, along a curve to the right whose radius point bears North 52 degrees 54 minutes 45 seconds East 75.00 feet from the last mentioned point, a distance of 42.07 feet and North 04 degrees 56 minutes 46 seconds West 2.90 feet to a point in the aforesaid Northwest line of U.S. Survey 124; thence Northeastwardly along said Northwest line of U.S. Survey 124, being also along the Southeast lines of property now or formerly of Robert Thompson and wife as described in deed recorded in Book 4106 page 239 of the St. Louis County records and property now or formerly Clifford Frazier and wife by deed recorded in Book 5933 page 179 of the St. Louis County records, North 52 degrees 55 minutes 10 seconds East 578.94 feet to the point of beginning.

_____, the owner(s) of the property shown on this plan for and in consideration of being granted a permit to develop property under the provisions of Chapter 1003 _____ of City of Chesterfield Ordinance #624, do hereby agree and declare that said property from the date of recording this plan shall be developed only as shown thereon, unless said plan is amended by the Planning Commission, or voided or vacated by order of ordinance of the City of Chesterfield Council.

(Signature): _____
 (Name Typed): _____

State of _____)
) SS.
 County of _____)

On this ____ day of _____, A.D., 20____, before me personally appeared _____

_____, to me known, who, being by me sworn in, did say that he/she is the _____ of the _____ (Title) _____ (Name of Corporation)

a corporation in the State of _____, and that the seal affixed to the foregoing instruments is the corporate seal of said corporation, and that said instrument was signed on behalf of said corporation by authority of its Board of Directors, and the said _____

(Officer of Corporation) _____, acknowledged said instrument to be the free act and deed of said corporation.

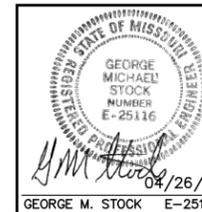
This Site (Development) Plan was approved by the City of Chesterfield Planning Commission and duly verified on the ____ day of _____, 20____, by the Chairperson of said Commission, authorizing the recording of this Site Plan pursuant to Chesterfield Ordinance Number 200, as attested to by the Director of Planning and the City Clerk.

 Director of Planning

 City Clerk

- ▲ CITY COMMENTS 04/26/07
- ▲ CITY COMMENTS 04/10/07
- ▲ COUNTY RESUBMITTAL 03/29/07
- ▲ AMEREN SUBMITTAL 03/28/07
- ▲ LANDSCAPE COORDINATION 03/06/07
- ▲ COUNTY COMMENTS 02/26/07
- ▲ CITY COMMENTS 02/16/07
- ▲ COUNTY COMMENTS 01/03/07
- ▲ SWPPP PLAN 12/15/06

M.S.D. P# 27551-00
 BASE MAP # 19U

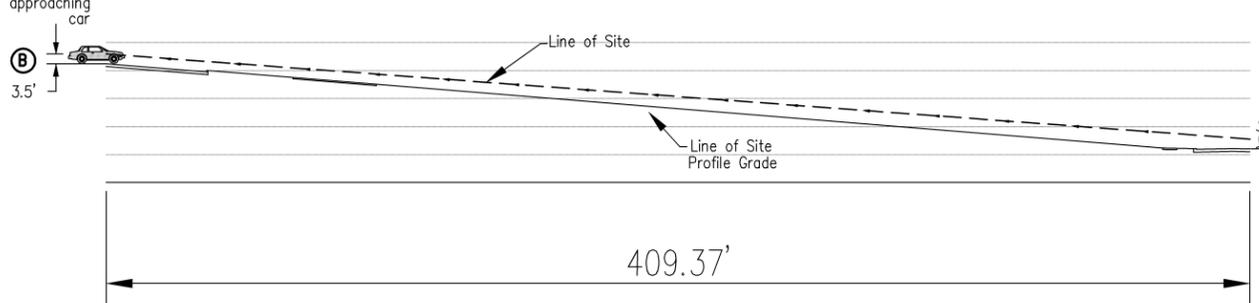
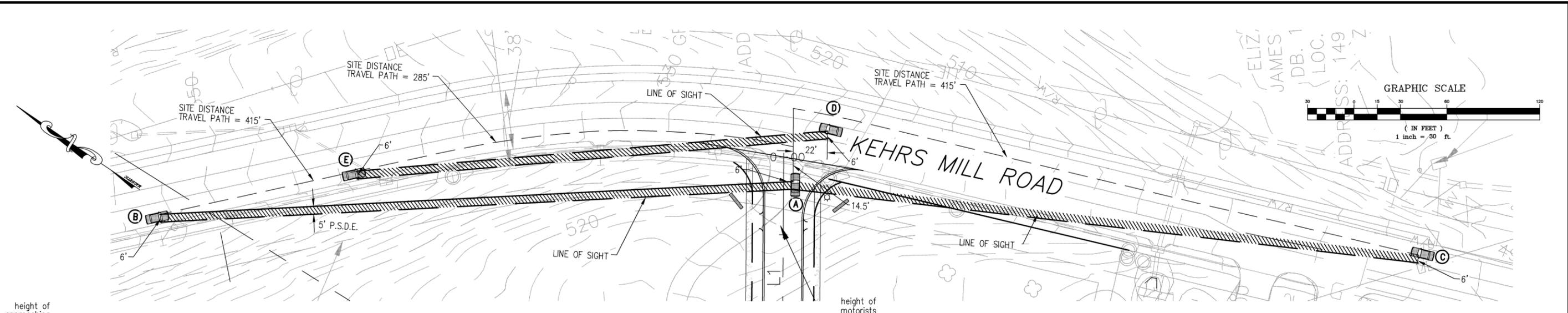


TUSCANY RESERVE
 ORDINANCE

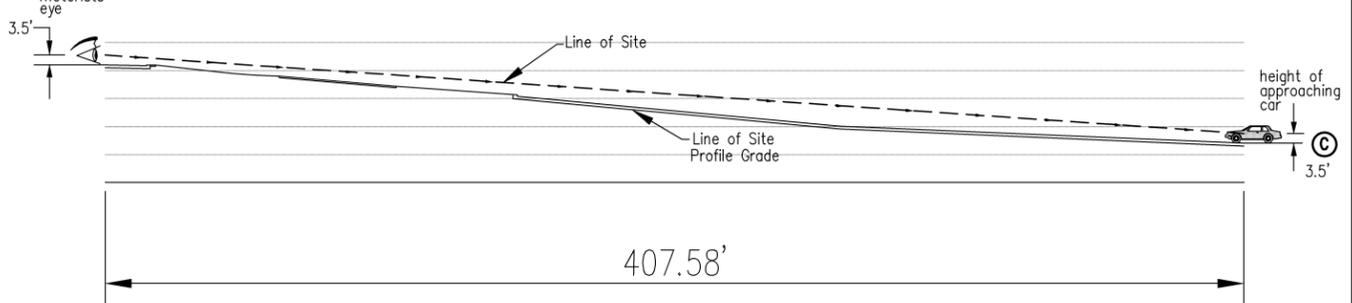
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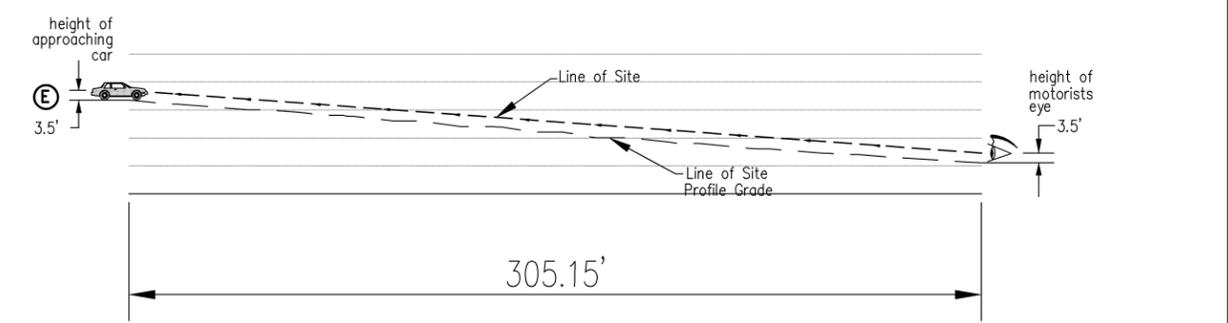
DATE: 12/12/06	CHECKED BY: P.R.G.	DATE: 12/12/06	CHECKED BY: C.A.M.	DATE: 12/12/06	CHECKED BY: 206-3933	SHEET: 10 OF 12
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**CAR "A" LOOKING LEFT
LINE OF SITE PROFILES**



**CAR "A" LOOKING RIGHT
LINE OF SITE PROFILES**



**CAR "D" MAKING LEFT HAND TURN
LINE OF SITE PROFILES**

9-15-2005
4025-1
SITE DISTANCE USED

Design Speed (M.P.H.)	Sight Distance for Left Turn & Through from Minor Road ("A" & "B")			Sight Distance for Left Turn from Major Road ("C")		
	"M" = 1 Lane	"M" = 2 Lanes	"M" = 3 Lanes	"M" = 1 Lane	"M" = 2 Lanes	"M" = 3 Lanes
15	110'	180'	190'	125'	135'	145'
20	225'	240'	250'	165'	180'	195'
25	290'	295'	315'	205'	225'	240'
30	335'	355'	375'	245'	265'	290'
35	390'	415'	440'	285'	310'	335'
40	445'	475'	500'	325'	355'	385'
45	500'	530'	565'	365'	400'	430'
50	555'	590'	625'	405'	445'	480'
55	610'	650'	690'	445'	490'	530'
60	665'	710'	750'	490'	535'	575'
65	720'	765'	815'	530'	575'	625'
70	775'	825'	875'	570'	620'	670'

GENERAL NOTES

- Do not scale drawing. Follow dimensions.
- Sight distance design criteria shall be based upon the number of lanes of the ultimate roadway.
- On existing roadways the design speed shall be the design speed of the roadway or the posted speed limit, whichever is greater.
- On new roadways the design speed shall be 5 m.p.h. greater than the anticipated posted speed limit.
- If "M" is greater than 3 lanes sight distance shall be increased by interpolating from values of the table.
- If "M" includes a median, sight distance data shall be increased by interpolating between values shown on the chart.
- Height of motorist eye in stopped vehicle = 3.5ft. Height of the object = 3.5ft.
- Sight distance requirements shall be specified looking left and right for a motorist in each lane of a side street or driveway.
- Sight distance requirement for left turning motorist on the through roadway attempting to enter a side street or driveway are measured from a vehicle stopped condition.
- All sight distances assume grades of 3 percent or less. If greater see AASHTO Geometric Design of Highways and Streets.
- Sight distance should be met for signalized intersection.

TYPICAL PROFILE ALONG LINE OF SIGHT TO THE LEFT

Visual Height of vehicle = 3.5'

Height of motorist eye = 3.5'

Accurate Grade

Measured along driving path

"A", "B", or "C"

Line of Sight to the Left (See Note #1)

Line of Sight for left turning motorist entering side street or driveway (See Note #1)

P.S.D.E.

14.5' Minimum

F.C. or L.P.

15'

"M" = total number of through and left hand lanes approaching intersection from the left.

"A" = Sight distance to the left for a motorist in each lane of a side street or driveway. Measured along driving path of nearest lane of approaching motorist.

"B" = total number of lanes to be traversed to make a right turn.

"C" = Sight distance to the right for a motorist in each lane of a side street or driveway. Measured along driving path of nearest lane approaching motorist.

"D" = Sight distance required for motorist turning left from major roadway. Measured along driving path of farthest lane of approaching motorist.

PERMITS: Permit Sight Distance (60801)

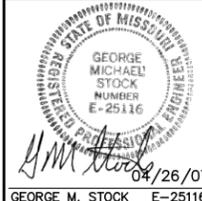
ST. LOUIS COUNTY
DEPARTMENT OF HIGHWAYS AND TRAFFIC
CLAYTON, MISSOURI

SIGHT DISTANCE AT INTERSECTIONS

REVISION DATE: January 10, 2006

POSTED SPEED = 30 MPH
DESIGN SPEED = 35 MPH

M.S.D. P# 27551-00
BASE MAP # 19U



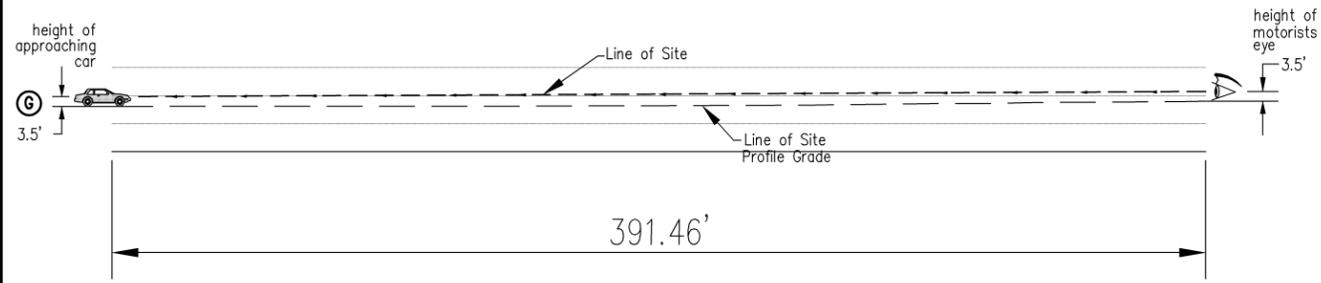
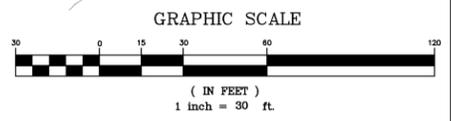
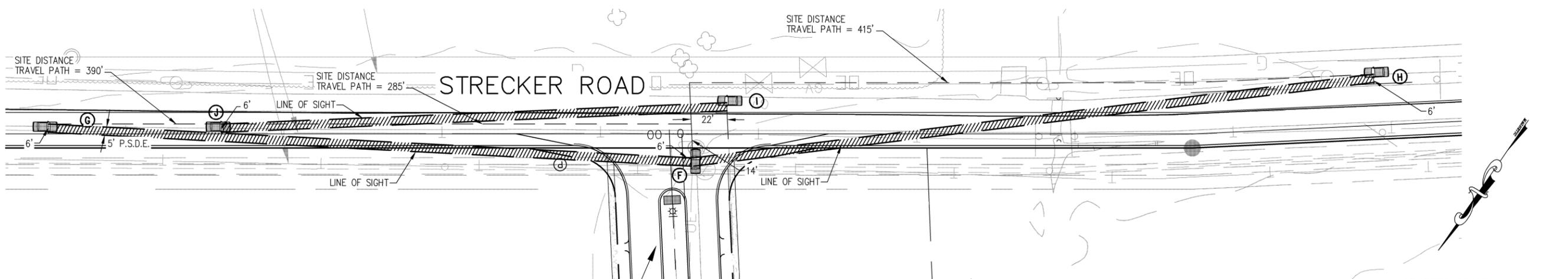
- △ CITY COMMENTS 04/10/07
- △ COUNTY RESUBMITTAL 03/29/07
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- △ LANDSCAPE COORDINATION 03/06/07
- △ COUNTY COMMENTS 02/26/07
- △ CITY COMMENTS 02/16/07
- △ COUNTY COMMENTS 01/03/07
- △ SWPPP PLAN 12/15/06

TUSCANY RESERVE
SITE DISTANCE - KEHRS MILL ROAD

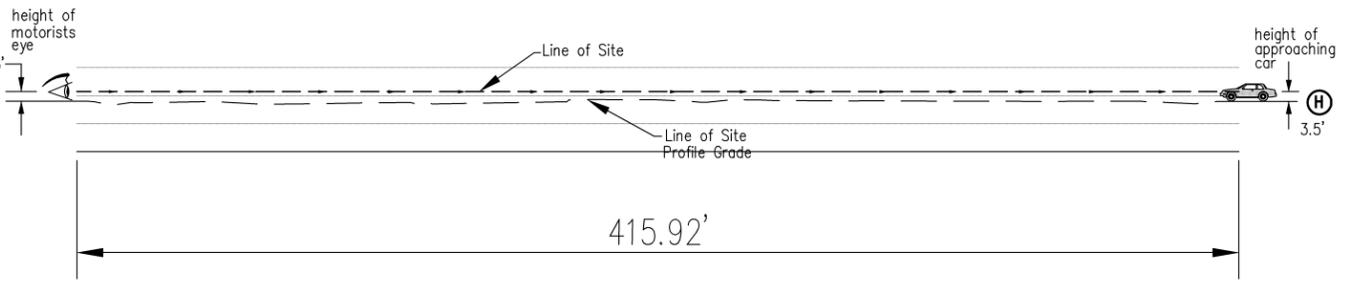
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PH. (636) 530-9100
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e-mail: general@stockassoc.com
Web: www.stockassoc.com

DRAWN BY: P.R.G. DATE: 12/12/06
CHECKED BY: C.A.M. DATE: 12/12/06
JOB NUMBER: 206-3933
SHEET: 11 OF 12



**CAR "F" LOOKING LEFT
LINE OF SITE PROFILES**

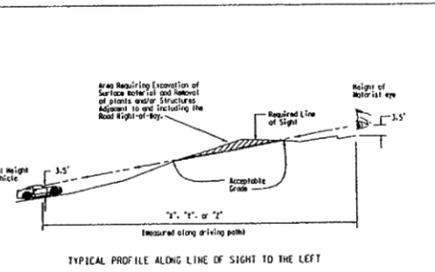


**CAR "F" LOOKING RIGHT
LINE OF SITE PROFILES**

9-15-2005

4025-1

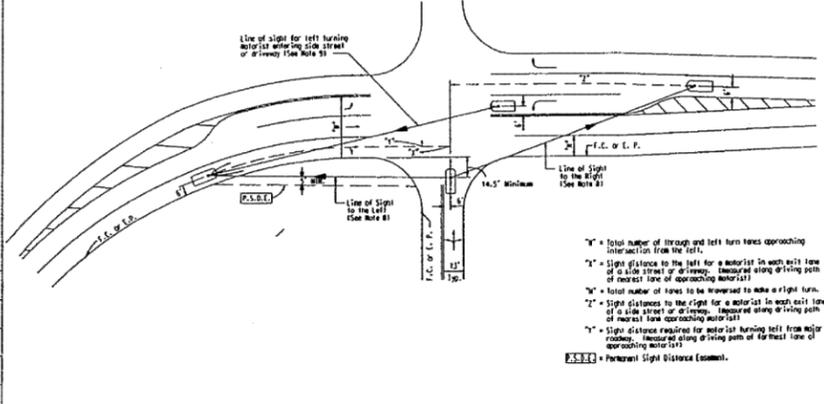
Design Speed (M.P.H.)	Sight Distance for Left Turn & Through from Minor Road ("A" & "I")			Sight Distance for Left Turn from Major Road ("J")			Sight Distance for Right Turn from Minor Road ("H")		
	"M" = 1 Lane	"M" = 2 Lanes	"M" = 3 Lanes	"M" = 1 Lane	"M" = 2 Lanes	"M" = 3 Lanes	"M" = 1 Lane	"M" = 2 Lanes	"M" = 3 Lanes
15	110'	180'	190'	125'	135'	145'	145'		
20	225'	240'	250'	165'	180'	195'	195'		
25	290'	295'	315'	205'	225'	240'	240'		
30	335'	335'	375'	245'	265'	290'	290'		
35	390'	415'	440'	285'	310'	335'	335'		
40	445'	475'	500'	325'	355'	385'	385'		
45	500'	530'	565'	365'	405'	430'	430'		
50	555'	590'	625'	405'	445'	480'	480'		
55	610'	650'	690'	445'	490'	530'	530'		
60	665'	710'	750'	490'	530'	575'	575'		
65	720'	765'	815'	530'	575'	625'	625'		
70	775'	825'	875'	570'	620'	670'	670'		



SAINT LOUIS COUNTY PROJ. 27551-00

GENERAL NOTES

- Do not scale drawing. Follow dimensions.
- Sight distance design criteria shall be based upon the number of lanes of the ultimate roadway.
- On existing roadways the design speed shall be the design speed of the roadway or the posted speed limit, whichever is greater.
- On new roadways the design speed shall be 5 m.p.h. greater than the unclassified posted speed limit.
- If "M" is greater than 3 lanes sight distance shall be increased by interpolating from values on the chart.
- If "M" includes a median, sight distance data shall be increased by interpolating between values shown on the chart.
- Height of motorist eye in stopped vehicle = 3.5ft. Height of the object = 3.5ft.
- Sight distance requirements shall be specified looking left and right for a motorist in each side lane of a side street or driveway.
- Sight distance requirement for left turning motorist on the through roadway intending to enter a side street or driveway are measured from a vehicle stopped condition.
- All sight distance assume grades of 3 percent or less. If greater see AASHTO Geometric Design of Highways and Streets.
- Sight distance should be met for signalized intersection.



"M" = total number of through and left turn lanes approaching intersection from the left.

"L" = sight distance to the left for a motorist in each side lane of a side street or driveway. Measured along driving path of nearest lane of approaching motorist.

"R" = total number of lanes to be traversed to make a right turn.

"S" = sight distance to the right for a motorist in each side lane of a side street or driveway. Measured along driving path of nearest lane approaching motorist.

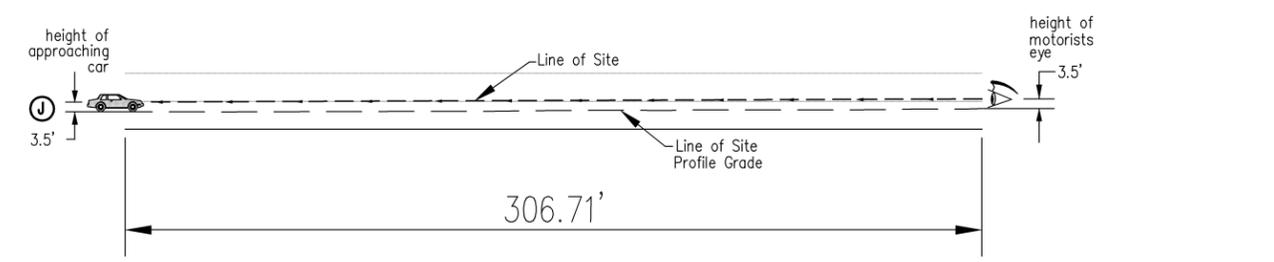
"T" = sight distance required for motorist turning left from major roadway. Measured along driving path of farthest lane of approaching motorist.

SEE 3.03.03 - Permanent Sight Distance (ES&M).

SAINT LOUIS COUNTY
DEPARTMENT OF HIGHWAYS AND TRAFFIC
CLAYTON, MISSOURI

**SIGHT DISTANCE
AT INTERSECTIONS**

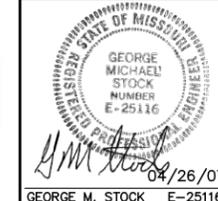
REVISION DATE: JANUARY 10, 2006



**CAR "I" MAKING LEFT HAND TURN
LINE OF SITE PROFILES**

- △ COUNTY COMMENTS 02/28/07
- △ CITY COMMENTS 04/10/07
- △ COUNTY RESUBMITTAL 03/29/07
- △ AMEREN SUBMITTAL 03/28/07
- △ LANDSCAPE COORDINATION 03/06/07
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- △ COUNTY COMMENTS 01/03/07
- △ SWPPP PLAN 12/15/06

M.S.D. P# 27551-00
BASE MAP # 19U



TUSCANY RESERVE
SITE DISTANCE - STRECKER ROAD

STOCK & ASSOCIATES
Consulting Engineers, Inc.

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St. Louis, MO 63005
PH. (636) 530-9100
FAX (636) 530-9130
e-mail: general@stockassoc.com
Web: www.stockassoc.com

DRAWN BY: P.R.G. DATE: 12/12/06 CHECKED BY: C.A.M. DATE: 12/12/06 JOB NUMBER: 206-3933 SHEET: 12 OF 12

POSTED SPEED = 30 MPH
DESIGN SPEED = 35 MPH

DRAWING FILE: C:\WORK\2006\2063933\2063933.dwg LAYOUT: 12_SiteDistance-Strecker PLOTTED: Apr 26, 2007 - 12:08pm PLOTTED BY: jvdgany

"TUSCANY RESERVE"

A TRACT OF LAND LYING PARTLY IN U.S. SURVEY 124,
TOWNSHIP 45 NORTH - RANGE 3 EAST, AND
TOWNSHIP 45 NORTH - RANGE 4 EAST, IN U.S. SURVEY 866 AND
FRACTIONAL SECTION 18, TOWNSHIP 45 NORTH - RANGE 5 EAST,
CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI

Upland Forest* areas (Sample Points 1 & 2)

1. Acreage: 15.9 ac. (4-1/2) total
2. Forest Type: Upland Forest - typical mature woods
3. Species: Oak, (primarily) black and white oak plus other oak varieties, are the dominant tree species, with a much smaller number of hickory, sycamore, and poplar. Other species include some persimmon, dogwood, and a few redbud, with an occasional sugar maple or black walnut.
4. Density: 100 trees per acre (based upon average of typical sample plots)
5. Average Diameter: 12.5" DBH average (based upon average of 2-30" x 30" typical sample areas)
6. Ave. overstory DBH: 16.2" DBH average (based upon 12 total DBH trees divided by 28 trees)
7. Dominant species: Approx. 40% of stand is oak
8. Understory: Trees include dogwood, redbud and some poplar, with an occasional cherry or sweetgum. Shrubs include cornus and Virginia creeper, with occasional areas of raspberry and mulberry rose. Groundcovers include large areas of ivy, apple and ferns, and scattered areas of poison ivy, lily fern and mosses, with other shade-tolerant herbaceous plants and asters.
9. Valuable stands: None
10. Apparent health: Generally good, except that many of the large older trees along top of Ridge have some deadwood and broken crowns.

Bottomland Forest* area (NE corner of Church & Strecker)

- Note: No sample plots in this area, but see Monarch Tree List and evaluation as follows.
1. Acreage: 1.7 ac. (1/2) total
 2. Forest Type: "Bottomland Forest" - generally the woods area in Flood plain along Church Street.
 3. Species: Silver maple appears to be the dominant tree species, followed closely by cottonwood and beech. Other common species include sycamore, ash and elm. There is also a group of trees.
 4. Density: Estimated at about 50 trees per acre (generally more sparse than normal due to the nature of the area - previous logging and pasture)
 5. Average Diameter: Not measured - this area is not typical forest area
 6. Ave. overstory DBH: Not measured - this area is not typical forest area
 7. Dominant species: Approx. 20% of stand is silver maple, with 20% being cottonwood.
 8. Understory: Trees include very small oaks, beech, ash and maple.
 9. Valuable stands: None
 10. Apparent health: Fair

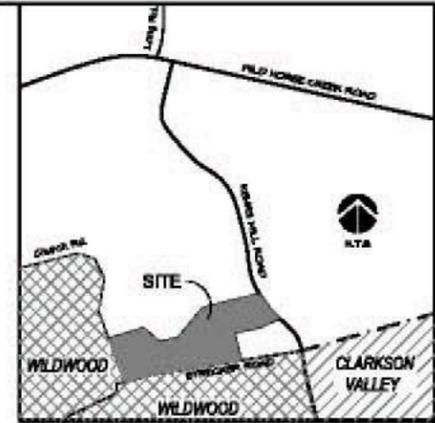
Sample Points:

Sample Point	Area Type	Distance	Area Type	Distance	Area Type	Distance
SP. 1	17' oak	12'	30' oak	40'	18' oak	10'
	18' oak	18'	20' oak	18'	18' oak	18'
	12' oak	40'	18' oak	10'	14' oak	30'
	17' oak	30'	18' oak	10'		
SP. 2	14' oak	18'	18' oak	30'	18' oak	10'
	12' oak	18'	30' oak	30'	18' oak	10'
	17' oak	30'	30' oak	30'	30' oak	30'

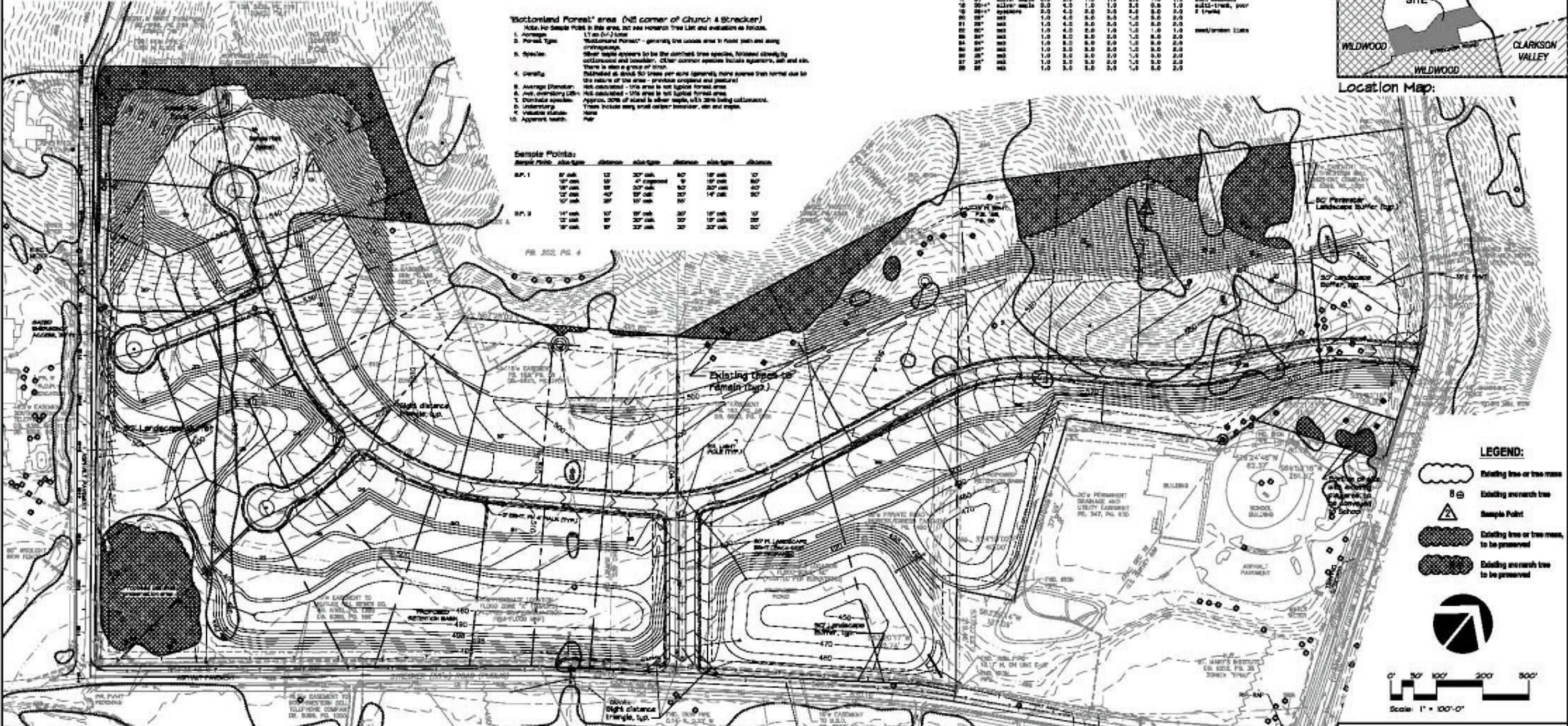
Monarch Trees

1 = Fair, 2 = Fair, 3 = Good, 4 = Excellent

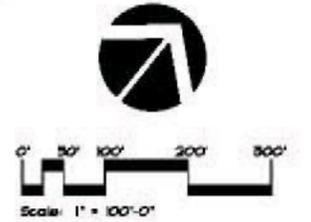
No.	DBH	Type	Height	DBH	Health	Quality	Notes	Other	Comments
1	28"	oak	1.0	4.0	3.0	2.0	1.0	3.0	2.0
2	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
3	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
4	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
5	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
6	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
7	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
8	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
9	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
10	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
11	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
12	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
13	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
14	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
15	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
16	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
17	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
18	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
19	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
20	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
21	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
22	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
23	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
24	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
25	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
26	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
27	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0
28	24"	oak	1.0	3.0	3.0	2.0	1.0	3.0	2.0



Location Map:



- LEGEND:**
- Existing line or tree mass
 - Existing monarch tree
 - Sample Point
 - Existing line or tree mass to be preserved
 - Existing monarch tree to be preserved



Tree Stand Delineation Notes:

1. 150 study method: The Aerial Photography (AP) to study method of preserving the Tree Stand Delineation was used.
2. Aerial photo: Provided by the Barton Map Company, Inc., Job no. 06-1-8204, exposure no. 028 63 86, Vernon 2004, scale 1" = 200'
3. Flood plain: Flood plain is shown along the Strecker Road frontage of the site, per 1981 Flood Insurance Rate Map, map number 22402C04H, with effective date of 8-28-76.
4. Vegetation types: 15.9 ac. (4-1/2) Upland Forest area
1.7 ac. (1/2) Bottomland Forest area
5. Monarch trees: Overstory trees (44 DBH) have been approximately located.
6. There were 2 Sample Points measured, as shown on Plan and as described elsewhere.
7. Two distinct vegetation types were found, as follows:
a. Upland Forest* (Sample Points 1 & 2)
b. Bottomland Forest* (NE corner of Church & Strecker)
8. Other observations:
a. Most of site is open and has been used as farmland (pasture and/or crops)
b. An area has been cleared for farming, trees are generally at the perimeter of the open area, in landscape areas, and on slope areas (trees not suitable to farm).

Tree Preservation:

1. Size of Tract: 56.9 acres (4-1/2)
2. Developed Portion: 86 single family detached lots
3. Existing tree canopy: 10.2 acres (1-1/2)
4. Trees required to be retained: 4.8 acres (20% of existing tree canopy)
5. Trees proposed to be retained: 5.3 acres (about 20% of existing tree canopy)

Open Space Calculation:

Open Area: 56.9 ac. site
- 2.80 ac. 219824
- 8.40 ac. 1/2 acre 4-1/2 ac. to be lots
= 45.70 ac. open space
= 47.02 ac. open space
= 85.12 ac. site

Note that some lots could have larger houses and/or more driveway tree space. A retention of 20% open space will be provided.

PRELIMINARY PLAN DISCLAIMER:

This drawing/plan has been developed using records or available survey, topographic, drainage and utility information. The design of the site improvements, including final grades, sewers, drainage, utilities and paving is strictly preliminary, having been based upon such available information. The proposed improvements are subject to revision during final engineering design, agency approval and review by utility companies. This drawing is not for construction.

HALL & HALSEY ASSOCIATES, INC.

Prepared For:

SIMON DEVELOPMENT, INC.
602 Trade Center Blvd.
Suite A
Chesterfield, MO 63005
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HALL & HALSEY ASSOCIATES, INC.
LAND PLANNING
LANDSCAPE ARCHITECTURE
424 SOUTH CLAY AVENUE, ST. LOUIS, MO 63122 314.996.5677

Drawn by: RAH
Project Number: 05038
Date: 01-31-06
Scale: 1 of 1
Revised: 02-21-06, 12-31-07, 1-10-07, 5-3-07

LEGEND:

- Existing tree or tree canopy
- Existing research tree
- Sample Point for TSD
- Existing tree or tree canopy to be preserved
- Existing research tree to be preserved
- Tree Preservation Fence line and Root Pruning Line (from canopy spread)
- 3/8" Rebar Control Line

"TUSCANY RESERVE"

A TRACT OF LAND LYING PARTLY IN U.S. SURVEY 124, TOWNSHIP 45 NORTH - RANGE 3 EAST, AND TOWNSHIP 45 NORTH - RANGE 4 EAST, IN U.S. SURVEY 886 AND FRACTIONAL SECTION 16, TOWNSHIP 45 NORTH - RANGE 5 EAST, CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI

General Notes:

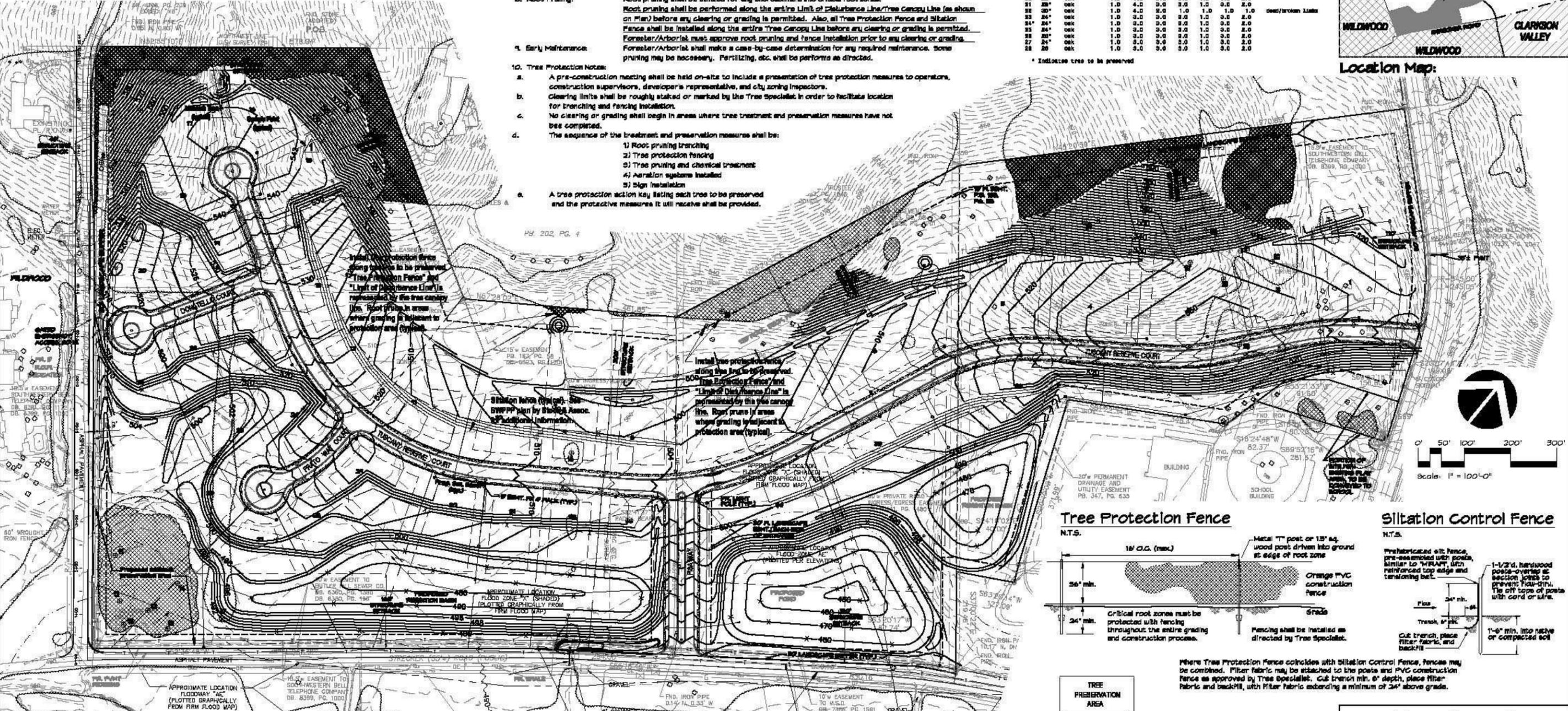
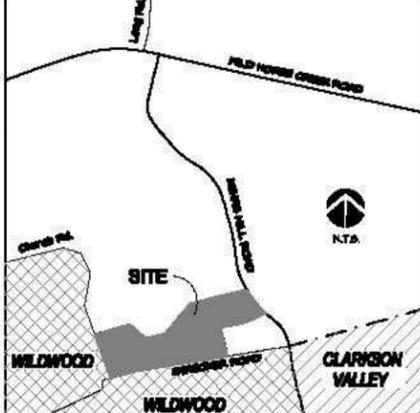
1. Subject Tract & Zoning:
2. Proposed Development:
3. Base Information:
4. Hooded Area Locations:
5. Tree Specialist:
6. Utility Corridors:
7. Critical Root Zones:
8. Root Pruning:
9. Early Maintenance:
10. Tree Protection Notes:

58.19 acres (1/4-), zoned "E-1" & "E-2" Estate District, per Ordinance 2522
 58 single family homes on individual lots
 Electronic files for "Site Development Plan" dated 3-14-07, by Stock & Associates (586/580-1100). Canopy was established based upon field inspection, and using aerial photographs. See Tree Stand Delineation Plan, prepared by Hall & Halsey Associates, Inc. originally dated 1-21-06 (plus revisions). A Tree Specialist was utilized in the preparation of the Tree Preservation Plan and will be used during the site construction, all as required by City Ordinance. The Tree Specialist is as follows:
Forestry Consultant Services, 4921 Menorah Drive, St. Louis, MO 63126, 314/244-2795
 Unless specifically noted, all sanitary and storm sewers are to be approximately centered within a 10' wide easement. All other utilities shall be located within the road RCM in front of the proposed homes. No special utility installation procedures are necessary or required.
 For planning purposes, CAZ's shall be considered to be the drip line and/or edge of vegetation unless otherwise noted. Forester/Arborist shall evaluate each area in the field before construction and location of tree protection lines, and shall increase protection area if deemed necessary.
 Root pruning shall be utilized for any encroachment into critical root zones.
 Root pruning shall be performed along the entire Limit of Disturbance Line/Tree Canopy Line (as shown on Plan) before any clearing or grading is permitted. Also, all Tree Protection Fence and Siltation Fence shall be installed along the entire Tree Canopy Line before any clearing or grading is permitted. Forester/Arborist must approve root pruning and fence installation prior to any clearing or grading.
 Forester/Arborist shall make a case-by-case determination for any required maintenance. Some pruning may be necessary. Fertilizing, etc. shall be performed as directed.

Monarch Trees
 1 = Pear, 2 = Pear, 3 = Oak, 4 = Buckeye

No.	DBH	Type	Adapt.	DBH	Health	Canopy	Roots	Value	Other	Comments
1	28"	oak	1.0	4.0	3.0	3.0	1.0	3.5	2.0	
2	24"	oak	1.0	3.0	3.0	3.0	1.0	3.0	2.0	
3	24"	oak	1.0	3.0	3.0	3.0	1.0	3.0	2.0	
4	24"	oak	1.0	3.0	3.0	3.0	1.0	3.0	2.0	
5	28"	oak	1.0	4.0	3.0	3.0	1.0	2.0	2.0	1/2 dead, crown v. poor
6	24"	oak	1.0	3.0	3.0	3.0	1.0	3.0	2.0	
7	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
8	24"	oak	1.0	3.0	3.0	3.0	1.0	3.0	2.0	
9	24"	oak	1.0	3.0	3.0	3.0	1.0	3.0	2.0	
10	24"	oak	1.0	3.0	3.0	3.0	1.0	3.0	2.0	
11	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
12	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
13	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
14	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
15	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
16	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
17	24"	oak	1.0	3.0	3.0	3.0	1.0	3.0	2.0	
18	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
19	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
20	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
21	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
22	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
23	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
24	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
25	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
26	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	
27	24"	oak	1.0	3.0	3.0	3.0	1.0	3.0	2.0	
28	28"	oak	1.0	4.0	3.0	3.0	1.0	3.0	2.0	

* Indicate tree to be preserved



Tree Stand Delineation Notes:

1. TSD study method: The Aerial Photography (AP) to study method of preparing the Tree Stand Delineation was used. Provided by the Northern Map Company, Inc. Job no. 05-1-026, easement no. 028 83-56, March 2004, scale 1" = 200'
2. Aerial photos: Flood plain is present along the Menorah Flood frontage of the site, per FEMA Flood Insurance Rate Map, map number 24160Z-00-14, with effective date of 8-25-05.
3. Flood Plain: 49.9 ac. (1/4) upland Forest area, 1.3 ac. (1/4) Bottomland Forest area, 1.2 ac. (1/4) total tree canopy area.
4. Vegetation types: 58.19 ac. (1/4) total Forest area, 1.3 ac. (1/4) total tree canopy area.
5. Monarch trees (hardwood trees 24" DBH) have been approximately located.
6. There were 2 Sample Points recorded, as shown on Plan and as described elsewhere.
7. Tree Stand Delineation types were found, as follows:
 - a. "Upland Forest" (Sample Point 1 & 2)
 - b. "Bottomland Forest" (NE corner of Church & Stricker)
8. Other observations:
 - a. Most of site is open and has been used as farmland (pasture and/or crops).
 - b. An area has been cleared for farming. Trees are generally at the perimeter of the open area. In addition, areas, and on slope areas (not suitable to farm).

Tree Preservation:

1. Site of Tract: 58.19 acres (1/4-)
2. Development Proposed: 58 single family detached lots
3. Existing tree canopy: 15.3 acres (1/4-)
4. Trees required to be retained: 4.8 acres (33% of net existing tree canopy)
5. Trees proposed to be retained: 8.2 acres (about 54% of existing tree canopy)
6. Trees proposed to be removed: 10.0 acres

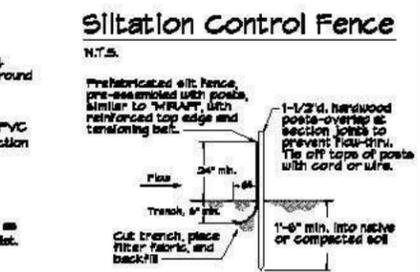
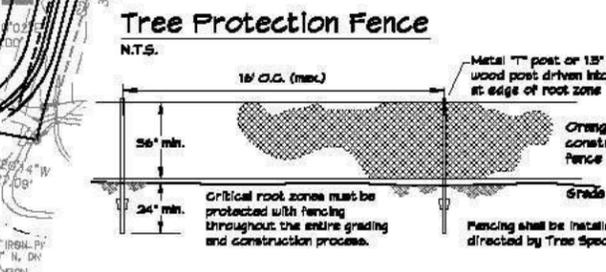
Note that calculations are based upon available information and preliminary engineering. All calculations are subject to obtaining an accurate boundary and topographic survey, and final engineering design being prepared. Final plans may show more or less trees being saved. As a minimum, there shall be at least 50% of the existing trees retained. Calculations and floodable information is from available records, provided by the steering company and Stock & Assoc. Base information subject to actual survey.

Open Space Calculation:

Open Area: 58.19 ac. site
 - 2.00 ac. streets
 - 0.91 ac. est. noise & drive in lots (0.19 ac/lot)
 46.88 ac. open space / 58.19 ac. x 100 = 80.7% Open Space

Vegetation Description:

1. "Upland Forest" area: northern portion of site
 18.8 ac. (1/4) total
 "Upland Forest" - typical mature woods. Oaks, primarily black and white oak plus other oak varieties, are the dominant tree species, with a much smaller number of Hickory (shagbark and pignut). Other species include some persimmon, dogwood, and a few redbud, with an occasional sugar maple or black walnut. Trees include dogwood, redbud and some pawpaw, with an occasional cherry or sassafras. Shrubs include cornus and Virginia creeper, with occasional areas of raspberry and hollyhock. Groundcovers include large areas of rag apple and ferns, and scattered areas of poison ivy, tickle and roseau, with other areas-tolerant herbaceous plants and wildflowers. Generally good, except that many of the large older trees along top of ridges have some deadwood and broken crowns.
2. "Bottomland Forest" area: (NE corner of Church & Stricker)
 1.1 ac. (1/4) total
 "Bottomland Forest" - generally the woods area in flood plain and along drainage. Other trees appear to be the dominant tree species, followed closely by cottonwood and boxelder. Other common species include sycamore, ash and elm. There is also a group of birch. Trees include many small calliper boxelder, etc and maple.
3. Open Space
4. Understory:
5. Apparent health:



Tree Specialist:
 FORESTRY CONSULTANT
 SERVICES
 4921 Menorah Drive
 St. Louis, MO 63126
 314/244-2795

Prepared For:
 SIMON DEVELOPMENT, INC.
 632 Trade Center Blvd.
 Chesterfield, MO 63005
 636/937-0700
 636/937-0725 fax

"Tuscany Reserve"
 Tree Preservation Plan

HALL & HALSEY ASSOCIATES, INC.
 LAND PLANNING
 LANDSCAPE ARCHITECTURE
 424 SOUTH CLAY AVENUE, ST. LOUIS, MO 63122 314.988.8577

Drawn by: RAH
 Checked by: RAH
 Date: 12-31-06
 Project Number: 05038
 Sheet Number: 1 of 1
 Revision: 1-10-07, 3-4-07, 3-20-07



"TUSCANY RESERVE"

A TRACT OF LAND LYING PARTLY IN U.S. SURVEY 124, TOWNSHIP 45 NORTH - RANGE 3 EAST, AND TOWNSHIP 45 NORTH - RANGE 4 EAST, IN U.S. SURVEY 886 AND FRACTIONAL SECTION 18, TOWNSHIP 45 NORTH - RANGE 3 EAST, CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI

Greenspace Calculation

Area A/Via: 56.75 ac. site
 - 3.55 ac. streets
 - 76 ac. sidewalk
 - 8.42 ac. top house, drive & patio & deck
 = 41.18 ac. green area and all non-paved surfaces
 41.18 ac. green area and all non-paved surfaces = 0.66 or 66% greenspace
 56.75 ac. site - 0.18 ac. sidewalk

General Notes:

- Subject Tract & Zoning: 56.75 acres (±), zoned R-11 & R-12 (Bate District), per Ordinance 2002
- Proposed Development: 56 single family homes on individual lots
- Base information, engineering design, boundary, topography, and other site information is from electronic files for the 2004 Development Plan dated 3-14-07, by Stock & Associates (066/00-1000)
- Various utilities have been installed around the project site that may not be shown on the drawings. Any utilities shown have been plotted from available records and must be considered approximate. The contractor has the responsibility to notify all utility companies a minimum 48 hours prior to construction to have existing utilities field located. Call 1-800-CBS-NOTE.
- The plantings shown shall in no way affect or alter intended surface runoff or drainage. Plants and other materials shall be installed in a manner consistent with positive surface drainage.
- No landscaping shall be installed within 5' of curbs of public streets, 25' of street lights, within eight (8) feet of intersections, or within 10' of street inlets or manholes, unless otherwise directed or approved by City.
- A 50' wide landscape buffer strip is required around the perimeter of the development, excepting along the northern side where a 50' buffer planting is proposed. As much of the north perimeter of the site is generally existing wooded area, only planting shown in these areas are where existing trees are being removed and/or if development is in close proximity to an existing home.
- Street trees shall be installed along all subdivisions streets, at the ratio of not less than one tree for each 40 linear feet of street frontage. Actual spacing of street trees may be adjusted due to driveway locations, street lights, manholes, etc., but the required planting ratio must be achieved.

Street Tree requirements are as follows:

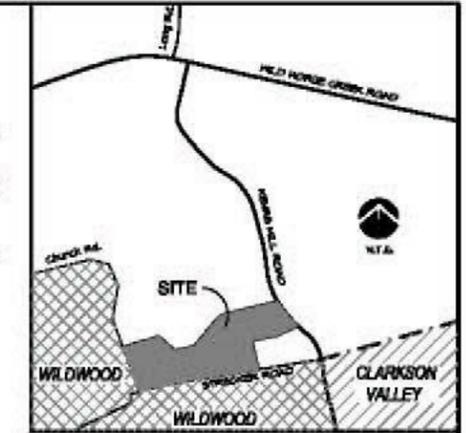
- There are about 1601 linear feet of street frontage, not including eight (8) triangle frontage at intersections. At the requirement of 1 tree per 40 L.F. of street frontage, 143 street trees are required.
 - Planting requirements:
 - Trees should be no closer than 25' to a street light.
 - Trees should be a minimum of 10 feet from street inlets or manholes.
 - At street intersections, trees should not be within the sight triangles formed by a line connecting two points 34' from the intersection of the projection of the pavement lines.
 - Tree locations should be adjusted to avoid other physical features, such as driveways, etc.
 - Trees shall be installed in the street right-of-way. In areas with sidewalks, trees shall be installed about midway between the back of curb and sidewalk. Where there are no sidewalks, trees shall be located with a similar relationship to the back of curb (5'-4' from back of curb).
 - A minimum of 40 percent of total of street trees in plant may be of one species. Trees shall be minimum of 2 1/2" caliper.
- Plant selections shall be as follows:
- All trees shall be selected from the "Recommended Tree List" in the Tree Manual.
 - All trees within 5' of existing or proposed RCM shall be taken from the "Recommended Street Tree List" in the Tree Manual.
 - A variety of trees must be utilized so that there is a mix of tree species, growth rate, and tree size.
 - A minimum of 30% of the trees must be of a species with a slow or medium growth rate.
 - Where more than 50 trees are installed, a variety of tree species within each category of "Deciduous", "Evergreen" and "Ornamental" must be utilized. Each category must provide a minimum of 20% of the total trees being planted (see Planting Schedule).

Planting Schedule:

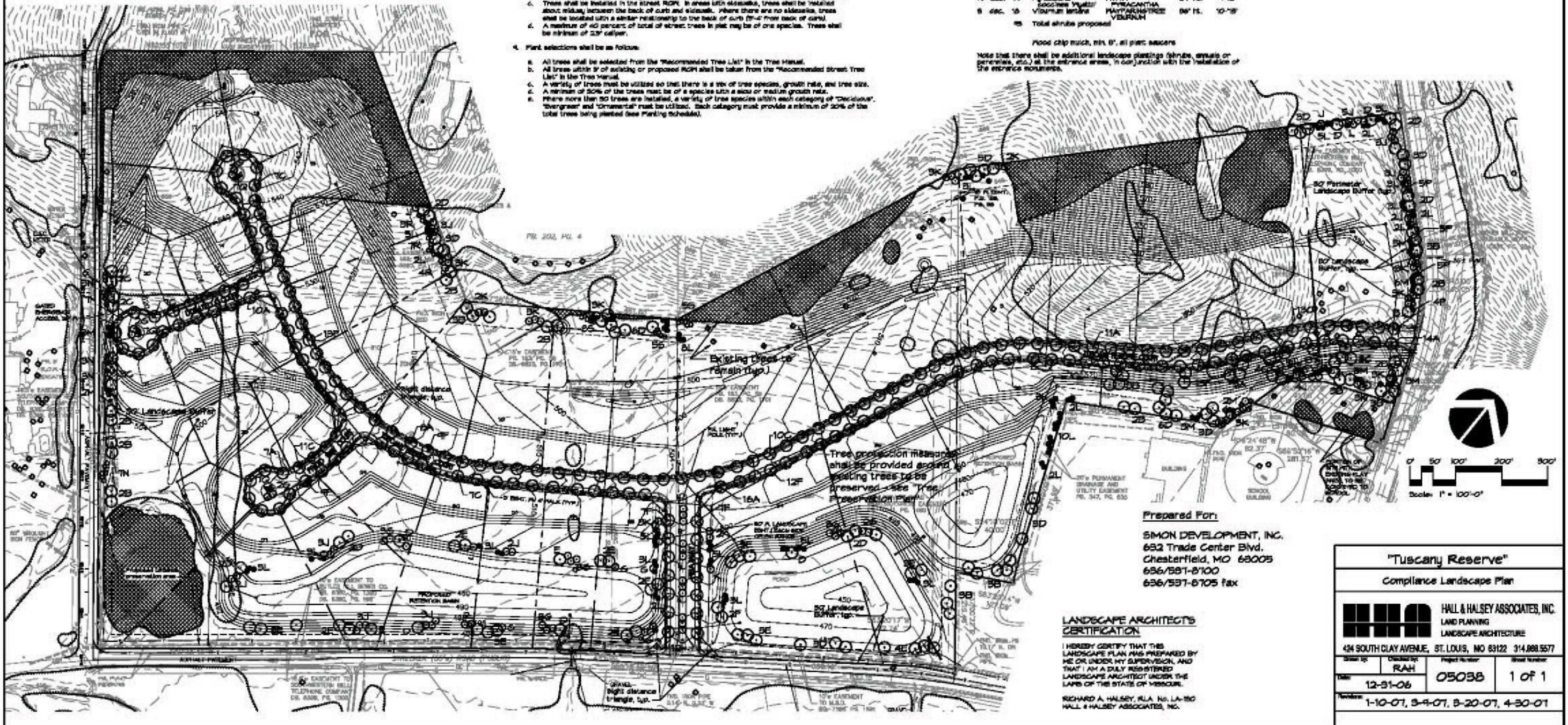
Code	Type	Qty	Botanical Name	Common Name	Size	Mature Ht.	Growth Rate
Deciduous (canopy) Trees - 504 total (50% of trees)							
A	dec.	66	Acer rubrum 'Fraxinoides'	FRANKS RED MAPLE	2.5' cal.	48'	F (60 are street trees)
B	dec.	41	Acer saccharum	SUGAR MAPLE	2.5' cal.	40'	S/M
C	dec.	77	Fraxinus americana	WHITE ASH	2.5' cal.	48'	M/F (32 are street trees)
D	dec.	50	Quercus nigra	RED OAK	2.5' cal.	48'	M/F
E	dec.	34	Taxodium distichum	BALDIPYRESS	2.5' cal.	48'	M
F	dec.	74	Zelkova serrata 'Green Vase'	GREEN VASE ZELKOVA	2.5' cal.	48'	M/F (61 are street trees)
Ornamental (flowering) Trees - 118 total (20% of trees)							
G	dec.	26	Debutia nigra	RIVER BRIGH	2.5' cal. / 50'-65'	17'	M/F
H	dec.	13	Corylus betula 'Fastigiata'	EUROPEAN UPRIGHT HORNBEAM	2.5' cal.	35'-40'	S
J	dec.	56	Cercis canadensis	EASTERN REDBUD	2.5' cal. / 25'-30'	9'	F
K	dec.	04	Cornus florida	FL. DOGWOOD	2.5' cal.	15'-25'	S/M
Evergreen Trees - 124 total (20% of trees)							
L	evg.	77	Pinus resinosa	RED PINE	6' Ht.	48'	M
M	evg.	52	Picea glauca	WHITE SPRUCE	6' Ht.	50'-60'	M
500 Total trees proposed							
Shrubs							
N	dec.	25	Aronia arbutifolia 'Briarlinea'	RED CHOKERBERRY	30' Ht.	6'-10'	
P	dec.	14	Cornus mas	DOGWOOD DOGWOOD	36' Ht.	20'-25'	
Q	dec.	21	Ilex glabra 'Wintergreen'	SHRUB DOGWOOD	36' Ht.	4'-6'	
R	dec.	14	Pyracantha 'Socome's Hybrid'	HYACINTH THORN	24' Ht.	4'-12'	
S	dec.	15	Viburnum lentago	WINTERBURN	36' Ht.	10'-15'	
25 Total shrubs proposed							

Flood chip much, min. 6", all plant sources

Note that there shall be additional landscape plantings (shrubs, annuals or perennials, etc.) at the entrance areas, in conjunction with the installation of the entrance monuments.



Location Map:



Prepared For:

SMON DEVELOPMENT, INC.
 632 Trade Center Blvd.
 Chesterfield, MO 63005
 636/591-8100
 636/591-8705 fax

LANDSCAPE ARCHITECT'S CERTIFICATION

I HEREBY CERTIFY THAT THIS LANDSCAPE PLAN WAS PREPARED BY ME OR UNDER MY SUPERVISION, AND THAT I AM A DULY REGISTERED LANDSCAPE ARCHITECT UNDER THE LAWS OF THE STATE OF MISSOURI.

RICHARD A. HALSEY, R.L.A. No. LA-180
 HALL & HALSEY ASSOCIATES, INC.

"Tuscany Reserve"

Compliance Landscape Plan

HALL & HALSEY ASSOCIATES, INC.
 LAND PLANNING
 LANDSCAPE ARCHITECTURE

424 SOUTH CLAY AVENUE, ST. LOUIS, MO 63122 314.868.5577

Drawn by: RAH
 Checked by: RAH
 Project Number: 05038
 Sheet Number: 1 of 1

Date: 12-31-06

Revised: 1-10-07, 3-4-07, 3-20-07, 4-30-07



Belle Meade - Elevation B



Bridal Spur - Elevation B



RICE ASSOCIATES

Mooreland - Elevation C



VII. I.

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

May 9, 2007

Planning Commission
City of Chesterfield
690 Chesterfield Parkway West
Chesterfield, MO 63017

The agenda for the May 14, 2007 Planning Commission will include the following:

Re: Chesterfield Commons Six Lot 3 (Chick-Fil-A): Amended Sign Package for a "C-8" Planned Commercial District located on the north side of Chesterfield Airport Road and 1,450 feet west of Boones Crossing.

Dear Commission Members:

Clayton Signs, on behalf of Chick-Fil-A, is requesting an amendment of the Sign Package for the above-reference development to allow this restaurant to maintain a pre-sale menu board sign in addition to other permitted signage.

REQUEST

The request involves the addition of the following signage:

- One 4'3" tall sign for use as a pre-sale menu. The current sign package for the site does not permit this use. The approved Site Development Plan for Chick-Fil-A contains illustrates its location, however, it can not be approved for Municipal Zoning Approval to receive a building permit because the package does not include this type of sign.

BACKGROUND

- The subject tract was zoned "C-83" Planned Industrial via St. Louis County Ordinance 13,933.
- On May 17, 2004, City of Chesterfield Ordinance 2096 was passed. Said ordinance repealed Ordinance 13,933 and added the drive-thru facilities as a permitted use for restaurants located on out parcels and increased the height of the light standards on the site. In addition, it added the criteria that a sign package be submitted for the overall development.
- Said sign package was approved by the Commission on September 13, 2004.

- The Site Development Section Plan for Chick-Fil-A was approved on September 11, 2006.

ACTION REQUESTED

The Department has reviewed the request to amend the Sign Package for Chesterfield Commons North (Commons Six) to permit a pre-sale menu board sign for Chick-Fil-A and requests action by the Planning Commission.

Respectfully Submitted,

A handwritten signature in black ink that reads "Annissa G. McCaskill-Clay". The signature is written in a cursive, flowing style.

Annissa G. McCaskill-Clay, AICP
Assistant Director of Planning

SPECIFICATIONS

CABINET

ALUMINUM CONSTRUCTION CABINET
WITH HINGED LEXAN COVERS

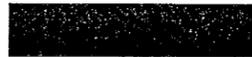
FACES

FACES ARE ACRYLIC WITH CLEAR WINDOWS
FOR COLOR TRANSPARENCIES PROVIDED
BY OTHERS

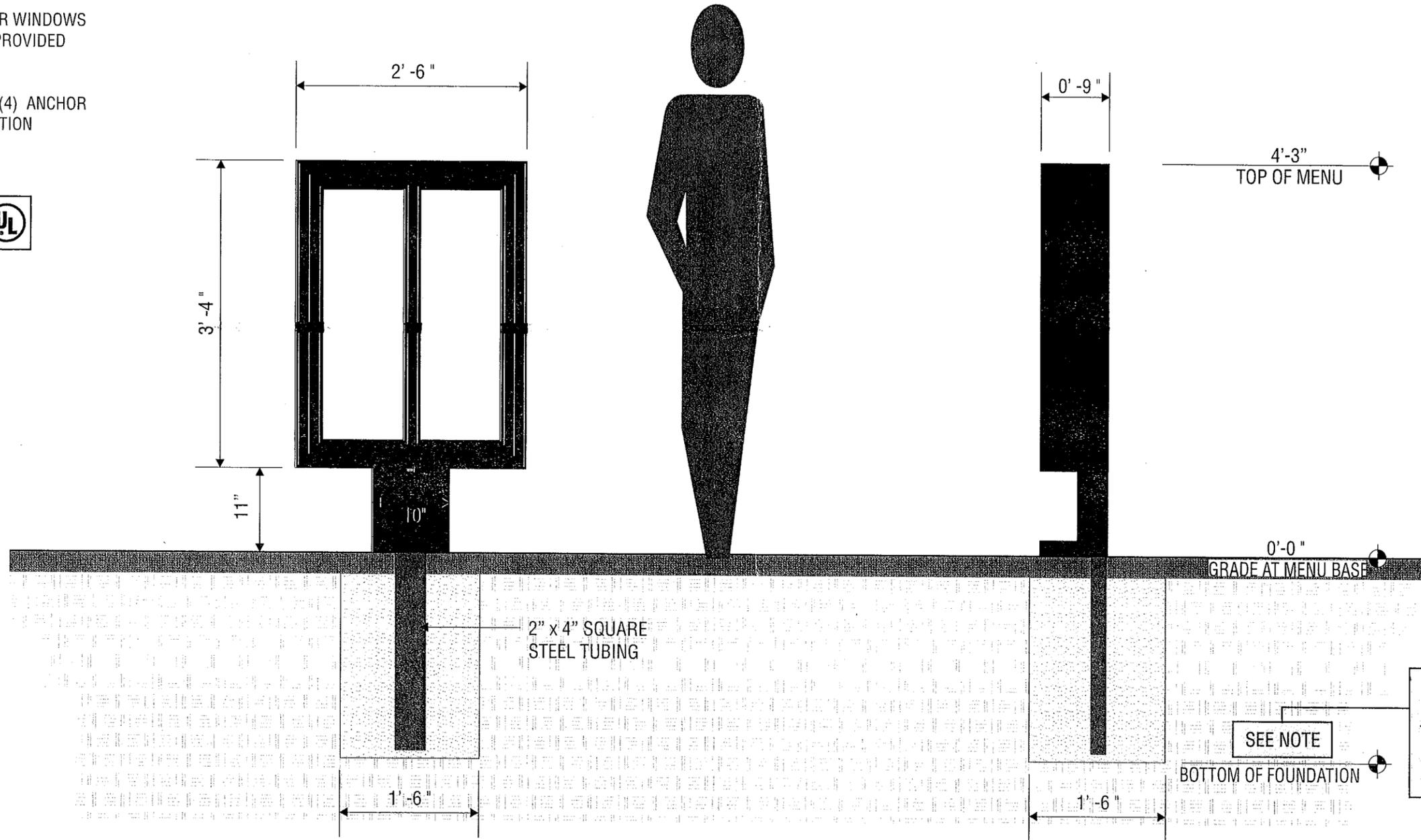
FOUNDATION

SIGN IS INSTALLED WITH FOUR (4) ANCHOR
BOLTS INTO CONCRETE FOUNDATION

This Product is Listed by
UNDERWRITERS LABORATORIES, INC.
and Bears the Mark:



METAL SURFACES
GRIPGARD #503 RED GLOSS



ELEVATION

SCALE - 3/4" = 1' - 0"

END VIEW

SCALE - 3/4" = 1' - 0"

IN LOCATIONS NORTH OF
TENN. & N. CAROLINA
THE FOOTINGS MUST BE 3'-0" DEEP
DUE TO THE FROST LINE
ALL OTHERS TO BE 2'-0" DEEP

SEE NOTE

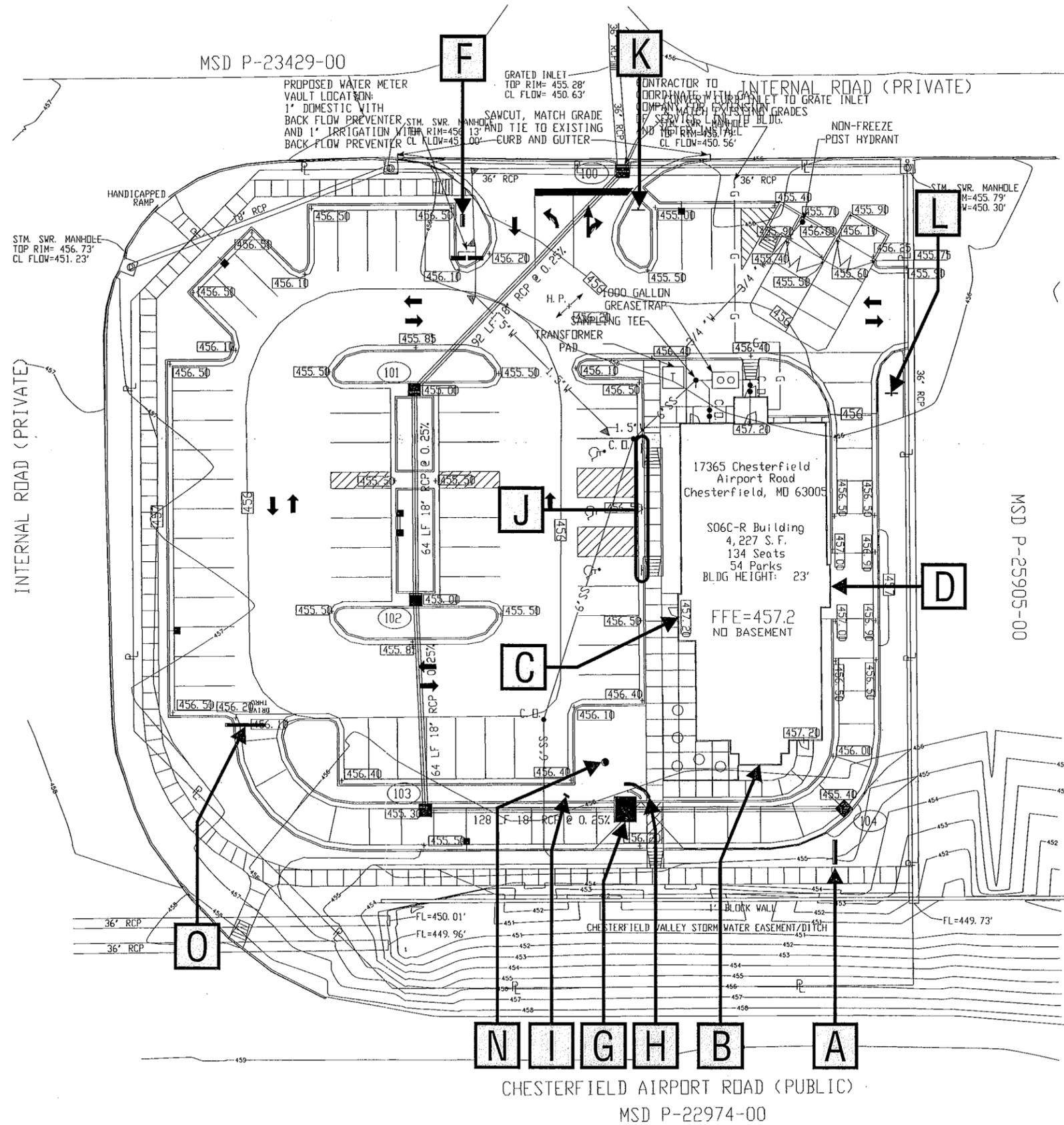


5198 North Lake Drive
Lake City, GA 30260
404-361-3800 fax 404-361-7038
website - www.claytonsigns.com

SIGN TYPE 9B	INTERNALLY-ILLUMINATED PRE-SALE DRIVE-THRU MENU	ALL ELECTRICAL SIGNS ARE 120 VOLTS UNLESS OTHERWISE INDICATED	DRAWN BY BEN HOLLIDAY ACCOUNT REP. BEN HOLLIDAY DRAWING DATE APRIL 13, 2006	STORE NUMBER 1976	STORE ADDRESS CHICK-FIL-A 17365 CHESTERFIELD AIRPORT ROAD CHESTERFIELD, MO 63005	THIS DRAWING IS AN ORIGINAL, UNPUBLISHED DESIGN CREATED BY CLAYTON SIGNS, INC. IT IS NOT TO BE REPRODUCED, CHANGED, OR EXHIBITED TO ANYONE OUTSIDE OF YOUR COMPANY IN WHOLE OR IN PART WITHOUT WRITTEN PERMISSION FROM THE OWNER OF CLAYTON SIGNS, INC.. ANY SUCH ACTIONS MAY BE SUBJECT TO LEGAL ACTION IN A COURT OF LAW. © 2005 ALL RIGHTS RESERVED	LOCATION I
	DRAWING FILE - CFA - CHESTERFIELD, MO SIGNAGE.CDR						

LEGEND

A	MAIN IDENTIFICATION SIGN
B	WALL SIGN
C	WALL SIGN / BULLNOSE CANOPY
D	WALL SIGN / BULLNOSE CANOPY
E	(NO WALL SIGN ALLOWED)
F	DIRECTIONAL SIGN
G	DRIVE-THRU MENU CANOPY
H	DRIVE-THRU MENU BOARD
I	DRIVE-THRU PRE-SALE MENU BOARD
J	DOT - HANDICAPPED PARKING SIGNS
K	DOT - STOP SIGN
L	DOT - EXIT ONLY SIGN
M	DOT - DRIVE-THRU DIRECTIONAL
N	FLAG POLE
O	CLEARANCE BAR
P	TENANT PANELS ON EXISING SIGN
Q	



5198 North Lake Drive
 Lake City, GA 30260
 404-361-3800 fax 404-361-7038
 website - www.claytonsigns.com

BUILDING
S06C
REVERSE

SITE PLAN

DRAWING FILE - CFA - CHESTERFIELD, MO SIGNAGE.CDR

ALL ELECTRICAL
 SIGNS ARE
 120 VOLTS
 UNLESS
 OTHERWISE INDICATED

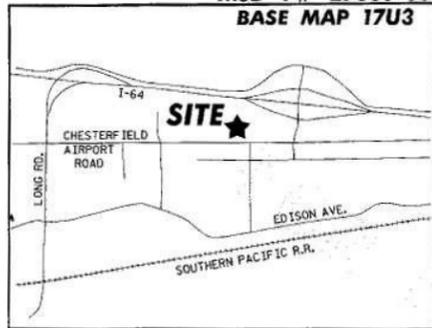
DRAWN BY BEN HOLLIDAY
 ACCOUNT REP. BEN HOLLIDAY
 DRAWING DATE APRIL 13, 2006

STORE NUMBER
1976

STORE ADDRESS
 CHICK-FIL-A
 17365 CHESTERFIELD AIRPORT ROAD
 CHESTERFIELD, MO 63005

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 © 2005 ALL RIGHTS RESERVED

SITE PLAN



VICINITY MAP
N.T.S.

NOTICE: PEDESTRIAN SAFETY

THE CONTRACTOR SHALL TAKE NECESSARY MEASURES TO SEPARATE EACH WORK AREA FROM PEDESTRIAN TRAFFIC AND TO INSURE SAFE PEDESTRIAN PASSAGE AT ALL TIMES. THE CONTRACTOR SHALL ALSO UTILIZE SAFE WARNING SIGNS, BARRICADES AND OTHER RELATED MEASURES, AS NECESSARY. THE CONTRACTOR SHALL COORDINATE PERIODICALLY WITH THE PROJECT MANAGER TO REVIEW SAFETY CONCERNS AND ADVISE OF ACTIVE WORK AREAS.

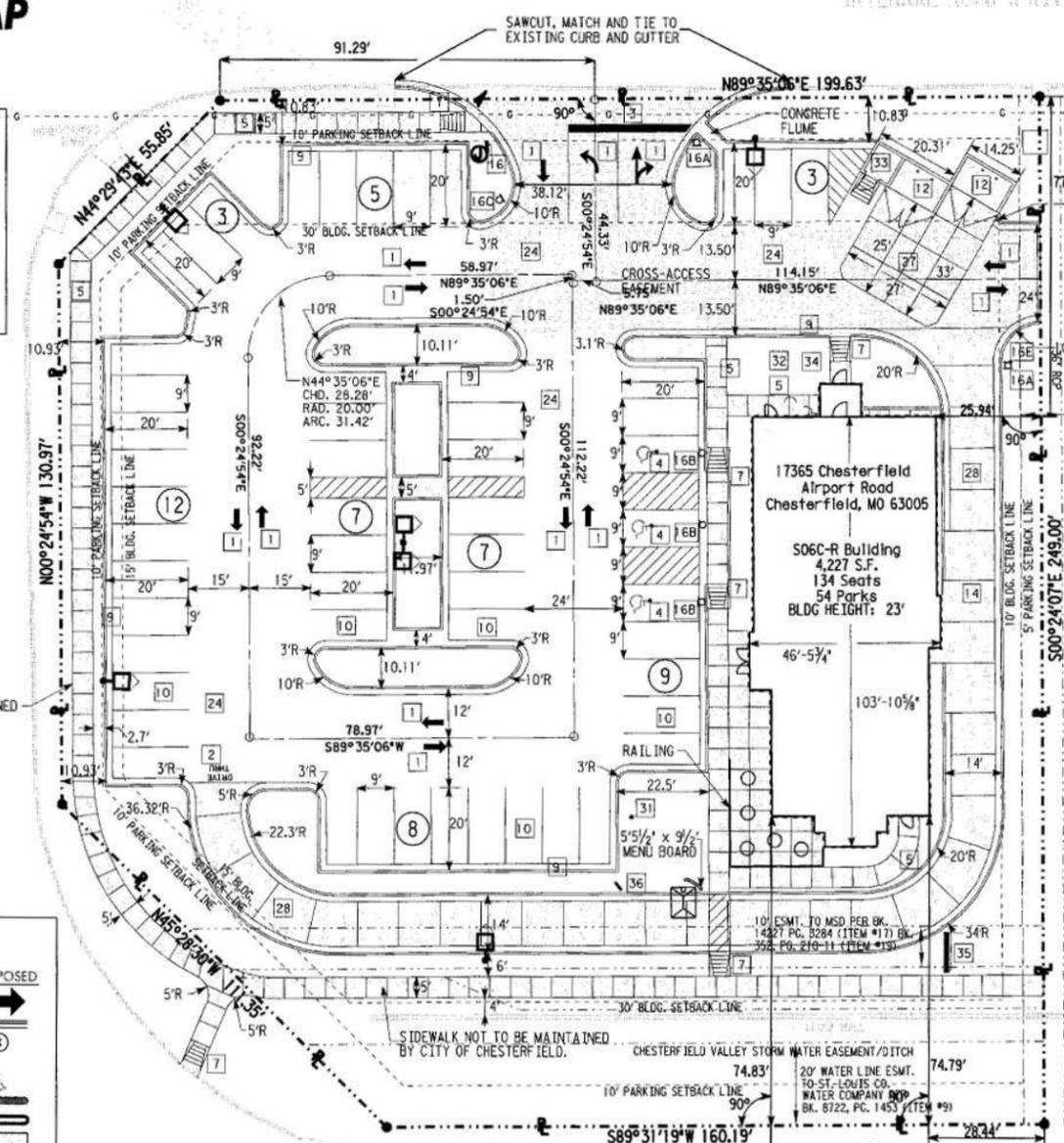
SITE IS LOCATED 1,450 FEET FROM BOONES CROSSING.

SITE NOTES:

- ALL BUFFERS AND TREE SAVE AREAS SHALL BE CLEARLY IDENTIFIED BY FLAGGING AND/OR FENCING PRIOR TO COMMENCEMENT OF ANY LAND DISTURBANCE ACTIVITY.
- ALL CONSTRUCTION TO COMPLY WITH CITY OF CHESTERFIELD STANDARDS.
- SITE ZONING IS HC.
- TOTAL BUILDING AREA = 4,227 S.F.
- ALL RADII ARE 3' UNLESS OTHERWISE NOTED.
- ALL DIMENSIONS SHOWN ARE TO FACE OF CURB, FACE OF STRUCTURE, EDGE OF PAVEMENT OR CENTERLINE AS APPLICABLE UNLESS OTHERWISE NOTED.
- CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING ALL LINES AND GRADES TOGETHER WITH ALL REFERENCE POINTS AS REQUIRED FOR ALL WORK UNDER THIS CONTRACT. ALL REQUIRED LAYOUT SHALL BE DONE USING COMPETENT AND EXPERIENCED PERSONNEL UNDER THE SUPERVISION OF A LAND SURVEYOR REGISTERED IN THE STATE OF MISSOURI. THE CONTRACTOR SHALL PROVIDE ALL LABOR, INSTRUMENTS, STAKES, TEMPLATES, AND OTHER MATERIALS NECESSARY FOR MARKING AND MAINTAINING ALL LINES AND GRADES.
- ALL SIGNING AND PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) (CURRENT EDITION). MAXIMUM SLOPES ON ALL HANDICAPPED RAMPS NOT TO EXCEED 1:2.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED GOVERNMENTAL INSPECTIONS.
- A COPY OF THE APPROVED LAND DISTURBANCE PLAN AND PERMIT SHALL BE PRESENT ON SITE WHENEVER LAND DISTURBING ACTIVITY IS IN PROGRESS. TOPOGRAPHIC AND BOUNDARY INFORMATION SHOWN HEREON IS BASED UPON INFORMATION FROM ALTA/ACSM LAND TITLE SURVEY BY FREELAND-KAUFMAN & FREEDEN, INC. DATED APRIL 12, 2006.
- CONSTRUCTION EQUIPMENT SHALL NOT BE PARKED IN RIGHT-OF-WAY AND MUST BE STORED WITHIN THE SITE.
- PROPOSED USE IS A RESTAURANT.
- TOTAL SITE AREA : 1.28 ACRES. DISTURBED AREA : 1.20 ACRES
- CONTRACTOR TO NOTIFY INSPECTOR 24 HOURS PRIOR TO CONSTRUCTION. ALL CONSTRUCTION MUST CONFORM TO CITY OF CHESTERFIELD STANDARDS AND SPECIFICATIONS, WHETHER OR NOT REVIEW COMMENTS WERE MADE.
- ALL LIGHTING SHALL BE IN COMPLIANCE WITH CITY OF CHESTERFIELD LIGHT ORDINANCE NUMBER 2228.
- MOUNTING HEIGHT OF LIGHT FIXTURES SHALL NOT EXCEED 30 FEET.
- ALL SIGNAGE SHALL BE IN CONFORMANCE WITH SECTION 1003.168(4) OF THE ZONING ORDINANCE AND ANY OTHER SIGN ORDINANCE FOR THE AREA.
- TREES AND LANDSCAPING MAY NOT BE LOCATED WITHIN 5 FEET OF THE RETAINING WALL OF CHESTERFIELD VALLEY MASTER STORMWATER DITCH.

SITE PLAN DESIGN NOTES & KEY PLAN

- 1 DIRECTIONAL ARROW
- 2 DRIVE-THRU GRAPHICS
- 3 STOP LINE GRAPHIC
- 4 PAINTED HANDICAP PARKING SYMBOL
- 5 CONCRETE SIDEWALK
- 6 CURBED HANDICAP RAMP
- 7 SIDEWALK HANDICAP RAMP
- 7A HANDICAP RAMP w/ FLARED SIDES
- 8 SIDEWALK WITH CURB & GUTTER
- 9 24" CONCRETE CURB AND GUTTER
- 9A SPILLING SECTION
- 9B CATCHING SECTION
- 10 STANDARD PARKING STALL (9'x20')
- 11 REFUSE ENCLOSURE FOUNDATION
- 12 REFUSE ENCLOSURE ALT. DRAINAGE
- 13 REFUSE ENCLOSURE FOUNDATION (ALT)
- 14 DRIVE THRU PLAN
- 15 DRIVE-THRU ISOMETRIC
- 16 DIRECTIONAL SIGNAGE
- 16A STOP SIGN
- 16B HANDICAP SIGN
- 16C DRIVE-THRU DIRECTIONAL SIGN
- 16D *RIGHT TURN ONLY* SIGN
- 16E EXIT ONLY SIGN
- 17 LANDSCAPE & IRRIGATION PROTECTOR
- 18 CONCRETE WHEEL STOP
- 19 SERVICE LINE CLEANOUT
- 20 TYPICAL LIGHT POLE BASE
- 21 HANDRAIL ELEVATION (USE ONLY ON SITES WHERE DRIVEWAY IS AGAINST BUILDING)
- 22 TYPICAL ADA RAMP AND HANDRAIL
- 23 PIPE BUMPER POST
- 23A AUTOMOBILE PARKING BUMPER POST
- 24 TYPICAL PAVEMENT SECTION
- 25 TRANSVERSE & LONGITUDINAL CONTRACTION JOINT
- 26 TRANSVERSE & LONGITUDINAL DOWELED CONST. JOINT
- 27 CONCRETE APRON @ TRASH ENCLOSURE
- 28 CONCRETE PAVING @ DRIVE THRU LANE
- 29 EXPANSION JOINT
- 30 CONTRACTION JOINT
- 31 FLAGPOLE
EXC SERIES 45 FOOT FLAG POLE PACKAGE, BY APPROVED VENDORS; THE FLAG COMPANY OR ATLAS FLAGS.
- 32 TRANSFORMER
- 33 REFUSE/STORAGE BUILDING (SEE ARCHITECT PLANS)
- 34 GREASE TRAP
- 35 CHICK-FIL-A PRIME SIGN
- 36 PRE-SELL MENUBOARD
- 37 MENU BOARD & CANOPY ORDERING STATION
- 38 CHICK-FIL-A *ENTER* SIGN
- 39 CHICK-FIL-A *EXIT* SIGN



NOTICE: EXISTING UTILITY LOCATIONS
PRIOR TO EXCAVATION AND INSTALLATION OF ANY UNDERGROUND UTILITY, STORM DRAINAGE FACILITIES, GRADING & PAVING, AND OTHER RELATED CONSTRUCTION ACTIVITY, THE CONTRACTOR SHALL CONTACT THE LOCAL UTILITY LOCATING COMPANY. THE CONTRACTOR SHALL ALSO BE REQUIRED TO COORDINATE DIRECTLY (AS NECESSARY) WITH EACH APPLICABLE UTILITY AGENCY FOR APPROPRIATE FIELD LOCATION MARKING. THE CONTRACTOR SHALL BE RESPONSIBLE TO ASSURE THAT ALL EXISTING UTILITIES HAVE BEEN LOCATED & MARKED TO PERFORM THE WORK IN A SAFE MANNER WITHOUT DAMAGE TO EXISTING UTILITIES. THE CONTRACTOR WILL BE RESPONSIBLE FOR REPAIRING ANY DAMAGE DONE TO EXISTING UTILITIES DURING CONSTRUCTION. THE CONTRACTOR SHALL VERIFY THE LOCATION, DIAMETER, PIPE MATERIAL, AND ELEVATION OF EXISTING UTILITIES AND STRUCTURES, AS APPLICABLE, PRIOR TO CONSTRUCTION, AND REPORT ANY DISCREPANCIES TO THE ENGINEER.

ALL ADJOINING PROPERTY IS OWNED BY THE REALTY.

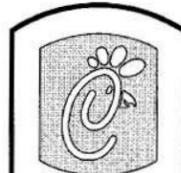


FORWARD COMMENTS TO:
TODD M. ROGERS, P.E.
5665 NEW NORTHSIDE DRIVE, SUITE 400
ATLANTA, GA 30328
(770) 933-0280

AREA SUMMARY: CHICK-FIL-A PARCEL		PROPOSED AREA TABULATION	
EXISTING AREA TABULATION		IMPERVIOUS AREA	
IMPERVIOUS AREA		BUILDING PAD	0.10 ACRES
BUILDING PAD	0.00 ACRES	ASPHALT PAVEMENT & CONCRETE	0.75 ACRES
ASPHALT PAVEMENT & CONCRETE	0.00 ACRES	PERVIOUS AREA	
GRASS & LANDSCAPE AREA	1.28 ACRES	GRASS & LANDSCAPE AREA (GREENSPACE)	0.43 ACRES (33.6%)
TOTAL AREA	1.28 ACRES	TOTAL AREA	1.28 ACRES

PARKING REQUIREMENTS

PARKING RATIO = 1 SPACE PER 2 SEATS PLUS 2 SPACES PER 3 EMPLOYEES ON LARGEST SHIFT.
134 SEATS, 12 EMPLOYEES
TOTAL SPACES REQUIRED = 75
STANDARD SPACES PROVIDED = 51
HANDICAP SPACES PROVIDED = 3
TOTAL SPACES PROVIDED = 54
REMAINDER OF REQUIRED SPACES IS PROVIDED IN SHOPPING CENTER LOT PER CROSS-PARKING EASEMENT.



5200 Buffington Rd.
Atlanta, GA 30349-2998
contact: Kristen Clever
(404) 684-8660

Revisions:
Mark Date By

△

Mark Date By

△

Mark Date By

△

Seal



STORE
SERIES S06C-R
17365
CHESTERFIELD AIRPORT ROAD
CHESTERFIELD, MISSOURI 63005
ZONING: C-8
PARCEL ID# 17U330189

SHEET TITLE

SITE PLAN

Preliminary
 80% Submittal
 For Construction

Job No.: 061781-00

Store: 1976

Date: 6-08-06

Drawn By: JDR

Checked By: TMR

Sheet

C-2.0

PROPOSED USE IS FAST SERVICE RESTAURANT.



VIII. A.

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

Planning Commission Staff Report

Meeting Date: May 14, 2007

From: Annissa G. McCaskill-Clay, AICP
Assistant Director of Planning

Subject: Rezoning Vote Report

Location: 163 S. Woods Mill Rd., 155 S. Woods Mill Rd., 133 S. Woods Mill Rd., 111 S. Woods Mill Rd. (18Q420023, 18Q510014, 18Q510025, 18Q510036)

Petition: P.Z. 9-2006 St. Luke's Episcopal Presbyterian Hospitals (Parcel D))

Proposal Summary

Michael J. Doster, on behalf of St. Luke's Episcopal Presbyterian Hospitals has submitted an application for a change of zoning from NU" Non-Urban District, "FPNU" Flood-Plain Non-Urban District and "R1" Residence District/FPR1" Flood-Plain Residence District to "MU" Medical Use District per the regulations of Section 1003.127 of the City of Chesterfield Zoning Ordinance. The subject site is four (4) parcels of land located ¼ mile north of intersection of Woods Mill and Conway Roads.

Staff Recommendation

The Attachment A for this request meets all of the development requirements of the City of Chesterfield and therefore, Staff recommends approval of the change of zoning for to "MU" Medical Use District.

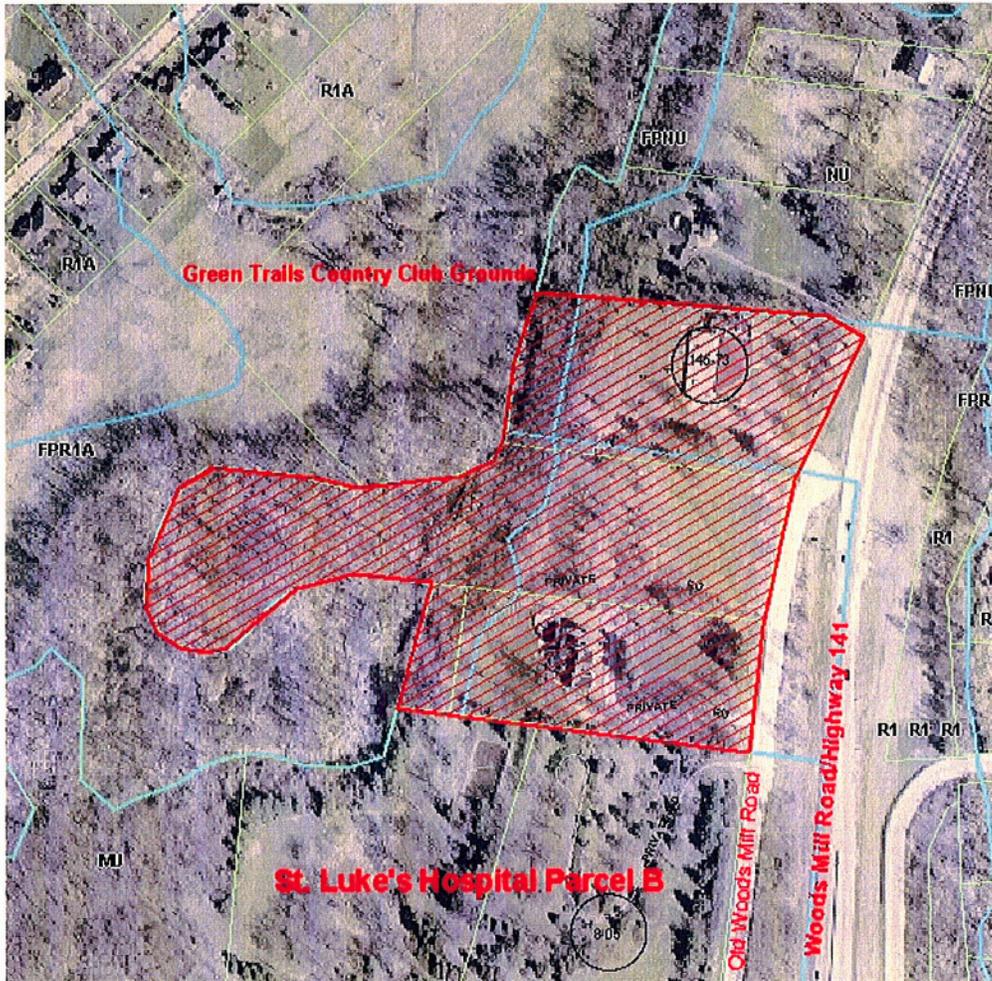
Zoning Analysis

A preliminary plan accompanies all rezoning requests when the change of zoning is to a Planned District. When a vote is taken on a rezoning request, the vote is to approve the

change of zoning with an Attachment A written by Staff. The vote is not to approve the accompanying preliminary plan which is provided for informational purposes only.

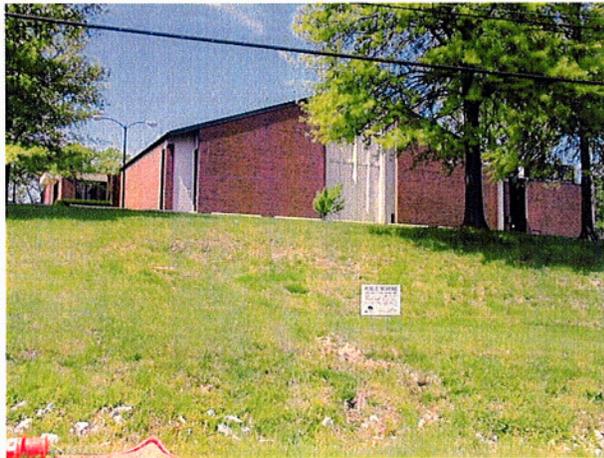
Surrounding Land Use and Zoning

The land use and zoning for the properties surrounding this parcel are as follows:



- North: The property to the north is a single-family residence on a "NU" Non-Urban District-zoned parcel.
- South: The property to the south is St. Luke's Hospital Parcel B, which is zoned "MU" Medical Use District.
- East: Immediately to the east is Old Woods Mill Road and Woods Mill Road/Highway 141. Beyond that is Ladue Farms Subdivision, which is zoned "R1" Residence District.
- West: To the west of the subject site is Green Trails Subdivision, which is zoned "R1A" Residence District.

The following pictures are from the subject site:



Comprehensive Plan Analysis

The subject site is located within Ward 1 of the City of Chesterfield. The Comprehensive Plan designates this as **single-family residential**. The subject parcels are not located in any sub-area identified by the Comprehensive Plan; therefore there are no additional development guidelines for this site.



Site Area History

The subject parcels were zoned by St. Louis County prior to incorporation of the City of Chesterfield.

Issues

A public hearing was held on this request on April 24, 2006. At the hearing, several issues were identified. Staff's issues report, which includes those issues with the Petitioner's responses, are attached.

Please note that open issues from this meeting have been addressed to Staff's satisfaction to move forward with the zoning request.

Request

Staff recommends approval of the change of zoning to "MU" Medical Use District with the Attachment A as written.

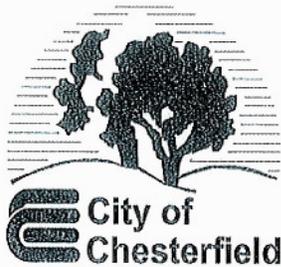
Respectfully submitted,

Annissa McCaskill-Clay

Annissa G. McCaskill-Clay, AICP
Assistant Director of Planning

Attachments

1. Attachment A
2. Preliminary Plan



VIII. B.

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

December 28, 2006

Planning Commission
City of Chesterfield
690 Chesterfield Pkwy W
Chesterfield, MO 630170-0760

The agenda for the Planning Commission meeting on **January 8, 2007** will include the below referenced matter for your review.

P.Z. 9-2006 St. Luke's Episcopal Presbyterian Hospitals (Parcel D): a request for a change of zoning from "NU" Non-Urban District, "FPNU" Flood-Plain Non-Urban District and "R1" Residence District/FPR1" Flood-Plain Residence District to "MU" Medical Use District for four (4) parcels of land located ¼ mile north of the Woods Mill and Conway Roads intersection.

A public hearing was held on April 14, 2006. For the purpose of this report staff responses will include:

Issue has been addressed- The Department of Planning has reviewed the material submitted and found the information complete and therefore the issue has been addressed. Unless directed by Planning Commission, this issue will be considered resolved and will be removed from future reports.

Petitioner has addressed the issue- The Department of Planning has reviewed the material submitted and request direction from the Planning Commission whether the issue has been resolved.

Issue remains open- The Department of Planning has reviewed the petitioner's response to this issue and finds it incomplete. The Planning Commission has an opportunity to clarify the issue with Staff and request additional information.

ISSUES IDENTIFIED AT PUBLIC HEARING AND BY CITY OF CHESTERFIELD STAFF FOR PETITIONER TO ADDRESS:

1. Comprehensive Plan compliance – the Comp Plan designates the site as "residential".

Petitioner's Response: A few years ago, the City requested St. Luke's Hospital to present (sic) its long range plan. Subsequently, St. Luke's applied for rezoning of Parcels "A" and "B" to "MU" Medical Use District. When that application was presented, St. Luke's presented a conceptual long range plan that included the Cowee and Church properties collectively now known as Parcel "D" and subject to the current rezoning request. While St. Luke's owned the Church property at the time of the application for rezoning of Parcels "A" and "B", it did not own the Cowee property and therefore could not present Parcel "D" at the same time. St. Luke's now owns the Cowee property.

Parcels "A" and "B" were rezoned to "MU" Medical Use District. St. Luke's is seeking the same zoning for Parcel "D" which abuts Parcel "B". The Comprehensive Plan also shows Parcel "B" as "residential". Rezoning Parcel "B" effectively updates the Comprehensive Plan (which by its terms is a statement of "policies and recommendations" that are "adaptable to changing needs"). We believe it has been generally recognized for some time that the properties abutting Highway 141 (as it is planned to be improved) in this area are more suitable for the Medical Use District uses than residential uses.

This issue was also raised during the rezoning of Parcels "A" and "B". Our response to this issue in that rezoning is still appropriate. Our response, in pertinent part, was as follows:

"Given the existing Hospital facilities and the need to redevelop and expand if St. Luke's is to continue to provide the best health care services to the community, the public interest would certainly be served by allowing the redevelopment and expansion as proposed so long as it is done pursuant to a reasonable and appropriate plan. We believe the Preliminary Plan is a reasonable and appropriate plan, and we further believe that the properties that are included in the plan are appropriate for a Medical Use District. In summary, we believe the proposal is in harmony with the intent of the Comprehensive Plan and fits with the area."

Staff response: Petitioner has addressed the issue.

2. Provide cross-sections showing an analysis to the homes in Ladue Farms.

Petitioner's Response: The attached cross-sections for the site have been extended across Highway 141 to include Ladue Farms.

Staff response: Issue has been addressed.

3. Development west of the creek.

Petitioner's Response: As this area is in flood-plain, we have no intention – now or in the future – of developing any portion of the St. Luke's site west of the creek.

Staff response: The Attachment A prohibits development west of the creek shown on the preliminary plan and all setbacks are written in consideration of this prohibition.

Issue has been addressed.

4. Approval by the Fire Protection District and other applicable agencies.

Petitioner's Response: The revised Preliminary Development Plan has been submitted to the appropriate agencies for comment and approval.

Staff response: Attached are comments received to date from the Metropolitan Fire Protection District and the Missouri Department of Transportation.

Issue remains open.

5. Does the expansion (the proposed buildings) need to be near the hospital for any particular medical reasons?

Petitioner's Response: The west Campus expansion of St. Luke's is designed to be patient/ user friendly. All of the diagnostic and medical services required by a patient can be provided in one recognized geographic area of the City of Chesterfield. It is an ideal circumstance, not available to most other medical centers, that St. Luke's can separate the diagnostic services from the major medical services on two complimentary adjacent parcels, thereby making the campus more accessible and user friendly. Each campus provides a unique service to patients and the separation makes each one more efficient and patient oriented. On the rare occasion where emergency treatment is required within the diagnostic campus the emergency treatment is immediately available in the shortest time possible. There are also those circumstances where patients need more extensive testing and that service would be immediately available on the main campus.

The ideal situation is where diagnostic analysis and treatment can compliment each other while making all the available medical services convenient and readily available to the majority of patients. St. Luke's is committed to maintaining it's top 100 provider position in the nation and this new diagnostic availability is yet another step in that commitment to excellence.

Staff response: Issue has been addressed.

6. Impact of heavy construction equipment on the subject site and residential developments in the area. Can this cause problems with foundations?

Petitioner's Response: Construction during this project will not cause structural and foundation problems for the nearby residences.

Staff Response: Issue has been addressed.

7. Is the hospital complex becoming too big and unmanageable?

Petitioner's Response: See responses to #1 and #5 above.

Staff Response: Issue has been addressed.

8. Notification of residents.

Petitioner's Response: We understand that Staff is responding to this issue.

Staff Response: The following is the notification process utilized by the City of Chesterfield:

- The Notice of Public Hearing was published in St. Louis Countian and Suburban Journal 15 days prior to the public hearing
- The Notice of Public Hearing is posted at City Hall and on the City of Chesterfield website.
- The properties subject to the rezoning request were also posted

Issue has been addressed.

9. In several locations the plan refers to Old Woods Mill Road. This should be revised to South Woods Mill Road.

Petitioner's Response: The Preliminary Development Plan has been changed.

Staff Response: Issue has been addressed.

10. Visibility of the proposed development from the adjacent residential neighborhoods, as well as from Woods Mill Road.

a. Are the heights of the trees adequate for proper screening?

Petitioner's Response: The height of the trees is adequate to screen the buildings. The attached site sections further illustrate the relationship of the building, tree heights and the existing residences.

Staff Response: Petitioner has addressed the issue.

b. Can the hospital begin increasing its buffering and density now to insure mature plantings by the time construction begins?

Petitioner's Response: *We agree to plant trees now to insure a better buffer by the time construction begins in the future.*

Staff Response: Section of the Draft Attachment A contains criteria for additional plantings to be shown on the Site Development Plan and to be planted prior to commencement of construction.

Issue has been addressed.

II. LIGHTING

1. Lighting of the proposed development.

Petitioner's Response: *The development of Parcel D will comply with the City of Chesterfield Lighting Ordinance.*

Staff Response: See Section of the Draft Attachment A. As with Ordinance , which governs Parcels A & B, additional criteria has been added regarding shut off times.

Issue has been addressed.

III. ACCESS

1. Access to the existing subdivision on Parcel A.

Petitioner's Response: *We believe MoDOT may accept a "right-in-only" access in addition to the existing access. No official approval has been received. We are still working on this. In any event, under the current zoning for Parcel A, the obligation to address this access has not been triggered. Nevertheless, St. Luke's wants a reasonable and feasible solution as soon as reasonably practicable.*

Staff Response: Petitioner has addressed the issue.

IV. PUBLIC/PRIVATE ROAD IMPROVEMENTS, INCLUDING PEDESTRIAN CIRCULATION

1. Improvements to South Woods Mill Road will be required. Currently the preliminary plan indicates that the road will be reconstructed to be 24' wide. South Woods Mill Road will need to be improved to a 39 foot wide pavement section, with curb and gutter and all attendant drainage facilities, along the entire frontage of the proposed development. The right-of-way will need to be a minimum of 60 feet wide with a 10 foot wide Roadway Maintenance and Utility

Easements on the west side. Should the development of the site occur prior to the improvements to the State Route 141 corridor, proposed to be constructed by the Missouri Department of Transportation (MoDOT); a temporary turnaround will need to be constructed at the northern terminus of South Woods Mill Road.

Petitioner's Response: *We acknowledge this requirement.*

Staff Response: *Issue has been addressed.*

2. A sidewalk, 5 feet wide minimum, will be required to be constructed along the west side of South Woods Mill Road along the entire frontage of the project.

Petitioner's Response: *We acknowledge this requirement.*

Staff Response: *Issue has been addressed.*

V. TRAFFIC

1. The traffic study for the St. Luke's Campus will need to be updated to address the additional building square footage and the additional access point.

Petitioner's Response: *The Traffic Study is currently being updated to include Parcel D. We will submit the Study when it is complete.*

2. The impact of traffic on the surrounding neighborhoods.

Petitioner's Response: *The Traffic Study is currently being updated to include Parcel D. We will submit the Study when it is complete.*

3. Traffic circulation.

Petitioner's Response: *The Traffic Study is currently being updated to include Parcel D. We will submit the Study when it is complete.*

4. How possible increased traffic from the proposed development may affect the area.

Petitioner's Response: *The Traffic Study is currently being updated to include Parcel D. We will submit the Study when it is complete.*

5. Road improvements in the subject area – particularly the timing of the improvements and cooperation/coordination with MoDOT.

Petitioner's Response: *The Traffic Study is currently being updated to include Parcel D. We will submit the Study when it is complete.*

Staff Response: *The City of Chesterfield received the Traffic Study in support of this project on September 5, 2006. The attached issues were forwarded to the Petitioner. To date, staff has not received a response.*

Issue remains open.

VI. STORMWATER AND SANITARY SEWER

1. General site drainage.

Petitioner's Response: *As proposed on Parcel B, the site will utilize underground detention basins to collect and disperse storm water.*

Staff Response: Issue has been addressed.

2. The preliminary plan does not indicate storm water detention facilities on the proposed development. The recent changes to the storm water regulations promulgated by the Metropolitan St. Louis Sewer District may have a significant impact on the layout of the site.

Petitioner's Response: *No changes are expected from the new MSD storm water regulations.*

Staff Response: Issue has been addressed.

3. The layout of the site may be impacted by storm sewer and underground detention basins shown on the Site Development Section Plan for the northern portion of Parcel B. These improvements are not shown on this preliminary plan and may impact the connection of parking structure "D" to the parking structure on Parcel B.

Petitioner's Response: *The placement of underground detention basins on Parcel D will not impact the connection of the parking garage on Parcels B and D.*

Staff Response: Issue has been addressed.

Attached are copies of all agency comments received to date, the draft Attachment A and Preliminary Plans.

Respectfully Submitted,



Annissa G. McCaskill-Clay, AICP
Assistant Director of Planning

ATTACHMENT A

All provisions of the City of Chesterfield City Code shall apply to this development except as specifically modified herein.

I. SPECIFIC CRITERIA

A. DEFINITIONS

1. The Permitted and Ancillary Uses for this development are as defined below:

a. Permitted Land Uses:

- i. Educational Services to the Public related to Health Care—Services provided to the Public which provide education regarding health matters, with the objective of improving physical and emotional health status.
- ii. Health Services; including clinics of doctors and dentists—Health care services offered by appointment on an ambulatory basis. Services may include, but is not limited to, outpatient surgery, examination, diagnosis, and treatment of a variety of medical conditions on a non-emergency basis, and laboratory and other diagnostic testing as ordered by staff or outside physician referral.
- iii. Laboratories—Specialized facilities capable of providing a variety of procedures specific to clinical laboratory services to health provider organizations, which are available on a regular and convenient basis.
- iv. Medical Care Facilities—Facilities providing health care services, including, but not limited to, hospitals, medical centers, ambulatory clinics, physicians' offices, surgical centers, skilled nursing centers, long-term care centers, assisted living, residential care and treatment facilities, diagnostic centers, psychiatric

care, medical imaging centers, reproductive health and fertility centers, physical rehabilitation, respiratory therapy, dentistry, hospice and home health services.

- v. Research Facilities—Facilities where research is conducted in support of clinical care and the collection of research data for clinical research programs.
 - vi. Residential Care and Treatment Facilities—Facilities that provide custodial care and/or treatment services to persons who are not able to live independently. Residential facilities may include, but are not limited to, nursing homes, assisted living facilities, group homes or supported living arrangements.
 - vii. Schools for the Handicapped—Centers equipped and staffed to provide education, support and encouragement to handicapped individuals and their families.
- a. Ancillary Uses:
- i. Assisted Living—A senior residence assisted by congregate meals, housekeeping, and personal services for persons who have difficulties with one or more essentials of daily living, but for whom full-time professional medical care is unnecessary.
 - ii. Cafeterias for use by employees and guests of primary users—A restaurant in which employees and guests of primary users are served at a counter and carry their meals on trays to tables after paying.
 - iii. Day Care, including Adult Day Care—A facility providing care for five (5) or more children under the age of thirteen (13), for less than twenty-four (24) hours per day. Adult day care facilities are those that receive payment for the care of persons over eighteen (18) years of age for less than twenty-four (24) hours per day. The adult day care center shall provide a structured program of personalized care for adults who are not capable of full independent living as a result of physical disability, developmental disabilities,

emotional impairment, or frailty resulting from advanced age.

iv. Duplicating, Mailing, Stenographic and Office Services—Services which provide reproduction of text, drawings, plans, maps, or other copy, by blueprinting, photocopying, mimeographing, reproducing shorthand or other methods of duplication, and providing clerical or professional services.

v. Hospitality Houses—Facilities that provide lodging and other supportive services to patients and their families.

vi. Orthopedic Stores—A store where orthopedic support devices for physical impaired individuals are sold and additional services, such as proper measurement and fitting of devices, and education on the proper use/maintenance of said devices is provided.

vii. Parking Structures, Public or Private—

Parking area—An area of land used or intended for off-street parking facilities for motor vehicles (City of Chesterfield Zoning Ordinance)

Parking Space—A durably dust-proofed, properly graded for drainage, usable space, enclosed in a main building or in an accessory building, or unenclosed, reserved for the temporary storage of one vehicle, and connected to a street, alley, or other designated roadway by a surfaced aisle or driveway. Each such designated space shall comply with the dimensional requirements set forth in Section 1003.165, "Off-Street Parking and Loading Requirements." (City of Chesterfield Zoning Ordinance)

viii. Pharmacies—A facility where prescription drugs are dispensed or compounded under the supervision of a registered pharmacist.

- ix. Schools and training facilities related to the Medical Professions, including but not limited to schools for nursing—Facilities which provide education and training, including but not limited to licensing and/or certifications, of individuals working in various areas of health care.
- x. Social Services—Services or activities undertaken to advance the welfare of citizens in need. Such services or activities may include, but are not limited to:
 - a. Assistance and counseling to patients and their families dealing with social, emotional and environmental problems associated with illness or disability.
 - b. Outpatient social work services—the above provided in ambulatory settings.
 - c. Emergency department social work services the above provided in emergency department settings within a hospital/medical center facility.
- xi. Substance Abuse Treatment Facility, Outpatient—Organized hospital/medical services that provide medical care and/or rehabilitative treatment services to outpatients for whom the primary diagnosis is alcoholism or other chemical dependency on an out-patient basis.
- xii. Terminals for buses and other Public Mass Transit vehicles—A depot building or area specifically designated for the storage of transfer of persons or material, or temporary storage and service of operable vehicles used in the transport of persons, goods or materials.

B. PERMITTED USES

- 1. The uses allowed in this “MU” Medical Use District shall be:
 - a. Educational services to the public related to health care;

- b. Health services; including clinics of doctors and dentists;
 - c. Laboratories;
 - d. Medical Care Facilities, Excluding Hospitals;
 - e. Research Facilities;
 - f. Residential care and treatment facilities;
 - g. Schools for the handicapped;
2. The following ancillary uses shall be permitted:
- a. Assisted Living;
 - b. Cafeterias for use by employees and guests of primary users;
 - c. Day Care, including adult day care;
 - d. Duplicating, mailing, stenographic and office services;
 - e. Hospitality houses;
 - f. Orthopedic stores;
 - g. Parking structures, public or private;
 - h. Pharmacies;
 - i. Restaurants, under 2,000 square feet in gross floor area without drive-thrus or drive-ins;
 - j. Schools and training facilities related to the Medical Professions, including but not limited to schools for nursing;
 - k. Social services;
 - l. Substance Abuse Treatment Facility, Outpatient;

- m. Terminals for buses and other Public Mass Transit vehicles.

C. FLOOR AREA, HEIGHT, BUILDING AND PARKING STRUCTURE REQUIREMENTS

1. FLOOR AREA

Total building floor area shall not exceed 385,000 square feet.

2. HEIGHT

- a. The maximum height of the building, exclusive of roof screening, shall not exceed five stories.
- b. The maximum height for the parking structure shall not exceed six levels.

3. BUILDING REQUIREMENTS

- a. A minimum of 70.3% greenspace is required for this development.
- b. This development shall have a maximum F.A.R. of (.63).

D. SETBACKS

1. STRUCTURE SETBACKS

No building or structure, other than: a freestanding project identification sign, boundary and retaining walls, light standards, flag poles or fences will be located within the following setbacks:

- a. No development shall be permitted west of the creek as shown on the preliminary plan.
- b. Fifty (50) feet from the right of way of Old Woods Mill Road on the eastern boundary of this "MU" District.
- c. Twenty-five (25) feet from the northern or southern boundaries of the "MU" District.

- d. One hundred fifty-five (155) feet from the western boundary of the "MU" District.

2. PARKING SETBACKS

No parking stall, loading space, internal driveway, or roadway, except points of ingress or egress, will be located within the following setbacks:

- a. No surface parking will be permitted.
- b. Fifty (50) feet from the right of way of Old Woods Mill Road on the eastern boundary of this "MU" District.
- c. Twenty-five (25) feet from the northern or southern boundaries of the "MU" District.
- d. One hundred fifty-five (155) feet from the western boundary of the "MU" District.

e.

E. PARKING AND LOADING REQUIREMENTS

- 1. Parking and loading spaces for this development will be as required in the City of Chesterfield Code.
- 2. Construction Parking
 - a. The streets surrounding this development and any street used for construction access thereto shall be cleaned throughout the day. The developer shall keep the road clear of mud and debris at all times.
 - b. Provide adequate off-street stabilized parking area(s) for construction employees and a washdown station for construction vehicles entering and leaving the site in order to eliminate the condition whereby mud from construction and employee vehicles is tracked onto the pavement causing hazardous roadway and driving conditions.

3. Parking lots shall not be used as streets.

F. LANDSCAPE AND TREE REQUIREMENTS

1. The developer shall adhere to the Tree Manual of the City of Chesterfield Code.

G. SIGN REQUIREMENTS

1. Ornamental Entrance Monument construction, if proposed, shall be reviewed by the City of Chesterfield, and/or the St. Louis County Department of Highways and Traffic, for sight distance considerations prior to installation or construction.
2. Signs shall be permitted in accordance with the regulations of the City of Chesterfield Code.

H. LIGHT REQUIREMENTS

Provide a lighting plan and cut sheet in accordance with the City of Chesterfield Code.

I. ARCHITECTURAL

1. The developer shall submit architectural elevations, including but not limited to, colored renderings and building materials. Architectural information is to be reviewed by the Architectural Review Board and the Planning Commission.
2. Building facades should be articulated by using color, arrangement or change in materials to emphasize the facade elements. The planes of the exterior walls may be varied in height, depth or direction. Extremely long facades shall be designed with sufficient building articulation and landscaping to avoid a monotonous or overpowering appearance.
3. Trash enclosures: The location, material, and elevation of any trash enclosures will be as approved by the Planning Commission on the Site Development Plan. All exterior trash areas will be enclosed with a six (6) foot high sight-proof enclosure complimented by adequate landscaping approved by the Planning Commission on the Site Development Plan.

4. Mechanical equipment will be adequately screened by roofing or other material as approved by the Planning Commission.

J. ACCESS/ACCESS MANAGEMENT

1. Access to South Woods Mill Road shall be limited to one main approach and one secondary approach that may only be utilized by emergency vehicles. The location of the drives shall be as directed by the Department of Public Works and the Missouri Department of Transportation.
2. No direct access to State Route 141 will be allowed from this parcel.

K PUBLIC/PRIVATE ROAD IMPROVEMENTS, INCLUDING PEDESTRIAN CIRCULATION

1. Provide any additional right of way and improve South Woods Mill Road along the entire frontage of the site to a 60 foot right of way and a 39 foot wide pavement including curb, and storm drainage facilities or equivalent section, as directed by the Department of Public Works. A 10 foot wide Roadway Maintenance and Utility Easements on the west side of South Woods Mill Road will also be required as directed by the Department of Public Works.
2. The improvements to South Woods Mill Road shall be constructed within one year of issuance of any building permit for Parcel D as directed by the Department of Public Works. If, near the end of the improvement completion period, the South Woods Mill Road improvements have not been completed in accordance with the approved plans, the developer may request an extension to the improvement completion period. If, after review by the Department of Public Works, such longer period is deemed necessary to facilitate adequate and coordinated provisions for transportation, utility facilities, or other required improvements, the Director of Public Works may grant an extension so long as all guarantees are extended. The Director may require as a condition of the extension, completion of certain items, recalculation of deposit amounts or other reasonable conditions as he may deem necessary.

3. Should improvements to South Woods Mill Road be required prior to the connection of South Woods Mill Road to Ladue Road, as shown on the MoDOT master plan for improvements to the State Route 141 corridor, a temporary turn around at the terminus of South Woods Mill Road will need to be constructed as directed by the Department of Public Works.
4. Provide any additional right-of-way and construct any improvements to State Route 141 as required by the Missouri Department of Transportation.
5. Provide a 5 foot wide sidewalk, conforming to ADA standards, along the west side of South Woods Mill Road along the entire length of the site as directed by the Department of Public Works.

L. TRAFFIC STUDY (if applicable)

1. Provide a traffic study as directed by the City of Chesterfield and/or the Missouri Department of Transportation. In general, the study will update the existing traffic study for the overall St. Luke's Hospital development (Parcels A, B, C and D). The scope of the study shall include internal and external circulation and shall include site specific impacts, such as the need for additional lanes, entrance configuration, geometrics, sight distance, traffic signal modifications or other improvements required, as long as the density of the proposed development falls within the parameters of the City's traffic model. Should the density be other than the density assumed in the model, regional issues shall be addressed as directed by the City of Chesterfield. The study shall also identify the thresholds of development at which intersection and roadway improvement will be required to be constructed. Said thresholds are to be reviewed and approved by the Department of Public Works.
2. The traffic study shall be submitted with the Site Development Concept Plan/Site Development Section Plan (whichever is the first to occur) and shall be updated as deemed necessary by the Department of Planning.

M. POWER OF REVIEW

The Mayor or a Councilmember of the Ward in which a development is proposed may request that the site plan be reviewed and approved by the entire City Council. This request must be made no later than 24 hours before posting the agenda for the next City Council meeting after Planning Commission review and approval of the site plan. The City Council will then take appropriate action relative to the proposal.

N. STORMWATER AND SANITARY SEWER

1. The site shall provide for the positive drainage of storm water and it shall be discharged at an adequate natural discharge point or connected to an adequate piped system.
2. Detention/retention and other storm water quantity and quality management measures are to be provided in each watershed as required by the City of Chesterfield. The storm water quantity management facilities, related to flood and channel protection, shall be operational prior to paving of any driveways or parking areas in non-residential development or issuance of building permits exceeding sixty (60%) of approved dwelling units in each plat, watershed or phase of residential developments. The location and types of storm water management facilities shall be identified on the Site Development Plan.
3. The lowest opening of all structures shall be set at least 2 feet higher than the 100-year high water elevation in detention/retention facilities. All structures shall be set at least 30 feet horizontally from the limits of the 100-year high water.

O. GEOTECHNICAL REPORT.

Provide a geotechnical report, prepared by a registered professional engineer licensed to practice in the State of Missouri, as directed by the Department of Public Works. The report shall verify the suitability of grading and proposed improvements with soil and geologic conditions and address the existence of any potential sinkhole, ponds, dams, septic fields, etc., and recommendations for treatment. A statement of compliance, signed and sealed by the geotechnical engineer preparing the report, shall be included on all Site Development Plans and Improvement Plans.

P. MISCELLANEOUS

1. All utilities will be installed underground. The development of this parcel will coordinate the installation of all utilities in conjunction with the construction of any roadway on site.

II. TIME PERIOD FOR SUBMITTAL OF SITE DEVELOPMENT CONCEPT PLANS AND SITE DEVELOPMENT PLANS

- A. The developer shall submit a concept plan within 18 months of City Council approval of the change of zoning.
- B. In lieu of submitting a Site Development Concept Plan and Site Development Section Plans, the petitioner may submit a Site Development Plan for the entire development within 18 months of the date of approval of the change of zoning by the City.
- C. Failure to comply with these submittal requirements will result in the expiration of the change of zoning and will require a new public hearing.
- D. Said Plan shall be submitted in accordance with the combined requirements for Site Development Section and Concept Plans. The submission of Amended Site Development Plans by sections of this project to the Planning Commission shall be permitted if this option is utilized.
- E. Where due cause is shown by the developer, this time interval for plan submittal may be extended through appeal to and approval by the Planning Commission.

III. COMMENCEMENT OF CONSTRUCTION

- A. Substantial construction shall commence within two (2) years of approval of the site development concept plan or site development plan, unless otherwise authorized by ordinance.
- B. Where due cause is shown by the developer, the Commission may extend the period to commence construction for not more than one additional year.

IV. GENERAL CRITERIA

A. Site Development Concept Plan, Site Development Section Plan, Site Development Plans.

1. Any site development plan shall show all information required on a sketch plan as required in the City of Chesterfield Code.
2. Include a conceptual landscape plan in accordance with the City of Chesterfield Code to indicate proposed landscaping along arterial and collector roadways.
3. Include a lighting plan in accordance with the City of Chesterfield Code to indicate proposed lighting along arterial collector roadways.
4. Provide comments/approvals from the appropriate Fire District, the Metropolitan St. Louis Sewer District, and the Missouri Department of Transportation.

A. SITE DEVELOPMENT CONCEPT PLAN SUBMITTAL REQUIREMENTS

The Site Development Concept Plan shall include, but not be limited to, the following:

1. Outboundary plat and legal description of the property.
2. Location of all roadways adjacent to the property and general location, size, and pavement widths of all interior roadways.
3. Depict the location of all buildings, size, including height and distance from adjacent property lines and proposed use.
4. Location and size of any commercial uses, types of uses proposed and general parking layout.
5. Zoning district lines and floodplain boundaries.
6. Density calculations.
7. Provide a conceptual landscape plan in accordance with the City of Chesterfield Code.

8. Provide a lighting plan in accordance with the City of Chesterfield Code.

B. SITE DEVELOPMENT SECTION PLAN SUBMITTAL REQUIREMENTS

The Site Development Section Plan shall adhere to the above criteria and to the following:

1. Location map, north arrow, and plan scale. The scale shall be no greater than one (1) inch equals one hundred (100) feet.
2. Parking calculations. Including calculation for all off street parking spaces, required and proposed, and the number, size and location for handicap designed.
3. Provide open space percentage for overall development including separate percentage for each lot on the plan.
4. Provide Floor Area Ratio (F.A.R.).
5. A note indicating all utilities will be installed underground.
6. A note indicating signage approval is separate process.
7. Depict the location of all buildings, size, including height and distance from adjacent property lines and proposed use.
8. Specific structure and parking setbacks along all roadways and property lines.
9. Indicate location of all existing and proposed freestanding monument signs
10. Zoning district lines, subdivision name, lot number, dimensions, and area, and zoning of adjacent parcels where different than site.
11. Floodplain boundaries.
12. Depict existing and proposed improvements within 150 feet of the site as directed. Improvements include, but are not limited to,

roadways, driveways and walkways adjacent to and across the street from the site, and significant natural features, such as wooded areas and rock formations, that are to remain or be removed.

13. Depict all existing and proposed easements and rights-of-way within 150 feet of the site and all existing or proposed off-site easements and rights-of-way required for proposed improvements.
14. Indicate the location of the proposed storm sewers, detention basins, sanitary sewers and connection(s) to the existing systems.
15. Depict existing and proposed contours at intervals of not more than one (1) foot, and extending 150 feet beyond the limits of the site as directed.
16. Address trees and landscaping in accordance with the City of Chesterfield Code.
17. Comply with all preliminary plat requirements of the City of Chesterfield Subdivision Regulations per the City of Chesterfield Code.
18. Signed and sealed in conformance with the State of Missouri Department of Economic Development, Division of Professional Registration, Missouri Board for Architects, Professional Engineers and Land Surveyors requirements.
19. Provide comments/approvals from the appropriate Fire District, the Metropolitan St. Louis Sewer District, and the Missouri Department of Transportation.
20. Compliance with Sky Exposure Plane.

V. RECORDING

Within 60 days of approval of any development plan by the City of Chesterfield, the approved Plan will be recorded with the St. Louis County Recorder of Deeds. Failure to do so will result in the expiration of approval of said plan and require re-approval of a plan by the Planning Commission.

VI. ENFORCEMENT

- A. The City of Chesterfield, Missouri will enforce the conditions of this ordinance in accordance with the Plan approved by the City of Chesterfield and the terms of this Attachment A.
- B. Failure to comply with any or all the conditions of this ordinance will be adequate cause for revocation of approvals/permits by reviewing Departments and Commissions.
- C. Non-compliance with the specific requirements and conditions set forth in this Ordinance and its attached conditions or other Ordinances of the City of Chesterfield shall constitute an ordinance violation, subject, but not limited to, the penalty provisions as set forth in the City of Chesterfield Code.
- D. Waiver of Notice of Violation per the City of Chesterfield Code.

This document shall be read as a whole and any inconsistency to be integrated to



VIII.B.

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

Planning Commission Staff Report

Meeting Date: May 14, 2007

From: Annissa G. McCaskill-Clay, AICP
Assistant Director of Planning

Subject: Rezoning Vote Report

Location: 1576 and 1600 Kehrs Mill Road

Petition: P.Z. 02- 2007 The Estates at Upper Kehrs Mill (Miceli Construction)

Proposal Summary

Miceli Construction has submitted an application for a change of zoning from "NU" Non-Urban to "E" One Acre District, per the regulations of City of Chesterfield Zoning Ordinance Section 1003.107 (Estate Districts). The location of the site is on the eastern side of Kehrs Mill Road, 4100 feet south of its intersection with Wild Horse Creek Road.

Staff Recommendation

The Attachment A for this request meets all of the development requirements of the City of Chesterfield and therefore, Staff recommends approval of the change of zoning from "NU" Non-Urban District to an "E" One Acre District.

Zoning Analysis

A preliminary plan accompanies all rezoning requests when the change of zoning is to a Planned District. When a vote is taken on a rezoning request, the vote is to approve the change of zoning with an Attachment A written by Staff. The vote is not to approve the accompanying preliminary plan which is provided for informational purposes only.

Surrounding Land Use and Zoning

The land use and zoning for the properties surrounding this parcel are as follows:

North: The properties to the north are zoned "NU" Non-Urban and "R1"/FPR1" (PEU) the "R1" properties are a part of the Country Place Subdivision

South
and

East: The property to the south is also the "R1/FPR1" (PEU)-zoned Country Place Subdivision

West: Immediately to the west is Tuscany Reserve, which was recently zoned "E2/E1" Acre District.



Comprehensive Plan Analysis

The subject site is located within Ward 4 of the City of Chesterfield. The Comprehensive Plan calls for this area to be single family residential with a one acre density. This subject site is not located in any sub-area identified by the Comprehensive Plan; therefore there are no additional development guidelines for this site.

Site Area History

The subject site consists of two parcels under single ownership. They were zoned "NU" Non-Urban prior to incorporation of the City of Chesterfield.

Issues

A public hearing was held on this request on March 12, 2007. At that time, several issues were identified. Those issues along with the Petitioner's responses are attached.

Below are the issues that remain open at this time as well as those that Staff was asked to respond to:

1. Review language in the Westland Acres Ordinance dealing with drainage and siltation control with the possibility of adapting it to the subject petition.

Staff Response: This language has been added as item M.4., pg. 5 of the Attachment A.

2. What is the setback of the Perry house from Kehrs Mill Road?

Staff Response: The Perry home, which is zoned "NU" Non Urban, is approximately 261 feet from Kehrs Mill Road.

3. Retain more trees on the north side of the cul-de-sac.

Staff Response: As noted at the meeting, this will be noted for review/discussion during Site Development Plan review.

Request

Staff recommends approval of the change of zoning from "NU" Non-Urban District to "E" One Acre District with the Attachment A as written.

Respectfully submitted,

Annissa McCaskill-Clay

Annissa G. McCaskill-Clay, AICP
Assistant Director of Planning

Attachments

1. Attachment A
2. Preliminary Plan
3. Issues responses
4. Letter from Mr. William E. Quinn

April 4, 2007

Mrs. Annissa G. McCaskill-Clay, AICP
Assistant Director of Planning
City of Chesterfield
690 Chesterfield Parkway W
Chesterfield, MO 63017-0760

Re: P.Z. 2-2007 The Estates at Upper Kehrs Mill (Miceli Construction)
Volz Project No. 8628-0

Dear Mrs. McCaskill-Clay:

Below is the response to your "issues" letter dated March 20, 2007, resulting from the Public Hearing held on March 8, 2007. Our responses are in *italic*.

1. Retention of the trees, particularly in the northeastern and northwestern portions of the site, with respect to erosion concerns and how neighboring properties may be affected.

The original plan was intended to show that the plan met the 30% tree retention requirement of the City. This was done because we do not currently have an accurate topographic survey of the site. In reality many of the trees along the north line will be preserved. The plan has been amended to give a more realistic depiction of the trees that can be saved on the site.

2. Storm water on the site, particularly how it will be handled and the direction in which water will be directed.

We have enclosed a Drainage Map, which depicts the area and direction of drainage leaving the site. It is important to note that the proposed drainage area to the north is less than exists today. In addition the drainage to the south will be routed through the proposed detention basin near Kehrs Mill Road. In effect, less water will be leaving the site along the entire perimeter of this project. And finally, The Metropolitan St. Louis Sewer District and the city's requirements will be met.

3. The proposed steepness of the grades in the development.

There are no proposed grades that will exceed 3:1. There are some existing grades that are slightly greater than 3:1, but these are in wooded areas and will not be disturbed.

4. The impact of the proposed development on the existing lakes in the Country Place Subdivision, as well as the maintenance of the site after development with respect to these lakes. Will the proposed development have any portion of responsibility in the event of any damage to the lakes?

Mrs. Anissa G. McCaskill-Clay, AICP
Assistant Director of Planning
City of Chesterfield
April 4, 2007
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Any impact to the adjacent property is a great concern of ours. With the permission of "Country Place", we would like to do a preconstruction survey of the existing lakes. In addition, BMP's and extensive erosion control measures will be implemented to protect these lakes. If damage to these lakes occurs as a result of this site development, Miceli Construction will repair this damage.

5. Consider whether E-Two zoning is appropriate for the site vs. E-One zoning.

E-One Zoning is appropriate for this site and is consistent with both the City Comprehensive Plan and the surrounding area. Minor adjustments were made to the lots so that six out of eight are now larger than one acre.

6. The appropriateness of the lot sizes proposed for the development.

As stated above, six out of eight lots exceed one acre in size. The remaining two lots average 0.8 acres and exceeds the adjacent lots in "Country Place" by 20%.

7. Possible erosion of the site due to development.

The very latest BMP's and the city's SWPPP requirements will be used to prevent erosion from this site that would harm any downstream property owners.

8. Possible Letter of Credit in the amount of \$2 million, above and beyond any sureties or bonds currently required by the City to address erosion and tree removal.

We do not feel a letter of credit is appropriate, but even more, we do not feel a letter of credit is necessary. This site has many existing safe guards in place to insure proper drainage and erosion control including: the requirement of Escrows, the ability of the city to issue Stop Work Orders, the requirement of the City, DNR, and EPA of a Stormwater Pollution Prevention Plan (SWPPP), the need for a NPDES permit from DNR, the need for a 404 permit or waiver from the Corp of Engineers, the developers willingness to As-Built the existing lakes, and bonds to guaranty tree preservation. All of the above agencies have enforcement authority.

9. Should field inlets be placed on the site immediately, or placed at the direction of the City at some future time?

It is premature in the development process to determine where inlets are needed. Inlets should be placed at the direction of the Civil Engineer, City Engineer and MSD.

Mrs. Annessa G. McCaskill-Clay, AICP
Assistant Director of Planning
City of Chesterfield
April 4, 2007
Page 3

10. Should the setback from Kehrs Mill Road reflect/be similar to those created for Tuscany Reserve?

As we understand it, the goal of this issue is to protect the "Visual Integrity" of Kehrs Mill Road as you travel past our site. We believe our plan accomplishes this. The entrance area of Tuscany Reserve is much lower than Kehrs Mill Road. In fact a driver heading south will likely be looking into the second story windows of any new homes. In contrast, "Upper Kehrs Mill" is significantly higher than Kehrs Mill Road. That same driver will have an existing tree mass to protect his view in addition to the required tree buffer along the road. A driver headed north has even more "Visual Protection". A car at our south property line will be over 50 feet lower than the first floor of our homes. As you reach a point perpendicular to lot 8, our first floor is still 20 feet higher than a car. Anyone traveling along Kehrs Mill Road will see a landscaped hillside as they look towards "Upper Kehrs Mill" thus providing visually protected.

11. Provide the setback requirements for Tuscany Reserve.

Staff has said they will provide this information.

12. How far are the houses in Pacland Place from Kehrs Mill Road?

Staff has said they will provide this information.

13. There was discussion regarding run-off from the cul-de-sac and Lot 3. Could the cul-de-sac be brought a little further to the south? It was noted that this would reduce the size of Lot 5 and possibly save more trees beyond the cul-de-sac on the northeast side.

It is not feasible to push the cul-de-sac to the south without creating significant grading issues. The street is currently on top of the ridge in the same location as the existing drive. As we noted earlier, the drainage area to the north will be less than exists today and additional trees will be preserved along the north property line. What we can do is to direct all the stormwater collected in the street inlets to the detention basin near Kehrs Mill Road. That will minimize the stormwater flowing to the north.

14. Twenty-foot side yard setbacks on the northeast corner where it is contiguous to Country Place.

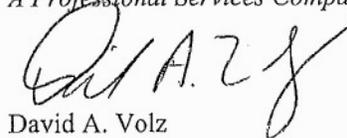
We would propose a 25-foot perimeter setback adjacent to the Country Place Subdivision.

Mrs. Annissa G. McCaskill-Clay, AICP
Assistant Director of Planning
City of Chesterfield
April 4, 2007
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As requested, enclosed are three (3) copies of the plan for your review in order to be placed on the Planning Commission agenda. If you need any additional information please do not hesitate to contact me at 314-581-6219 or Mr. Mike Miceli at 636-537-1171.

Sincerely,

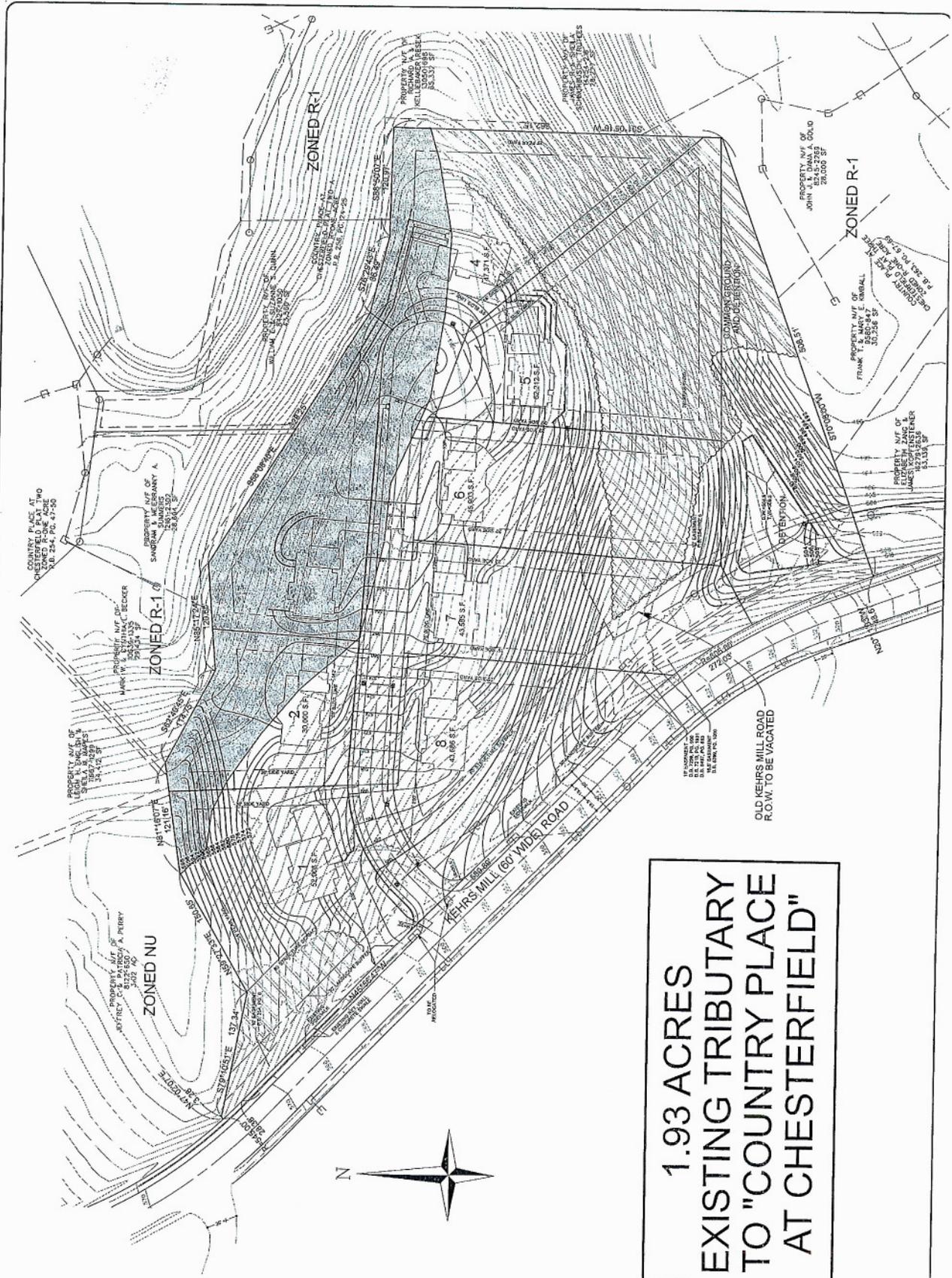
Volz Incorporated
A Professional Services Company



David A. Volz

Enclosure

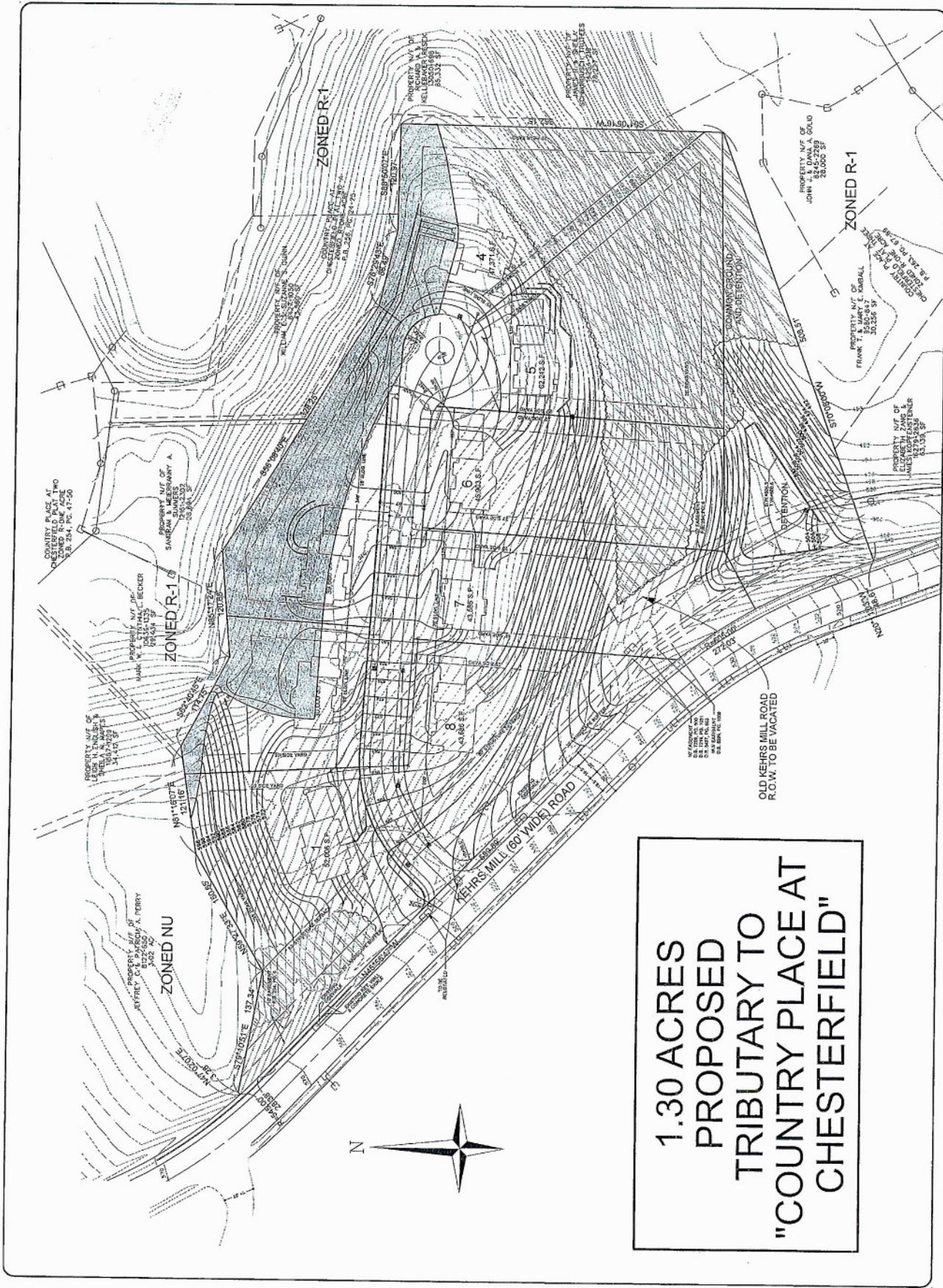
Cc: Mr. Mike Miceli



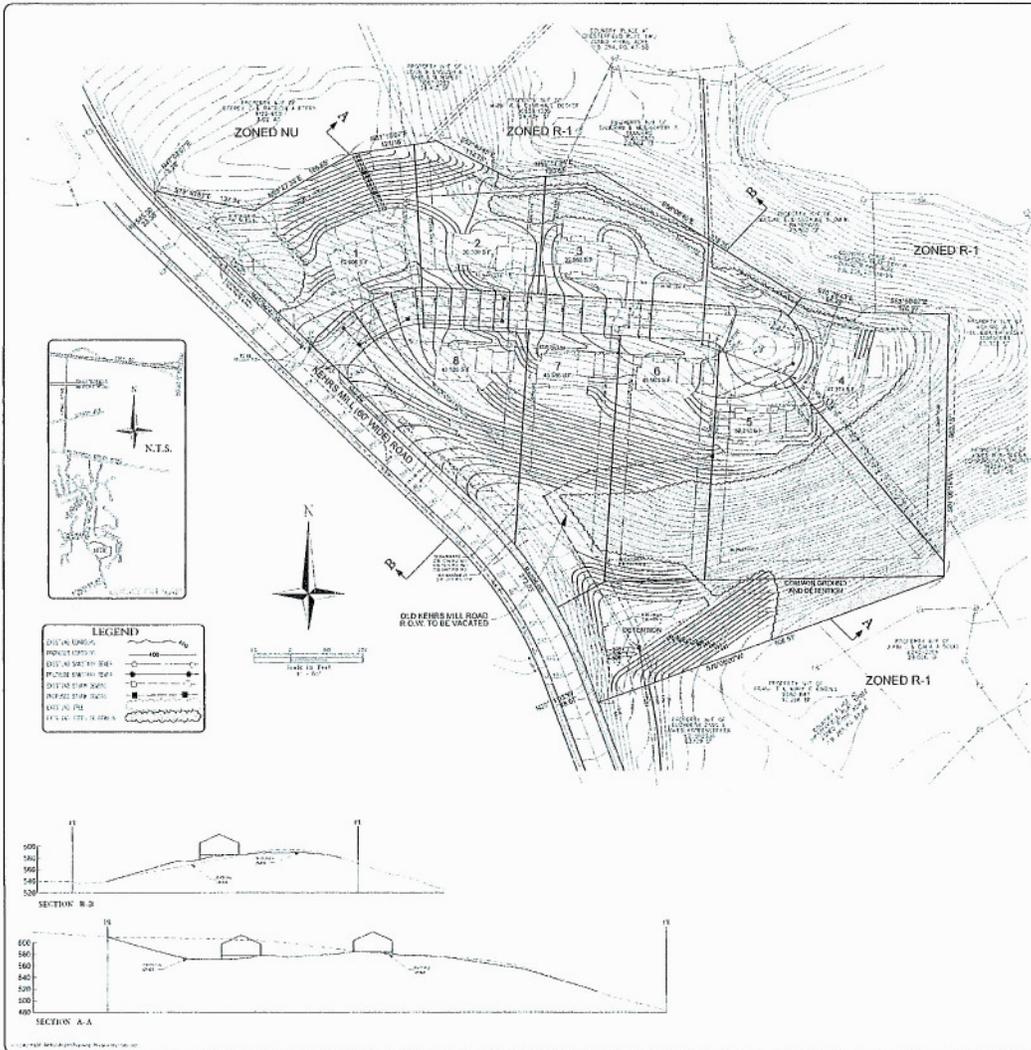
1.93 ACRES
 EXISTING TRIBUTARY
 TO "COUNTRY PLACE"
 AT CHESTERFIELD"



OLD KEHRS MILL ROAD
 R.O.W. TO BE VACATED



1.30 ACRES
 PROPOSED
 TRIBUTARY TO
 "COUNTRY PLACE AT
 CHESTERFIELD"



PROPERTY DESCRIPTION:

A certain portion of the... (Detailed description of the property, including acreage and location details.)

LEGEND:

- EXISTING DRAINAGE
- PROPOSED DRAINAGE
- EXISTING EASEMENTS
- PROPOSED EASEMENTS
- EXISTING UTILITIES
- PROPOSED UTILITIES

NOTES:

1. The proposed development is subject to the following conditions...
2. The proposed development is subject to the following conditions...
3. The proposed development is subject to the following conditions...
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17. The proposed development is subject to the following conditions...
18. The proposed development is subject to the following conditions...
19. The proposed development is subject to the following conditions...
20. The proposed development is subject to the following conditions...

PRELIMINARY PLAN

DEVELOPMENT PLAN

SCALE

DATE

DRAWN BY

CHECKED BY

APPROVED BY

VOLZ ENGINEERING

200 THUNDERBOLT ROAD
COLUMBIANA, MISSISSIPPI 39232

PRELIMINARY PLAN

DEVELOPMENT PLAN

SCALE

DATE

DRAWN BY

CHECKED BY

APPROVED BY

The Estates at Upper Kehrs Mill



To The Department of Planning

April 29, 2007

Re: The Upper Kehrs Mill Proposed Development.

During the last Planning and Zoning meeting Mr. Volz made a few comments I considered misleading, capricious and arbitrary.

- 1.) Mr. Volz talked about his conceived retention pond, but down played the fact that any overflow will end up in a lake within our subdivision. The measurement of silt resulting from that overflow would of course be measured by his company.
- 2.) Mr. Volz failed to inform the commission he plans to remove 8 feet to 10 feet from the top of the hill which would, of course, remove 1/3 of the entire hillside and the accompanying trees. An ecological disaster!
- 3.) Mr. Volz alluded to the fact that enough protections for the environment and existing property owners were already in place. A convenient conclusion and a self serving one.
- 4.) Mr. Volz thought that moving the planned cul de sac more than 10 feet from the Top of the hillside would cause him difficulties, evidently unconcerned with the Tremendous difficulties his current configurations would cause innocent existing Homeowners.

Observations: During my lifetime in the financial sales arena I have seen many techniques. One of the most effective and subsequently most devastating is the machine gun approach. The listener is rapidly fed a bewildering array of information until they are overwhelmed and then the speaker offers to "take care of everything" for them.

P.S. Let's not forget a secure letter of credit and due diligence.

Sincerely, William E. Quinn

ATTACHMENT A

All provisions of the City of Chesterfield City Code shall apply to this development except as specifically modified herein.

Comprehensive Plan Policies

- 2.1 **Quality Residential Development**
- 2.1.1 **Conservation of Existing Quality of Life**
- 2.1.4 **Compatible In-Fill Residential Construction**
- 2.1.5 **Provide Buffer for Existing Residential Development -**
- 2.1.6 **Reinforce Existing Residential Development Pattern -**
- 2.1.11 **Restrict Access of Individual Homes on Arterial Streets -**

I. SPECIFIC CRITERIA

A. PERMITTED USES

1. The uses allowed in this "E-One Acre" District shall be:
 - a. Single family detached residences.
2. The above uses in the "E-One Acre District shall be restricted as follows:
 - a. Development within this District shall maintain a density of one (1) acre).
 - b. The minimum lot size shall be no less than 22,000 square feet.

B. HEIGHT

The maximum height of the detached single family homes shall be fifty (50) feet.

C. SETBACKS

1. STRUCTURE SETBACKS

No building or structure, other than a freestanding subdivision monument sign, boundary and retaining walls, light standards, flag poles or fences will be located within the following setbacks:

- a. Eighty (80) feet from the right of way of Kehrs Mill Road.
- b. Twenty-five feet from the all other perimeter boundaries..

2. LOT CRITERIA

In addition to the above-referenced requirements, no building or structures other than boundary and retaining walls, light standards, flag poles or fences, the following lot criteria shall apply:

- a. Front yard: Twenty-five (25) feet from the internal road.
- b. Side yard: Twenty (20) feet from the side property line.
 - i. This side yard setback may be reduced up to five feet if the side yard setback on the adjacent property
 - ii. A minimum of forty (40) feet must be maintained between structures.
- c. Rear yard setback: Twenty-five (25) feet from the rear property line. .

D. PARKING AND LOADING REQUIREMENTS

1. Parking and loading spaces for this development will be as required in the City of Chesterfield Code.
2. Construction Parking
 - a. The streets surrounding this development and any street used for construction access thereto shall be cleaned throughout the day. The developer shall keep the road clear of mud and debris at all times.
 - b. Provide adequate off-street stabilized parking area(s) for construction employees and a washdown station for construction vehicles entering and leaving the site in order to eliminate the condition whereby mud from construction and employee vehicles is tracked onto the pavement causing hazardous roadway and driving conditions.

3. Parking lots shall not be used as streets.

E. LANDSCAPE AND TREE REQUIREMENTS

1. The developer shall adhere to the Tree Manual of the City of Chesterfield Code.
2. A minimum of 40% of the tree mass shall be maintained.

F. SIGN REQUIREMENTS

2. Ornamental Entrance Monument construction, if proposed, shall be reviewed by the City of Chesterfield, and/or the St. Louis County Department of Highways and Traffic, for sight distance considerations prior to installation or construction.
3. Signs shall be permitted in accordance with the regulations of the City of Chesterfield Code.

G. LIGHT REQUIREMENTS

Provide a lighting plan and cut sheet in accordance with the City of Chesterfield Code.

H. ARCHITECTURAL

1. Building facades should be articulated by using color, arrangement or change in materials to emphasize the facade elements. The planes of the exterior walls may be varied in height, depth or direction. Extremely long facades shall be designed with sufficient building articulation and landscaping to avoid a monotonous or overpowering appearance.

I. ACCESS/ACCESS MANAGEMENT

1. Access to Kehrs Mill Road shall be limited to one street approach. The street approach shall be as close to the north property line as practical and adequate sight distance shall be provided, as directed by the Department of Public Works and St. Louis County Department of Highways and Traffic.
2. If required sight distance cannot be provided at the access locations, acquisition of right-of-way, reconstruction of pavement including correction to the vertical alignment and other off-site improvements may be required to provide adequate sight distance

as directed by the Saint Louis County Department of Highways and Traffic

J. PUBLIC/PRIVATE ROAD IMPROVEMENTS, INCLUDING PEDESTRIAN CIRCULATION

1. Improve Kehrs Mill to one half of a sixty (60) foot right-of-way and a thirty nine (39) foot pavement including all storm drainage facilities as directed by the Saint Louis County Department of Highways and Traffic.
2. Provide a sidewalk conforming to Saint Louis County ADA standards adjacent to Kehrs Mill Road , as directed by the Saint Louis County Department of Highways and Traffic and the City of Chesterfield.
3. Any request to install a gate at the entrance to this development must be approved by the City of Chesterfield and the St. Louis County Department of Highways and Traffic. No gate installation will be permitted on public right of way.
4. If a gate is installed on a street in this development, the streets within the development or that portion of the development that is gated shall be private and remain private forever.
5. All roadway and related improvements in each plat or phase of the development shall be constructed prior to 60% occupancy of that plat or phase.

K. TRAFFIC STUDY (if applicable)

1. Provide a traffic study as directed by the City of Chesterfield and/or the St. Louis County Highways and Traffic. The scope of the study shall include internal and external circulation and may be limited to site specific impacts, such as the need for additional lanes, entrance configuration, geometrics, sight distance, traffic signal modifications or other improvements required, as long as the density of the proposed development falls within the parameters of the City's traffic model. Should the density be other than the density assumed in the model, regional issues shall be addressed as directed by the City of Chesterfield.
2. Provide a sight distance evaluation report, as required by the City of Chesterfield and/or St. Louis County Highways and Traffic for the proposed entrance onto Kehrs Mill Road. If adequate sight

distance cannot be provided at the access location, acquisition of right of way, reconstruction of pavement, including correction to the vertical alignment, and/or other off-site improvements shall be required, as directed by the City of Chesterfield and/or the Missouri Department of Transportation.

L. POWER OF REVIEW

The Mayor or a Councilmember of the Ward in which a development is proposed may request that the site plan be reviewed and approved by the entire City Council. This request must be made no later than 24 hours before posting the agenda for the next City Council meeting after Planning Commission review and approval of the site plan. The City Council will then take appropriate action relative to the proposal.

M. STORMWATER

1. The site shall provide for the positive drainage of storm water and it shall be discharged at an adequate natural discharge point or connected to an adequate piped system.
2. Detention/retention and other storm water quantity and quality management measures are to be provided in each watershed as required by the City of Chesterfield. The storm water quantity management facilities, related to flood and channel protection, shall be operational prior to paving of any driveways or parking areas in non-residential development or issuance of building permits exceeding sixty (60%) of approved dwelling units in each plat, watershed or phase of residential developments. The location and types of storm water management facilities shall be identified on the Site Development Plan.
3. Detention may be required for the entire project such that the release rates will not exceed the allowable release rates for the post developed peak flow of the 2-year and 100-year, 24 hour storm event. Stormwater must be discharged at an adequate discharge point. Wetland mitigation will not be allowable within the detention basin area
4. Lake(s), ponds(s), detention area(s), etc., are located downstream from the proposed development which may, in the opinion of the Chesterfield, be impacted by development of the subject site. A bond, in a form acceptable to the City of Chesterfield, shall be posted to assure compliance with this section. The developer shall perform preconstruction and post-construction surveys of these facilities and determine any changed condition. Preconstruction

surveys shall be performed prior to any clearing, grading, demolition or other construction related to the proposed development. Post-construction surveys shall be performed within twelve (12) months of the completion of the proposed development or two (2) years from the start of the development, whichever is greater. The developer shall return affected facilities to their preconstruction condition within 3 months of the post-construction survey. If the owner/operator of potentially impacted facilities will not grant the developer the necessary easements to complete the surveys and/or restorative work, the requirements in this paragraph are null and void. The required bond and preconstruction survey of downstream facilities shall be submitted prior to approval of a grading permit or improvement plans.

N. SANITARY SEWER

1. Treatment for water quality and channel protection, in accordance with MSD regulations dated February 2006, may be required..
2. This project is in the Caulks Creek Surcharge area and is subject to a surcharge of \$2,750 per acre.

O. GEOTECHNICAL REPORT.

Provide a geotechnical report, prepared by a registered professional engineer licensed to practice in the State of Missouri, as directed by the Department of Public Works. The report shall verify the suitability of grading and proposed improvements with soil and geologic conditions and address the existence of any potential sinkhole, ponds, dams, septic fields, etc., and recommendations for treatment. A statement of compliance, signed and sealed by the geotechnical engineer preparing the report, shall be included on all Site Development Plans and improvement plans

P. MISCELLANEOUS

1. All utilities will be installed underground. The development of this parcel will coordinate the installation of all utilities in conjunction with the construction of any roadway on site.
2. The developer is advised that utility companies will require compensation for relocation of their facilities with public road right-of-way. The developer should also be aware of extensive delays in utility company relocation and adjustments. Such delays will not

constitute a cause to allow occupancy prior to completion of road improvements.

3. Prior to Special Use Permit issuance by the Saint Louis County Department of Highways and Traffic, a special cash escrow or a special escrow supported by an Irrevocable Letter of Credit, must be established with the Saint Louis County Department of Highways and Traffic to guarantee completion of the required roadway improvements.
4. As this development is not subject to traffic generation assessment, the roadway improvements required herein represent the developer's road improvement obligation. These improvements will not exceed an amount established by multiplying the ordinance-required parking spaces by the following applicable rates:

II. TIME PERIOD FOR SUBMITTAL OF SITE DEVELOPMENT CONCEPT PLANS AND SITE DEVELOPMENT PLANS

1. The developer shall submit a concept plan within 18 months of City Council approval of the change of zoning.
2. In lieu of submitting a Site Development Concept Plan and Site Development Section Plans, the petitioner may submit a Site Development Plan for the entire development within 18 months of the date of approval of the change of zoning by the City.
3. Failure to comply with these submittal requirements will result in the expiration of the change of zoning and will require a new public hearing.
4. Said Plan shall be submitted in accordance with the combined requirements for Site Development Section and Concept Plans. The submission of Amended Site Development Plans by sections of this project to the Planning Commission shall be permitted if this option is utilized.
5. Where due cause is shown by the developer, this time interval for plan submittal may be extended through appeal to and approval by the Planning Commission.

III. COMMENCEMENT OF CONSTRUCTION

- A. Substantial construction shall commence within two (2) years of approval of the site development concept plan or site development plan, unless otherwise authorized by ordinance.

- B. Where due cause is shown by the developer, the Commission may extend the period to commence construction for not more than one additional year.

IV. GENERAL CRITERIA

A. Site Development Concept Plan, Site Development Section Plan, Site Development Plans.

1. Any site development plan shall show all information required on a sketch plan as required in the City of Chesterfield Code.
2. Include a conceptual landscape plan in accordance with the City of Chesterfield Code to indicate proposed landscaping along arterial and collector roadways.
3. Include a lighting plan in accordance with the City of Chesterfield Code to indicate proposed lighting along arterial collector roadways.
4. Provide comments/approvals from the appropriate Fire District, the Metropolitan St. Louis Sewer District, and the St. Louis County Department of Highways and Traffic.

V. RECORDING

Within 60 days of approval of any development plan by the City of Chesterfield, the approved Plan will be recorded with the St. Louis County Recorder of Deeds. Failure to do so will result in the expiration of approval of said plan and require re-approval of a plan by the Planning Commission.

VI. ENFORCEMENT

- A. The City of Chesterfield, Missouri will enforce the conditions of this ordinance in accordance with the Site Development Plan approved by the City of Chesterfield and the terms of this Attachment A.
- B. Failure to comply with any or all the conditions of this ordinance will be adequate cause for revocation of approvals/permits by reviewing Departments and Commissions.

- C. Non-compliance with the specific requirements and conditions set forth in this Ordinance and its attached conditions or other Ordinances of the City of Chesterfield shall constitute an ordinance violation, subject, but not limited to, the penalty provisions as set forth in the City of Chesterfield Code.
- D. Waiver of Notice of Violation per the City of Chesterfield Code.
- E. This document shall be read as a whole and any inconsistency to be integrated to carry out the overall intent of this Attachment A.



VIII.C

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

Planning Commission Staff Report

Subject: Rezoning Vote Report

Meeting Date: May 14, 2007

From: Aimee Nassif, Senior Planner of Zoning Administration

Location: 18652, 18630, 18650, 18660 Olive Street Road

Petition: P.Z. 07-2007 Spirit Valley Business Park

Proposal Summary

Michael Doster, on behalf of Spirit Valley Business Park, has submitted an application for a change of zoning from "NU" Non Urban District to "PI" Planned Industrial District per the regulations of City of Chesterfield Zoning Ordinance Section 1003.150. The location of the site is north of Olive Street Road and east of Eatherton.

Staff Recommendation

The Attachment A for this request meets all of the development requirements of the City of Chesterfield and therefore, Staff recommends approval of the change of zoning from "NU" Non Urban District to "PI" Planned Industrial District.

Zoning Analysis

A preliminary plan accompanies all rezoning requests when the change of zoning is to a Planned District. When a vote is taken on a rezoning request, the vote is to approve the change of zoning with an Attachment A written by Staff. The vote is not to approve the accompanying preliminary plan which is provided for informational purposes only.

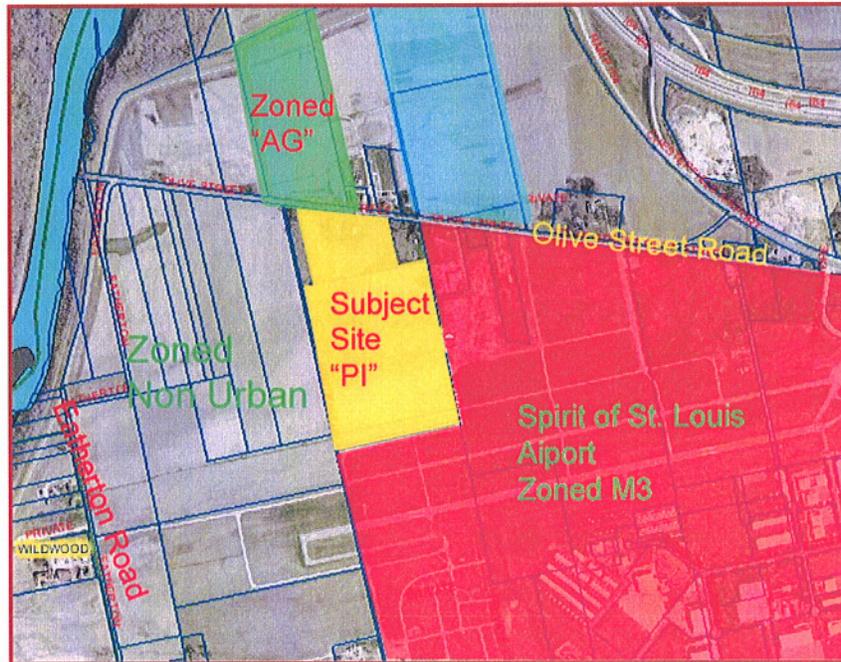
While preparing the Attachment A for this development, Staff reviewed the conditions established in the Comprehensive Plan and Zoning Ordinance. The Attachment A requires that this development will adhere to the requirements of both.

The rezoning request along with the uses requested is compatible with this area and the surrounding developments.

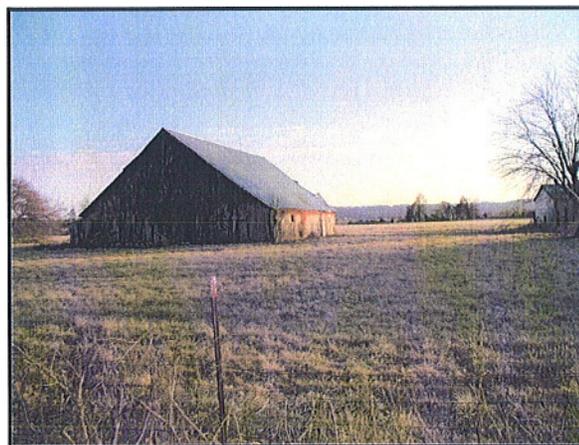
Surrounding Land Use and Zoning

The land use and zoning for the properties surrounding this parcel are as follows:

- North: The property to the north is zoned "AG" Agricultural District.
- South/East: The property to the south and east of the site is Spirit of St. Louis Airport which is zoned "M3" Planned Industrial District.
- West: The property to the west is zoned "NU" Non Urban District.



View looking south across subject site.
Residential structure is currently on site.



View looking south across subject site.
Barn structure is currently on site.

Comprehensive Plan Analysis

The subject site is located within Ward 4 of the City of Chesterfield. The Comprehensive Plan calls for this area to be Low-Intensity Industrial. This subject site is located in Sub Area 1 of the Comprehensive Plan which identifies additional development criteria which have been incorporated into the Attachment A.

The parking requirement for this Sub Area is 4 parking spaces per every 1,000 square feet of development. The Petitioner is requesting instead that the parking regulations of the City of Chesterfield Zoning Ordinance Section 1003.165 be applied which establishes parking requirements on the use for each building.

Site Area History

The subject site was zoned "NU" Non Urban District by St. Louis County in 1965 prior to the incorporation of the City of Chesterfield. The site currently has one (1) barn which was built in 1950 and one (1) residential structure which was built in 1957.

Issues

A public hearing was held on this request on February 26, 2007. At that time there were no speakers on this matter other than the Petitioner. At the hearing, several issues were identified. Those issues along with the Petitioner's responses are attached. Currently, all issues have been addressed. The Petitioner is requesting that parking for this development adhere to the requirements in Section 1003.165 of the Zoning Ordinance in lieu of the Comprehensive Plan requirement of 4 spaces for every 1,000 square feet of development.

Request

Staff recommends approval of the change of zoning from "NU" Non Urban District to a "PI" Planned Industrial District with the Attachment A as written. Staff is also requesting action on Petitioner's request regarding the parking requirement.

Respectfully submitted,



Aimee Nassif
Senior Planner of Zoning Administration

Attachments

1. Attachment A
2. Preliminary Plan
3. Response Letter from Petitioner

ATTACHMENT A

All provisions of the City of Chesterfield City Code shall apply to this development except as specifically modified herein.

I. SPECIFIC CRITERIA

A. PERMITTED USES

1. The uses allowed in this "PI" District shall be:
 - a. Animal hospitals, veterinary clinics.
 - b. Meat packing facilities.
 - c. Broadcasting studios for radio and television.
 - d. Broadcasting, transmitting, or relay towers, studios, and associated facilities for radio, television, and other communications.
 - e. Business, professional, and technical training schools.
 - f. Business service establishments.
 - g. Cafeterias for employees and guests only.
 - h. Churches shall be allowed on tracts of land of at least one (1) acre in size.
 - i. Financial institutions.
 - j. Filling stations, including emergency towing and repair services.
 - k. Gymnasiums, indoor swimming pools, indoor handball and racquetball courts (public or private), and indoor and unlighted outdoor tennis courts (public or private).
 - l. Storage and charter of boats on land, repair facilities for boats, and sale of fuel and other supplies for marine use.
 - m. Highway department garages.
 - n. Hotels and motels.
 - o. Laundries and dry cleaning plants, not including personal and individual drop-off and pick-up service.
 - p. Local public utility facilities, provided that any installation, other than poles and equipment attached to the poles, shall be:
 - i. Adequately screened with landscaping, fencing or walls, or any combination thereof; or
 - ii. Placed underground; or
 - iii. Enclosed in a structure in such a manner so as to blend with and complement the character of the surrounding area.

All plans for screening these facilities shall be submitted to the Department of Planning for review. No building permit or

installation permit shall be issued until these plans have been approved by the Department of Planning.

- q. Mail order sale warehouses.
- r. Manufacturing, fabrication, assembly, processing, or packaging of any commodity except:
 - i. Facilities producing or processing explosives or flammable gases or liquids;
 - ii. Facilities for animal slaughtering or rendering;
 - iii. Sulphur plants, rubber reclamation plants, or cement plants; and
 - iv. Steel mills, foundries, or smelters.
- s. Medical and dental offices.
- t. Office or office buildings.
- u. Parking areas, including garages, for automobiles, but not including any sales of automobiles, or the storage of wrecked or otherwise damaged and immobilized automotive vehicles for a period in excess of seventy-two (72) hours.
- v. Plumbing, electrical, air conditioning, and heating equipment sales, warehousing and repair facilities.
- w. Police, fire, and postal stations.
- x. Printing and duplicating services.
- y. Public utility facilities.
- z. Research facilities, professional and scientific laboratories, including photographic processing laboratories used in conjunction therewith.
- aa. Restaurants, fast food.
- bb. Restaurants, sit down.
- cc. Sales, rental, and leasing of new and used vehicles, including automobiles, trucks, trailers, construction equipment, agricultural equipment, and boats, as well as associated repairs and necessary outdoor storage of said vehicles.
- dd. Sales, servicing, repairing, cleaning, renting, leasing, and necessary outdoor storage of equipment and vehicles used by business, industry, and agriculture.
- ee. Service facilities, studios, or work areas, for antique salespersons, artists, candy makers, craft persons, dressmakers, tailors, music teachers, dance teachers, typists, and stenographers, including cabinet makers, film processors. Goods and services associated with these uses

may be sold or provided directly to the public on the premises.

- ff. Storage and repair garages for public mass transit vehicles.
- gg. Storage yards for lumber, coal, and construction materials.
- hh. Stores, shops, markets, service facilities, and automotive vending facilities in which goods or services of any kind, including sale of motor vehicles, are being offered for sale or hire to the general public on the premises.
- ii. Union halls and hiring halls.
- jj. Vehicle repair facilities.
- kk. Vehicle service centers.
- ll. Welding, sheet metal, and blacksmith shops.
- mm. Yards for storage of contractors' equipment, materials, and supplies, excluding junk yards and salvage yards.

2. The following Ancillary Uses shall be permitted:

a. Automatic vending facilities for:

- i. Ice and solid carbon dioxide (dry ice);
- ii. Beverages;
- iii. Confections.

B. FLOOR AREA, HEIGHT, BUILDING AND PARKING STRUCTURE REQUIREMENTS

1. HEIGHT

- a. The maximum height of the buildings, exclusive of roof screening, shall not exceed forty (40) feet.

2. BUILDING REQUIREMENTS

- a. Until such time as the connection of the site to a permanent sanitary sewer system, a minimum fifty percent (50%) open space is required for this development. Upon connection to a sanitary sewer system, a minimum of thirty percent (30%) openspace will be required for this development.

C. SETBACKS

1. STRUCTURE SETBACKS

No building or structure, other than: a freestanding project identification sign, boundary and retaining walls, light standards, flag poles or fences will be located within the following setbacks:

- a. Fifty (50) feet from the right-of-way of Olive Street Road.
- b. Fifty (50) feet from 18668 Olive Street Road.
- c. Fifty (50) feet from 18626 Olive Street Road.
- d. Fifty (50) feet from the eastern and western property lines of this development.
- e. Fifty (50) feet from the southern property line of this development.

2. PARKING SETBACKS

No parking stall or loading space will be located within the following setbacks:

- a. Thirty five (35) feet from the right-of-way Olive Street Road.
- b. Ten (10) feet from the internal property lines, with the exception of shared driveways.
- c. Twenty (20) feet from the principal internal street.
- d. Fifteen (15) feet from the eastern and western property lines of this development.
- e. The parking setback along the western property line contiguous to 18668 Olive Street Road for the first 175 feet from Olive Street Road shall be thirty (30) feet. The parking setback for the remainder of this property line shall be fifteen (15) feet.
- f. Fifteen (15) feet from the northern property line bearing S78°19'49"W.

3. No internal driveway or roadway, except points of ingress and egress, will be located within the following setbacks:
 - a. Thirty (30) feet from the right-of-way of Olive Street Road.
 - b. Three hundred fifty (350) feet from the southern property line.
 - c. Two hundred seventy five (275) feet from the eastern and western property lines of this development.
 - d. Zero (0) feet from the eastern property line bearing N11°25'51"W.

D. PARKING AND LOADING REQUIREMENTS

1. Parking and loading spaces for this development will be 4 spaces for every 1,000 square feet of development.
2. Construction Parking
 - a. The streets surrounding this development and any street used for construction access thereto shall be cleaned throughout the day. The developer shall keep the road clear of mud and debris at all times.
 - b. Provide adequate off-street stabilized parking area(s) for construction employees and a washdown station for construction vehicles entering and leaving the site in order to eliminate the condition whereby mud from construction and employee vehicles is tracked onto the pavement causing hazardous roadway and driving conditions.
 - c. No construction related parking shall be permitted within the Olive Street Road right-of-way.
3. No parking shall be permitted on any roadway in or adjacent to the development. The parking restriction and requirement for signage shall be indicated on the Site Development Plan and improvement plans. Signage shall be posted within thirty (30) days of the placement of street pavement.

E. LANDSCAPE AND TREE REQUIREMENTS

1. The developer shall adhere to the Tree Manual of the City of Chesterfield Code.
2. The landscape buffer along the western property line shall be as follows:
 - a. A thirty (30) foot landscape buffer measuring no less than 175 feet in length from Olive Street Road will be required along the western property line contiguous to the residential structure at 18668 Olive Street Road.
 - b. A fifteen (15) foot landscape buffer shall be required for the remainder of the western property line.
 - c. If the residential structure at 18668 Olive Street Road is re-developed into a use other than residential, the thirty (30) foot landscape buffer requirement may be reduced to fifteen (15) feet as directed by the City of Chesterfield.
3. There shall be a fifteen (15) foot landscape buffer along the northern property line bearing S78°19'49"W.

F. SIGN REQUIREMENTS

1. Sign package submittal materials shall be required for this development. All sign packages shall be reviewed and approved by the City of Chesterfield Planning Commission.
2. Ornamental Entrance Monument construction, if proposed, shall be reviewed by the City of Chesterfield, and/or the St. Louis County Department of Highways and Traffic, for sight distance considerations prior to installation or construction.

G. LIGHT REQUIREMENTS

1. Provide a lighting plan and cut sheet in accordance with the City of Chesterfield Code.

H. ARCHITECTURAL

1. The developer shall submit architectural elevations, including but not limited to, colored renderings and building materials. Architectural information is to be reviewed by the Architectural Review Board and the Planning Commission.

2. Building facades should be articulated by using color, arrangement or change in materials to emphasize the facade elements. The planes of the exterior walls may be varied in height, depth or direction. Extremely long facades shall be designed with sufficient building articulation and landscaping to avoid a monotonous or overpowering appearance.
3. Trash enclosures: The location, material, and elevation of any trash enclosures will be as approved by the Planning Commission on the Site Development Plan. All exterior trash areas will be enclosed with a six (6) foot high sight-proof enclosure complimented by adequate landscaping approved by the Planning Commission on the Site Development Plan.
4. Mechanical equipment will be adequately screened by roofing or other material as approved by the Planning Commission.

I. ACCESS/ACCESS MANAGEMENT

1. Access to Olive Street Road shall be limited to two (2) street approaches, as directed by the Department of Public Works and St. Louis County Department of Highways and Traffic.
2. Provide for cross access to the properties adjoining this development to the east and west, as directed by the City of Chesterfield, to allow for future connection to the internal roadway system within this development.

J. PUBLIC/PRIVATE ROAD IMPROVEMENTS, INCLUDING PEDESTRIAN CIRCULATION

1. Provide a street connection to the property to the west. The connection shall be a minimum of five hundred (500) feet from Olive Street Road, as directed by the City of Chesterfield.
2. All roadway and related improvements shall be constructed prior to 60% occupancy of the development.
3. Provide any additional right of way and construct any improvements to Olive Street Road as required by St. Louis County Department of Highways and Traffic.
4. Provide a 40 foot right of way with a minimum of 10 foot roadway improvement, maintenance, utility and sewer easement son both sides for all interior roadways. Minimum roadway sections shall provide a 26 foot travel way with 7 foot shoulders on both sides and

appurtenant storm drainage facilities as required by the Department of Public Works.

5. Provide a five (5) foot wide sidewalk, conforming to ADA standards, along the Olive Street Road frontage of the site and along one (1) side of all interior roadways, as directed by the City of Chesterfield. The sidewalk shall be privately maintained; therefore, no public easements shall be required.
6. Improve Olive Street Road to one half of a sixty (60) foot right-of-way and a thirty eight (38) foot pavement with seven (7) foot full depth shoulders and additional widening to provide a minimum thirty three (33) foot wide road surface for a two hundred (200) foot left turn lane serving each entrance along with fifty to one (50:1) tapers and including all storm drainage facilities as directed by the St. Louis County Department of Highways and Traffic. Widening may be required on opposite side of street to provide a minimum thirty three (33) foot pavement for three driving lanes in addition to seven (7) foot shoulder.
7. Access to this development from Olive Street Road shall be restricted to two (2) commercial entrances, each having a three (3) lane section of a minimum thirty six (36) foot pavement for a minimum distance of one hundred (100) feet, in order to provide separate left and right turn lanes for outbound traffic along with a single inbound lane. These driveways shall be located to provide required sight distance and constructed to St. Louis County standards as directed by the St. Louis County Department of Highways and Traffic and the City of Chesterfield. The easternmost driveway serving this site should be located opposite an existing commercial entrance on the north side of Olive Street Road in this area.
8. Provide cross access easement and temporary slope construction license or other appropriate legal instrument or agreement guaranteeing permanent access between this site and adjacent properties as directed by the St. Louis County Department of Highways and Traffic and the City of Chesterfield.
9. If required sight distance cannot be provided at the access locations, acquisition of right-of-way, reconstruction of pavement including correction to the vertical alignment and other off-site improvements may be required to provide adequate sight distance as directed by the St. Louis County Department of Highways and Traffic.

K. TRAFFIC STUDY

1. Provide a traffic study as directed by the City of Chesterfield and/or the St. Louis County Department of Highways and Traffic. The scope of the study shall include internal and external circulation and may be limited to site specific impacts, such as the need for additional lanes, entrance configuration, geometrics, sight distance, traffic signal modifications or other improvements required, as long as the density of the proposed development falls within the parameters of the City's traffic model. Should the density be other than the density assumed in the model, regional issues shall be addressed as directed by the City of Chesterfield.

L. MONARCH-CHESTERFIELD LEVEE DISTRICT

1. East entrance location will require relocation of fire hydrant proposed on the MCLD Sewer Improvements.
2. Stormwater ditches shall match the latest provided by the City of Chesterfield.
3. The easement that will be required for the proposed MCLD force main at the southwest corner of the Albrecht tract and the easements for the proposed utilities along Olive Street Road shall be shown on the site development concept and section plans.

M. POWER OF REVIEW

The Mayor or a Councilmember of the Ward in which a development is proposed may request that the site plan be reviewed and approved by the entire City Council. This request must be made no later than 24 hours before posting the agenda for the next City Council meeting after Planning Commission review and approval of the site plan. The City Council will then take appropriate action relative to the proposal.

N. STORMWATER

1. Due to the inherent nature of development, the specific size, location, and configuration of the storm water infrastructure associated with the Chesterfield Valley Master Storm Water Plan are conceptual in nature. The exact location, size, and type of each segment of storm water infrastructure are to be reviewed and approved in conjunction with the development of specific sites. The developer will be required to construct ditches along the west, north, and east property lines of the property governed by this ordinance.

The developer may elect to propose alternate geometry, size and/or type of storm sewer improvements that are functionally equivalent to the required improvements. Functional equivalence is said to be achieved when, as determined by the City of Chesterfield, the alternate proposal provides the same hydraulic function, connectivity, and system-wide benefits without adversely affecting any of the following: water surface profiles at any location outside the development; future capital expenditures; maintenance obligations; equipment needs; frequency of maintenance; and probability of malfunction. The City will consider, but is not obligated to accept, the developer's alternate plans. If the City determines that the developer's proposal may be functionally equivalent to the Chesterfield Valley Master Storm Water Plan improvements, hydraulic routing calculations will be performed to make a final determination of functional equivalence. The Director will consider the developer's proposal, but is not obligated to have the hydraulic analysis performed if any of the other criteria regarding functional equivalence will not be met. The hydraulic routing calculations regarding functional equivalence may be performed by a consultant retained by the City. The developer shall be responsible for all costs related to the work performed by the consultant.

2. Provide any additional Chesterfield Valley Storm Water Easements along the north, east, and west property lines as required and directed by the Department of Public Works to accommodate the Chesterfield Valley Master Storm Water Plan channel in those areas, and depict the channel(s) on the Site Development Plan and improvement plans. The maintenance of the required storm water/ditch system shall be the responsibility of the property owner(s).
3. All Chesterfield Valley Master Storm Water Plan improvements shall be operational prior to the paving of any driveways or parking areas, as directed by the City of Chesterfield.
4. The site shall provide for the positive drainage of storm water and it shall be discharged at an adequate natural discharge point or connected to an adequate piped system.
5. Detention/retention and other storm water quantity and quality management measures are to be provided in each watershed as required by the City of Chesterfield. The storm water quantity management facilities, related to flood and channel protection, shall be operational prior to paving of any driveways or parking areas in non-residential development or issuance of building permits

exceeding sixty (60%) of approved dwelling units in each plat, watershed or phase of residential developments. The location and types of storm water management facilities shall be identified on the Site Development Plan.

6. Stormwater should be controlled as required by the Chesterfield Valley Master Facility Plan.

O. SANITARY SEWER

1. Private sewer lines and laterals cannot cross property lines. Extension of public sewer lines will be necessary to serve this site and proper easements required.
2. Downstream pump stations and force mains shall be evaluated to ensure adequate capacity.
3. Provide public sewer service for the site, including sanitary force main, gravity lines and/or regional pump stations, as directed by the Metropolitan St. Louis Sewer District and the City of Chesterfield.
4. This project is in the Caulks Creek Surcharge Area and is subject to a surcharge of \$2750.00 per acre.

P. GEOTECHNICAL REPORT.

Prior to Site Development Concept Plan approval, provide a geotechnical report, prepared by a registered professional engineer licensed to practice in the State of Missouri, as directed by the Department of Public Works. The report shall verify the suitability of grading and proposed improvements with soil and geologic conditions and address the existence of any potential sinkhole, ponds, dams, septic fields, etc., and recommendations for treatment. A statement of compliance, signed and sealed by the geotechnical engineer preparing the report, shall be included on all Site Development Plans and Improvement Plans.

Q. MISCELLANEOUS

1. All utilities will be installed underground. The development of this parcel will coordinate the installation of all utilities in conjunction with the construction of any roadway on site.
2. The developer is advised that utility companies will require compensation for relocation of their facilities with public road right-of-way. Utility relocation cost shall not be considered as an

allowable credit against the petitioner's traffic generation assessment contribution. The developer should also be aware of extensive delays in utility company relocation and adjustments. Such delays will not constitute a cause to allow occupancy prior to completion of road improvements.

II. TIME PERIOD FOR SUBMITTAL OF SITE DEVELOPMENT CONCEPT PLANS AND SITE DEVELOPMENT PLANS

- A. The developer shall submit a concept plan within 18 months of City Council approval of the change of zoning.
- B. In lieu of submitting a Site Development Concept Plan and Site Development Section Plans, the petitioner may submit a Site Development Plan for the entire development within 18 months of the date of approval of the change of zoning by the City.
- C. Failure to comply with these submittal requirements will result in the expiration of the change of zoning and will require a new public hearing.
- D. Said Plan shall be submitted in accordance with the combined requirements for Site Development Section and Concept Plans. The submission of Amended Site Development Plans by sections of this project to the Planning Commission shall be permitted if this option is utilized.
- E. Where due cause is shown by the developer, this time interval for plan submittal may be extended through appeal to and approval by the Planning Commission.

III. COMMENCEMENT OF CONSTRUCTION

- A. Substantial construction shall commence within two (2) years of approval of the site development concept plan or site development plan, unless otherwise authorized by ordinance.
- B. Where due cause is shown by the developer, the Commission may extend the period to commence construction for not more than one additional year.

IV. GENERAL CRITERIA

A. Site Development Concept Plan, Site Development Section Plan, Site Development Plans.

1. Any site development plan shall show all information required on a sketch plan as required in the City of Chesterfield Code.
2. Include a conceptual landscape plan in accordance with the City of Chesterfield Code to indicate proposed landscaping along arterial and collector roadways.
3. Include a lighting plan in accordance with the City of Chesterfield Code to indicate proposed lighting along arterial collector roadways.
4. Provide comments/approvals from the appropriate Fire District, St. Louis County Department of Highways and Traffic, Monarch Chesterfield Levee District, Spirit of St. Louis Airport and St. Louis County Department of Highways and Traffic.

V. TRUST FUND CONTRIBUTION

- A. The developer shall be required to contribute to the Chesterfield Valley Trust Fund. Traffic generation assessment contributions shall be deposited with St. Louis County prior to the issuance of building permits. If development phasing is anticipated, the developer shall provide the traffic generation assessment contribution prior to issuance of building permits for each phase of development.

Roads

The amount of the developer's contribution to this fund shall be computed based on the following:

<u>Type of Development</u>	<u>Required Contribution</u>
Commercial	\$2.07/sq.ft.of building space
Office	\$1.44/ sq.ft.of building space
Industrial	\$4,986.59/acre

(Parking spaces as required by the City of Chesterfield Code.)

If types of development differ from those listed, the Department of Highways and Traffic will provide rates.

Credits for roadway improvements will be awarded by the City of Chesterfield and/or St. Louis County Department of Highways and Traffic. Any portion of the roadway improvement contribution that remains, following completion of road improvements as required by the development shall be retained in the appropriate Trust Fund.

The roadway improvement contribution shall be deposited with the Saint Louis County Department of Highways and Traffic. The deposit shall be made before the issuance of a Special Use Permit (SUP) by the Saint Louis County Highways and Traffic. Funds shall be payable to the "Treasurer, Saint Louis County."

Trust fund contributions shall be deposited with Saint Louis County in the form of a cash escrow prior to the issuance of building permits.

Water Main

The primary water line contribution is based on gross acreage of the development land area. The contribution shall be a sum of \$654.66 per acre for that total area as approved on the Site Development Plan to be used solely to help defray the cost of construction the primary water line serving the Chesterfield Valley area.

The primary water line contribution shall be deposited with the St. Louis County Department of Highways and Traffic. The deposit shall be made before approval of the Site Development Plan by St. Louis County Highways and Traffic. Funds shall be payable to the "Treasurer, Saint Louis County".

Stormwater

The storm water contribution is based on gross acreage of the development land area. These funds are necessary to help defray the cost of engineering and construction improvements for the collection and disposal of storm water from the Chesterfield Valley in accordance with the Master Plan on file with and jointly approved by Saint Louis County and MSD. The amount of storm water contribution will be computed based on \$2,077.15 per acre for the total area as approved on the Site Development Plan. The storm water contributions to the Trust Fund shall be deposited with the Saint Louis County Department of Highways and Traffic. The deposit shall be made before the issuance of a Special Use Permit (SUP) by Saint Louis County Department of Highways and Traffic. Funds shall be payable to the "Treasure, Saint Louis County."

Sanitary Sewer

The sanitary sewer contribution is collected as the Caulks Creek Impact Fee. The sanitary sewer contribution within Chesterfield Valley area shall be deposited with MSD as required by the District.

The amount of these required contributions for the roadway, storm water and primary water line improvements, if not submitted by January 1, 2008 shall be

adjusted on that date and on the first day of January in each succeeding year thereafter in accordance with the construction cost index as determined by the Saint Louis County Department of Highways and Traffic.

Trust Fund contributions shall be deposited with St. Louis County in the form of a cash escrow prior to the issuance of building permits.

VI. RECORDING

Within 60 days of approval of any development plan by the City of Chesterfield, the approved Plan will be recorded with the St. Louis County Recorder of Deeds. Failure to do so will result in the expiration of approval of said plan and require re-approval of a plan by the Planning Commission.

VII. VERIFICATION PRIOR TO RECORD PLAT APPROVAL

The developer shall cause, at his expense and prior to the recording of any plat, the re-establishment, restoration or appropriate witnessing of all Corners of the United States Public Land Survey located within, or which define or lie upon, the outboundaries of the subject tract in accordance with the Missouri Minimum Standards relating to the preservation and maintenance of the United States Public Land Survey Corners.

VIII. FINAL RELEASE OF SUBDIVISION DEPOSITS

Prior to final release of subdivision construction deposits, the developer shall provide certification by a registered land surveyor that all monumentation depicted on the record plat has been installed and United States Public Land Survey Corners have not been disturbed during construction activities or that they have been reestablished and the appropriate documents filed with the Missouri Department of Natural Resources Land Survey Program.

IX. ENFORCEMENT

- A. The City of Chesterfield, Missouri will enforce the conditions of this ordinance in accordance with the Plan approved by the City of Chesterfield and the terms of this Attachment A.
- B. Failure to comply with any or all the conditions of this ordinance will be adequate cause for revocation of approvals/permits by reviewing Departments and Commissions.
- C. Non-compliance with the specific requirements and conditions set forth in this Ordinance and its attached conditions or other Ordinances of the City of Chesterfield shall constitute an ordinance violation, subject, but not

limited to, the penalty provisions as set forth in the City of Chesterfield Code.

- D. Waiver of Notice of Violation per the City of Chesterfield Code.
- E. This document shall be read as a whole and any inconsistency to be integrated to carry out the overall intent of this Attachment A.

Spirit Valley Business Park – Revised Proposed "PI" Uses

PERMITTED USES:

- (b) Animal hospitals, veterinary clinics, ~~and kennels.~~
- (c) ~~Animal slaughtering, meat packing or rendering facilities.~~
- (e) ~~Arenas and stadiums.~~
- (f) ~~Auditoriums, churches, clubs, lodges, meeting rooms, libraries, reading rooms, theaters, or any other facility for public assembly.~~
- (h) Broadcasting studios for radio and television.
- (i) Broadcasting, transmitting, or relay towers, studios, and associated facilities for radio, television, and other communications.
- (j) Business, professional, and technical training schools.
- (k) Business service establishments.
- (l) Cafeterias for employees and guests only.
- (n) Churches shall be allowed on tracts of land of at least one acre in area.
- (q) Financial institutions.
- (u) Filling stations, including emergency towing and repair services.
- (v) Gymnasiums, indoor swimming pools, indoor handball and racquetball courts (public or private), and indoor and unlighted outdoor tennis courts (public or private).
- (w) ~~Harbors, marinas, and docks for water-borne vehicles, including~~ storage and charter of boats, on land ~~or in the water~~, repair facilities for boats, and sale of fuel and other supplies for marine use.
- (x) Highway Department garages.
- (y) Hotels and motels.
- (bb) Laundries and dry cleaning plants, not including personal and individual drop-off and pick-up service.
- (cc) Local public utility facilities, provided that any installation, other than poles and equipment attached to the poles, shall be:
 - (i) Adequately screened with landscaping, fencing or walls, or any combination thereof; or
 - (ii) Placed underground; or

- (iii) Enclosed in a structure in such a manner so as to blend with and complement the character of the surrounding area.

All plans for screening these facilities shall be submitted to the Department of Planning for review. No building permit or installation permit shall be issued until these plans have been approved by the Department of Planning.

- (dd) Mail order sale warehouses.
- (ee) ~~Manufacturing, fabrication and processing of flammable gases, liquids, and explosives.~~
- (ff) Manufacturing, fabrication, assembly, processing, or packaging of any commodity except:
 - (i) ~~Facilities producing or processing explosives or flammable gases or liquids;~~
 - (ii) Facilities for animal slaughtering, ~~meat packing~~, or rendering;
 - (iii) Sulphur plants, rubber reclamation plants, or cement plants; and
 - (iv) Steel mills, foundries, or smelters.
- (gg) Medical and dental offices.
- (ii) Office or Office Buildings.
- (ll) Parking areas, including garages, for automobiles, but not including any sales of automobiles, or the storage of wrecked or otherwise damaged and immobilized automotive vehicles for a period in excess of seventy-two (72) hours.
- (mm) Plumbing, electrical, air conditioning, and heating equipment sales, warehousing and repair facilities.
- (nn) Police, fire, and postal stations.
- (oo) Printing and duplicating services.
- (pp) Public utility facilities.
- (tt) ~~Recreational facilities, indoor and illuminated outdoor facilities, including swimming pools, golf courses, golf practice driving ranges, tennis courts, and gymnasiums, and indoor theaters, including drive-in theaters.~~
- (uu) Research facilities, professional and scientific laboratories, including photographic processing laboratories used in conjunction therewith.
- (vv) Restaurants, fast food.
- (ww) Restaurants, sit down.
- (xx) Sales, rental, and leasing of new and used vehicles, including automobiles, trucks, trailers, construction equipment, agricultural equipment, and boats, as well as associated repairs and necessary outdoor storage of said vehicles.

- (yy) Sales, servicing, repairing, cleaning, renting, leasing, and necessary outdoor storage of equipment and vehicles used by business, industry, and agriculture.
- (zz) ~~Sales yards operated for a charitable purpose by a church, school, or other not-for-profit organization.~~
- (ccc) Service facilities, studios, or work areas for antique salespersons, artists, candy makers, craftpersons, dressmakers, tailors, music teachers, dance teachers, typists, and stenographers, including cabinet makers, film processors, ~~fishing tackle and bait shops, and souvenir sales~~. Goods and services associated with these uses may be sold or provided directly to the public on the premises.
- (eee) ~~Permitted signs (See Section 1003.168 'Sign Regulations').~~
- (ggg) Storage and repair garages for public mass transit vehicles.
- (hhh) Storage yards for lumber, coal, and construction materials.
- (iii) Stores, shops, markets, service facilities, and automatic vending facilities in which goods or services of any kind, including indoor sale of motor vehicles, are being offered for sale or hire to the general public on the premises.
- (nnn) Union halls and hiring halls.
- (ooo) Vehicle repair facilities.
- (ppp) Vehicle service centers.
- (qqq) Vehicle washing facilities.
- (sss) Welding, sheet metal, and blacksmith shops.
- (ttt) Yards for storage of contractors' equipment, materials, and supplies, excluding junk yards and salvage yards.

ANCILLARY USES:

- (g) Automatic vending facilities for:
 - (i) Ice and solid carbon dioxide (dry ice);
 - (ii) Beverages;
 - (iii) Confections.

St. Louis
17107 Chesterfield Airport Rd.
Suite 300
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(636) 532-0042
(636) 532-1082 (fax)



Kansas City
4600 Madison
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(816) 531-1888
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Michael J. Doster
mike_doster@dmjulaw.com

Reply to St. Louis Office

May 3, 2007

HAND DELIVERED

Ms. Aimee E. Nassif, Senior Planner
City of Chesterfield
690 Chesterfield Parkway West
Chesterfield, Missouri 63017

Re: P.Z. 7-2007 ---- Spirit Valley Business Park

Dear Aimee:

In response to your letter dated April 27, 2007, we offer the following responses:

1. Provide comments from the St. Louis County Department of Highways and Traffic and the Monarch Fire Protection District.

Response: Comments from these agencies are forthcoming.

2. The Comprehensive Land Use Plan designates this area as low intensity industrial. The primary uses in this area according to the Comprehensive Plan, includes manufacturing, fabrication, assembly, processing, warehousing and storage.

Response: We believe the uses proposed for this development, as amended, are compatible with the Comprehensive Land Use Plan for this area of Chesterfield Valley

3. This development is located in Sub-Area 1 of the Chesterfield Valley.

Response: Spirit Valley Business Park, located in Sub-Area 1 of Chesterfield Valley, proposes "Low-Intensity Industrial" uses, consistent with the Comprehensive Plan. The development guidelines of Sub-Area 1 include:

1. Retention of 50% Open Space;
2. Maximum building height of forty (40) feet, exclusive of mechanical equipment;
3. Parking ratio of 4.0 spaces/1,000 square-feet of development; and
4. Requirement that all new development be reviewed by the Architectural Review Board for design considerations.

Items 1, 2 and 3 are addressed later in this letter. We acknowledge the fourth guideline regarding the Architectural Review Board.

4. The current preliminary plan does not provide the adjoining properties, to the east and west, access to the proposed internal roadway system for this development. In order for the roadway system to be accepted as public, a stub must be constructed to the Rombach property to the west and cross access provided such that the Horobec property to the west and the Hornet Properties Inc. property to the east may connect to the internal roadway at a future date.

Response: Cross-access is provided as shown on the revised Preliminary Development Plan. We will provide future cross-access to the Horobec and Hornet Properties, Inc. sites as directed by the City of Chesterfield.

The following conditions related to site specific issues shall be applied to the petition:

Openspace

1. A minimum of 50% openspace will be required for this development until such time that permanent connection to sanitary sewer is provided. Upon connection, a minimum of 30% openspace will be required.

Response: We acknowledge the 30% Open Space requirement upon connection to sanitary sewer.

Landscaping

1. A thirty (30) foot landscape buffer is required along arterial streets.

Response: There are no arterial streets in this development. We acknowledge this requirement along Olive Street Road and note that the stormwater drainage ditch is shown within this buffer on the revised Preliminary Development Plan.

2. A thirty (30) foot landscape buffer is required when the development abuts a residential subdivision.

Response: The proposed Spirit Valley Business Park is outlined in yellow in the figure below. The only two residences in the area are labeled "1" (the Horobec property) and "2" (the Hornet Properties, Inc. site). We do not believe either of these constitutes a subdivision. However, we propose a thirty (30) foot landscape buffer for the first 175 feet South from Olive Street Road. We propose a fifteen (15) foot landscape buffer for the remainder of the site abutting the Horobec property. We also propose that the thirty (30) foot landscape buffer revert to a fifteen (15) foot requirement at such time that the Horobec property is developed.

Regarding the Hornet Properties, Inc. site, we propose a fifteen (15) foot landscape buffer between the site and the southern end of the Hornet Properties, Inc. site. The eastern access road for Spirit Valley Business Park has been moved immediately adjacent to the Hornet Properties. A landscape buffer along this property line is not necessary or feasible. It is our

understanding that this site will be redeveloped in the near future under zoning consistent with Spirit Valley's "PI" designation.



Building Height

1. Maximum building height shall be forty (40) feet exclusive of mechanical equipment.

Response: We acknowledge the forty (40) foot building height limitation, exclusive of mechanical equipment.

Setbacks

1. No building or structure, other than: a freestanding project identification sign, boundary and retaining walls, light standards, flag poles or fences will be located within the following setbacks:
 - a. Fifty (50) feet from the right-of-way of Olive Street Road.
 - b. Fifty (50) feet from 18668 Olive Street Road.
 - c. Fifty (50) feet from 18626 Olive Street Road.
 - d. Fifty (50) feet from the eastern and western property lines of this development.
 - e. Fifty (50) feet from the southern property line of this development.

Response: We acknowledge these setback requirements.

2. No parking stall or loading space will be located within the following setbacks:

a. Thirty five (35) feet from the right-of-way of Olive Street Road.

Response: We acknowledge this setback requirement.

b. Ten (10) feet from the internal property lines, with the exception of shared driveways.

Response: We acknowledge this setback requirement.

c. Twenty (20) feet from the principal internal street.

Response: We acknowledge this setback requirement.

d. Fifteen (15) feet from the eastern and western property lines of this development.

Response: We acknowledge this setback requirement.

e. Fifteen (15) feet from the southern property line of this development.

Response: We acknowledge this setback requirement.

f. Thirty (30) feet from the western property line abutting 18668 Olive Street Road for approximately 175 feet. The setback 175 feet back from the 18668 Olive Street Road shall be fifteen (15) feet.

Response: We acknowledge these setback requirements.

g. Fifteen (15) feet from the northern property line bearing S°78'49"W.

Response: We acknowledge this setback requirement.

3. No internal driveway, or roadway, except points of ingress and egress, will be located within the following setbacks:

a. Thirty (30) feet from the right-of-way of Olive Street Road.

b. Three hundred fifty (350) feet from the southern property line.

c. Two hundred seventy five (275) feet from the eastern and western property lines of this development.

Response: We acknowledge these setback requirements.

The following conditions related to site specific issues shall be applied to the rezoning petition:

Access

1. Access to Olive Street Road shall be limited to two street approaches, as directed by the Department of Public Works and St. Louis County Department of Highways and Traffic. The eastern most street approach shall align with the commercial entrance on the north side of Olive Street Road.

Response: We acknowledge the restriction of two curb cuts on Olive Street Road. As we discussed at our meeting on April 27th, it is our understanding that we do not need to align either of these with the commercial entrance on the north side of Olive Street Road.

2. Provide for cross access to the properties adjoining this development to the east and west, as directed by the City of Chesterfield, to allow for future connection to the internal roadway system within this development.

Response: We acknowledge this requirement. Cross-access is shown on the revised Preliminary Development Plan.

Road Improvements

3. Provide a street connection to the property to the west. The connection shall be a minimum of 500 feet from Olive Street Road, as directed by the City of Chesterfield.

Response: We acknowledge this requirement.

4. All roadway and related improvements shall be constructed prior to 60% occupancy of the development.

Response: We acknowledge this requirement.

5. Provide any additional right of way and construct any improvements to Olive Street Road as required by St. Louis County Department of Highways and Traffic.

Response: We acknowledge this requirement.

6. Provide a 40 foot right of way with a minimum of 10 foot roadway improvement, maintenance, utility and sewer easements on both sides for all interior roadways. Minimum roadway sections shall provide a 26 foot travel way with 7 foot shoulders on both sides and appurtenant storm drainage facilities as required by the Department of Public Works.

Response: We acknowledge this requirement.

Pedestrian Circulation

7. Provide a 5 foot wide sidewalk, conforming to ADA standards, along the Olive Street Road frontage of the site and along both sides of all interior roadways, as directed by the City of Chesterfield. The sidewalk shall be privately maintained; therefore, no public easements shall be required.

Response: We acknowledge the requirement of sidewalks along Olive Street Road, but we do not believe sidewalks should be required along both sides of the interior street. We propose sidewalks along the outside of the interior "U" shaped road. Sidewalks along the interior of this road would severely impact the design of the inner lots and would seldom be used by these tenants.

Parking

8. No construction related parking shall be permitted within the Olive Street Road right of way.

Response: We acknowledge this requirement.

9. No parking shall be permitted on any roadway in or adjacent to the development. The parking restriction and requirement for signage shall be indicated on the Site Development Plan and improvement plans. Signage shall be posted within 30 days of the placement of street pavement.

Response: We acknowledge this requirement.

10. Parking for this development shall be 4 spaces for every 1,000 square feet of development.

Response: We believe 4 spaces/1,000 square-feet of development is excessive parking for the proposed development. We would rather apply the Zoning Ordinance standards to each section plan as it is submitted to the City for review. Many of the uses would need much less parking than this standard.

Traffic Studies

11. Provide a traffic study as directed by the City of Chesterfield and/or St. Louis County Department of Highways and Traffic. The scope of the study shall include internal and external circulation and may be limited to site specific impacts, such as the need for additional lanes, entrance configuration, geometrics, sight distance, traffic signal modifications or other improvements required, as long as the density of the proposed development falls within the parameters of the City's traffic model. Should the density be other than the density assumed in the model, regional issues shall be addressed as directed by the City of Chesterfield.

Response: We acknowledge this requirement. Traffic issues can be addressed during site plan review.

Storm Water

12. Due to the inherent nature of development, the specific size, location, and configuration of the storm water infrastructure associated with the Chesterfield Valley Master Storm Water Plan are conceptual in nature. The exact location, size, and type of each segment of storm water infrastructure are to be reviewed and approved in conjunction with the development of specific sites. The developer will be required to construct ditches along the west, north, and east property lines of the property governed by this ordinance.

The developer may elect to propose alternate geometry, size and/or type of storm sewer improvements that are functionally equivalent to the required improvements. Functional equivalence is said to be achieved when, as determined by the City of Chesterfield, the alternate proposal provides the same hydraulic function, connectivity, and system-wide benefits without adversely affecting any of the following: water surface profiles at any location outside the development; future capital expenditures; maintenance obligations; equipment needs; frequency of maintenance; and probability of malfunction. The City will

consider, but is not obligated to accept, the developer's alternate plans. If the City determines that the developer's proposal may be functionally equivalent to the Chesterfield Valley Master Storm Water Plan improvements, hydraulic routing calculations will be performed to make a final determination of functional equivalence. The Director will consider the developer's proposal, but is not obligated to have the hydraulic analysis performed if any of the other criteria regarding functional equivalence will not be met. The hydraulic routing calculations regarding functional equivalence may be performed by a consultant retained by the City. The developer shall be responsible for all costs related to the work performed by the consultant.

Response: We acknowledge this requirement.

13. Provide any additional Chesterfield Valley Storm Water Easements along the north, east, and west property lines as required and directed by the Department of Public Works to accommodate the Chesterfield Valley Master Storm Water Plan channel in those areas, and depict the channel(s) on the Site Development Plan and improvement plans. The maintenance of the required storm water/ditch system shall be the responsibility of the property owner(s).

Response: We acknowledge this requirement.

14. All Chesterfield Valley Master Storm Water Plan improvements shall be operational prior to the paving of any driveways or parking areas, as directed by the City of Chesterfield.

Response: We acknowledge this requirement.

Sanitary Sewers

15. Provide public sewer service for the site, including sanitary force main, gravity lines and/or regional pump stations, as directed by the Metropolitan St. Louis Sewer District and the City of Chesterfield.

Response: We acknowledge this requirement.

The following standard general conditions shall be applied to the petition.

Geotechnical Report

- G-4. Prior to Site Development Plan approval, provide a geotechnical report, prepared by a registered professional engineer licensed to practice in the State of Missouri, as directed by the Department of Public Works. The report shall verify the suitability of grading and proposed improvements with soil and geologic conditions and address the existence of any potential sinkhole, ponds, dams, septic fields, etc., and recommendations for treatment. A statement of compliance, signed and sealed by the geotechnical engineer preparing the report, shall be included on all Site Development Plans and improvement plans.

Response: We acknowledge this requirement.

Verification Prior to Record Plat Approval

G-10. The developer shall cause, at his expense and prior to the recording of any plat, the reestablishment, restoration or appropriate witnessing of all Corners of the United States Public Land Survey located within, or which define or lie upon, the outboundaries of the subject tract in accordance with the Missouri Minimum Standards relating to the preservation and maintenance of the United States Public Land Survey Corners.

Response: We acknowledge this requirement.

Final Release of Subdivision Deposits

G-11. Prior to final release of subdivision construction deposits, the developer shall provide certification by a registered land surveyor that all monumentation depicted on the record plat has been installed and United States Public Land Survey Corners have not been disturbed during construction activities or that they have been reestablished and the appropriate documents filed with the Missouri Department of Natural Resources Land Survey Program.

Response: We acknowledge this requirement.

Finally, the petitioner should be advised of the following items which may impact the project during the later stages of the development process.

- It appears the site may contain a US Survey corner which may be disturbed by construction activities. U.S. Survey corners must be protected or restored. The corner in question is located between U.S. Surveys 153 and 133 along the west property line of the site.

Response: Acknowledged.

- Access management principals to be applied to this development can be found in Chapter 26 of the City code.

Response: Acknowledged.

- Access/utility easements will be required throughout the development.

Response: Acknowledged.

- Any structure built within 200 feet of the master plan drainage channels must be elevated to 1 foot above the 100 year water surface calculated by the model. Please reference Chapter 14 of the City code for flood damage prevention requirements.

Response: Acknowledged.

- The typical section provided for the internal roadway system omitted sidewalks from both sides of the roadway and contained some unidentified dimensions. The sidewalks should be located outside the roadway improvement, maintenance, utility, and sewer easement adjacent to the right of way.

Response: Acknowledged.

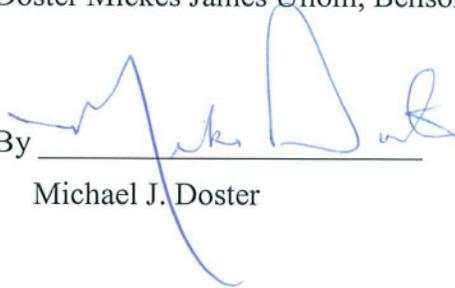
- The current preliminary site plan depicts the roadway improvement, maintenance, utility, and sewer easement extending onto the Hornet Properties Inc. property. This would imply that the required sidewalk would be located on the adjoining property for which easements would need to be obtained.

Response: Acknowledged.

Please call if you have any questions. Thank you.

Respectfully submitted,
Doster Mickes James Ullom, Benson & Guest L.L.C.

By



Michael J. Doster

cc: Dan Hayes, NAI Desco
George Stock, Stock & Associates Consulting Engineers, Inc.

SPIRIT VALLEY BUSINESS PARK

A TRACT OF LAND BEING PART OF LOT 5 OF THE AMELIA BOISSELER ESTATE AS RECORDED IN PLAT BOOK 16 PAGE 27 AND PART OF COSMOS TAMIAN KROENUNG AND HERMAN SCHAEFER AND AGNES SCHAEFER PARCELS OF THE PARTITION IN ESTATE OF VALENTINE KROENUNG LOCATED IN U.S. SURVEYS 133 AND 153, TOWNSHIP 45 NORTH, RANGE 3 EAST OF THE 5TH PRINCIPAL MERIDIAN CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI

EXISTING ZONING: NU 'NON URBAN'
PROPOSED ZONING: PI 'PLANNED INDUSTRIAL'

Property Description for Zoning

A TRACT OF LAND IN ST. LOUIS COUNTY, MISSOURI, BEING PART OF LOT 5 OF THE SUBDIVISION OF AMELIA BOISSELER ESTATE, IN U.S. SURVEYS 368, 1937 AND 133, TOWNSHIP 45 NORTH, RANGE 3 EAST, ACCORDING TO PLAT THEREOF RECORDED IN PLAT BOOK 16 PAGE 27 OF THE ST. LOUIS COUNTY RECORDS, AND A TRACT OF LAND IN U.S. SURVEY 153, TOWNSHIP 45 NORTH, RANGE 3 EAST, BEING COMPOSED OF TWO PARCELS, ONE MARKED "COSMOS TAMIAN KROENUNG" AND THE OTHER MARKED "HERMAN SCHAEFER AND AGNES SCHAEFER" ACCORDING TO THE PLAT ACCOMPANYING COMMISSIONER'S REPORT IN PARTITION IN ESTATE OF VALENTINE KROENUNG, DECEASED, BEING CAUSE NO. 16263 OF THE CIRCUIT COURT OF THE CITY (FORMER COUNTY) OF ST. LOUIS, MISSOURI, IN THE YEAR 1870, A COPY OF WHICH REPORT IS RECORDED IN BOOK 439 PAGE 448 OF THE RECORDS OF SAID CITY, AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT ON THE SOUTHERN LINE OF OLIVE STREET ROAD AT THE POINT OF INTERSECTION WITH THE WESTERN LINE OF LOT 5 OF THE SUBDIVISION OF THE AMELIA BOISSELER ESTATE AS RECORDED IN PLAT BOOK 16, PAGE 27 OF THE ST. LOUIS COUNTY RECORDS; THENCE ALONG THE SOUTHERN LINE OF SAID OLIVE STREET ROAD SOUTH 82 DEGREES 31 MINUTES 57 SECONDS EAST, A DISTANCE OF 863.63 FEET TO THE POINT OF INTERSECTION WITH THE WESTERN LINE OF A TRACT OF LAND DESCRIBED AS PARCEL 1 IN A DEED TO HORNET PROPERTIES INC., AS RECORDED IN DEED BOOK 10602, PAGE 2235 OF THE ST. LOUIS COUNTY RECORDS; THENCE ALONG SAID WESTERN LINE SOUTH 11 DEGREES 25 MINUTES 51 SECONDS EAST, A DISTANCE OF 444.89 TO THE SOUTHERN LINE OF THE AFORESAID LOT 5; THENCE ALONG SAID SOUTHERN LINE AND THE PROLONGATION THEREOF NORTH 78 DEGREES 19 MINUTES 49 SECONDS EAST, A DISTANCE OF 268.25 FEET TO THE NORTHEAST CORNER OF A TRACT OF LAND DESCRIBED AS PARCEL 2 IN A DEED TO PAUL N. ALBRECHT, TRUSTEE, ETAL AS RECORDED IN DEED BOOK 10392, PAGE 1830 OF THE AFORESAID ST. LOUIS COUNTY RECORDS; THENCE ALONG THE EASTERN LINE OF SAID PARCEL 2 OF SAID ALBRECHT TRACT SOUTH 11 DEGREES 44 SECONDS EAST, A DISTANCE OF 1536.29 FEET TO A FOUND IRON ROD AT THE SOUTHEAST CORNER OF SAID TRACT; THENCE ALONG THE SOUTHERN LINE OF SAID PARCEL 2 OF SAID ALBRECHT TRACT SOUTH 78 DEGREES 12 MINUTES 13 SECONDS WEST, A DISTANCE OF 1171.65 FEET TO THE SOUTHWEST CORNER OF SAID TRACT; THENCE ALONG THE WESTERN LINE OF SAID PARCEL 2 OF SAID ALBRECHT TRACT NORTH 12 DEGREES 24 MINUTES 49 SECONDS WEST, A DISTANCE OF 1171.4 FEET TO A STONE AT THE SOUTHEAST CORNER OF LOT 3 OF THE AFORESAID SUBDIVISION OF THE AMELIA BOISSELER ESTATE; THENCE CONTINUING ALONG THE WESTERN LINE OF THE AFORESAID PARCEL 2 OF SAID ALBRECHT TRACT, BEING THE EASTERN LINE OF SAID LOT 3, NORTH 12 DEGREES 22 MINUTES 59 SECONDS WEST, A DISTANCE OF 1421.85 FEET TO A STONE AT THE NORTHWEST CORNER OF THE AFORESAID PARCEL 2 OF SAID ALBRECHT TRACT; THENCE ALONG THE NORTHERN LINE OF SAID TRACT NORTH 78 DEGREES 19 MINUTES 49 SECONDS EAST, A DISTANCE OF 112.42 FEET TO THE SOUTHWEST CORNER OF THE AFORESAID LOT 5 OF THE SUBDIVISION OF THE AMELIA BOISSELER ESTATE; THENCE ALONG THE WESTERN LINE OF SAID LOT 5 NORTH 11 DEGREES 25 MINUTES 51 SECONDS WEST, A DISTANCE OF 728.04 FEET TO THE POINT OF BEGINNING AND CONTAINING 2,300,839 SQ. FT. 52.619 ACRES.

_____, the owner of the property shown on this plan for and in consideration of being granted a permit to develop property under the provisions of Chapter _____

(applicable subsection) _____ (Ordinance No. _____) of the City of Chesterfield do hereby agree that said property from the date of recording this plan shall be developed only as shown thereon, unless said plan is amended by the Planning Commission, or voided or vacated by order of the City of Chesterfield Council.

(Signature) _____

STATE OF MISSOURI }
COUNTY OF ST. LOUIS } SS.

On this _____ day of _____, 2007, before me personally appeared _____, who being by me duly sworn, did say he is the _____ of _____ and that said instrument was signed on behalf of said limited liability company, and that said _____ acknowledged said instrument to be the free act and deed of said limited liability company.

IN WITNESS WHEREOF, I have signed and sealed the foregoing the day and year first above written.

Notary Public

Print Name

My commission expires: _____

SURVEYOR'S CERTIFICATION

THIS IS TO CERTIFY THAT STOCK AND ASSOCIATES CONSULTING ENGINEERS, INC. HAS PREPARED THIS PRELIMINARY DEVELOPMENT PLAN FROM A FIELD SURVEY AND DOES NOT REPRESENT A PROPERTY BOUNDARY SURVEY. THIS PRELIMINARY DEVELOPMENT PLAN IS A CORRECT REPRESENTATION OF ALL EXISTING AND PROPOSED LAND DIVISIONS.

STOCK AND ASSOCIATES CONSULTING ENGINEERS, INC.
L.S. No. 222-D

By: DANIEL EHLMANN, MISSOURI L.S. NO. 2215

GENERAL NOTES:

- BOUNDARY AND TOPOGRAPHICAL SURVEY BY STOCK AND ASSOCIATES CONSULTING ENGINEERS, INC. (BASIS OF BEARINGS: MISSOURI STATE PLANE, GRID NORTH)
- SUBJECT PROPERTY LIES WITHIN FLOOD ZONE "X" (AREAS OF 500-YEAR FLOOD; AREAS OF 100-YEAR FLOOD WITH AVERAGE DEPTHS OF LESS THAN 1 FOOT OR WITH DRAINAGE AREAS LESS THAN 1 SQUARE MILE, AND AREAS PROTECTED BY LEVEES FROM 100-YEAR FLOOD) ACCORDING TO THE NATIONAL FLOOD INSURANCE PROGRAM, FLOOD INSURANCE RATE MAP FOR ST. LOUIS COUNTY, MISSOURI AND INCORPORATED AREAS PER MAP NO. 29189C0120 H WITH AN EFFECTIVE DATE OF AUGUST 2, 1995 AND REVISED TO REFLECT LOMR DATED APRIL 17, 2000. (APPROXIMATE 100 YR. EL. 460)
- ALL UTILITIES SHOWN HAVE BEEN LOCATED BY THE ENGINEER FROM AVAILABLE RECORDS. THEIR LOCATION SHOULD BE CONSIDERED APPROXIMATE. THE CONTRACTOR HAS THE RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES, PRIOR TO CONSTRUCTION, TO HAVE EXISTING UTILITIES FIELD LOCATED. SHOULD ANY CONFLICTS BE EVIDENT, THE CONTRACTOR SHALL NOTIFY THE OFFICE OF THE ENGINEER IMMEDIATELY.
- ARCHITECTURAL ELEVATIONS, SITE LANDSCAPING PLANS, SITE LIGHTING PLANS AND SITE SIGNAGE PLANS SHALL BE SUBMITTED TO THE CITY OF CHESTERFIELD AS INDIVIDUAL LOTS ARE DEVELOPED ON THEIR SITE DEVELOPMENT SECTION PLAN.
- FOR CLARITY, STREET LIGHTS WERE NOT SHOWN ON THIS PLAN. STREET LIGHTS SHALL BE PROVIDED AS REQUIRED PER THE CITY OF CHESTERFIELD SUBDIVISION ORDINANCE SECTION 1005.320.
- ON-SITE STORM WATER DRAINAGE REQUIREMENTS SHALL BE IN ACCORDANCE WITH THE CHESTERFIELD VALLEY MASTER STORM WATER DRAINAGE PLAN AND AS DIRECTED BY THE CITY OF CHESTERFIELD.
- OFF-SITE GRADING EASEMENTS, IF REQUIRED, SHALL BE EXECUTED AND RECORDED PRIOR TO THE COMMENCEMENT OF ANY OFF-SITE GRADING.
- CONTACT GEORGE HUTH @ ENBRIDGE PIPELINE (1-636-584-4582) 48 HOURS PRIOR TO ANY WORK COMMENCING.
- ALL PROPOSED UTILITIES SHALL BE CONSTRUCTED TO SAINT LOUIS COUNTY AND THE CITY OF CHESTERFIELD STANDARDS.
- ALL GRADING AND DRAINAGE TO BE IN CONFORMANCE WITH SAINT LOUIS COUNTY AND MSD STANDARDS.
- STORM WATER SHALL BE DISCHARGED AT AN ADEQUATE NATURAL DISCHARGE POINT. SINKHOLES ARE NOT ADEQUATE DISCHARGE POINTS.
- NO SLOPES WITHIN SAINT LOUIS COUNTY RIGHT-OF-WAY SHALL EXCEED 3 (HORIZONTAL) TO 1 (VERTICAL)
- PRIOR TO SPECIAL USE PERMIT ISSUANCE BY THE SAINT LOUIS COUNTY DEPARTMENT OF HIGHWAYS AND TRAFFIC, A SPECIAL CASH ESCROW OR A SPECIAL ESCROW SUPPORTED BY AN IRREVOCABLE LETTER OF CREDIT, MUST BE ESTABLISHED WITH THE SAINT LOUIS COUNTY DEPARTMENT OF HIGHWAYS AND TRAFFIC TO GUARANTEE COMPLETION OF THE REQUIRED ROADWAY IMPROVEMENTS.
- IMPROVE OLIVE STREET ROAD TO ONE-HALF OF A 60' RIGHT-OF-WAY WITH A 38' PAVEMENT AND A 7' PAVED SHOULDER, OR A MINIMUM 33' PAVEMENT AND A 7' PAVED SHOULDER.
- ACCESS TO THIS DEVELOPMENT FROM OLIVE STREET ROAD SHALL BE VIA TWO ENTRANCES LOCATED A MINIMUM 400' APART (CENTERLINE TO CENTERLINE) TO PROVIDE REQUIRED SIGHT DISTANCE AND CONSTRUCTED TO SAINT LOUIS COUNTY STANDARDS AS DIRECTED BY THE SAINT LOUIS COUNTY DEPARTMENT OF HIGHWAYS AND TRAFFIC.
- ALL EXISTING CURB CUTS ON SOUTH SIDE OF OLIVE STREET ROAD SHALL BE REMOVED AND RIGHT OF WAY RESTORED.

LEGEND	
EXISTING SANITARY SEWER	
EXISTING STORM SEWER	
PROPOSED SANITARY SEWER	
PROPOSED STORM SEWER	
EXISTING TREE	
EXISTING BUILDING	
EXISTING CONTOUR	
SPOT ELEVATION	
EXISTING UTILITIES	
FOUND 1/2" IRON PIPE	
SET IRON PIPE	
FOUND CROSS	
FOUND STONE	
FIRE HYDRANT	
LIGHT STANDARD	
BUSH	
SIGN	
NOTES PARKING SPACES	
GUY WIRE	
POWER POLE	
WATER VALVE	
DENOTES RECORD INFORMATION	
HANDICAPPED PARKING	

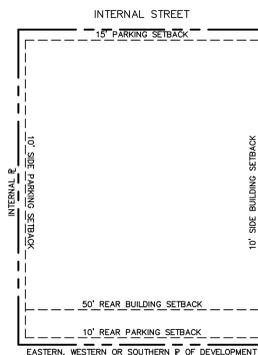
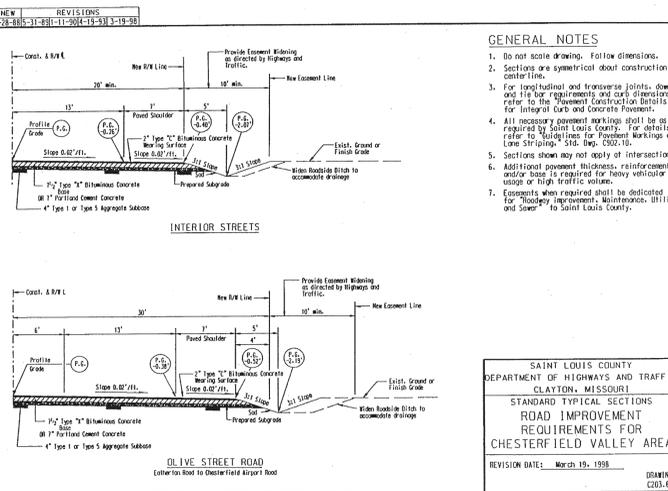
ABBREVIATIONS

C.O.	- CLEANOUT
DBI	- DEED BOOK
E	- ELECTRIC
FL	- FLOWLINE
FT	- FEET
FND.	- FOUND
G	- GAS
M.H.	- MANHOLE
N/P	- NOW OR FORMERLY
PB.	- PLAT BOOK
PG.	- PAGE
P.V.C.	- POLYVINYL CHLORIDE PIPE
R.C.P.	- REINFORCED CONCRETE PIPE
SQ.	- SQUARE
T	- TELEPHONE CABLE
V.C.P.	- VETTERED CLAY PIPE
W	- WATER
(86'W)	- RIGHT-OF-WAY WIDTH



SITE INFORMATION

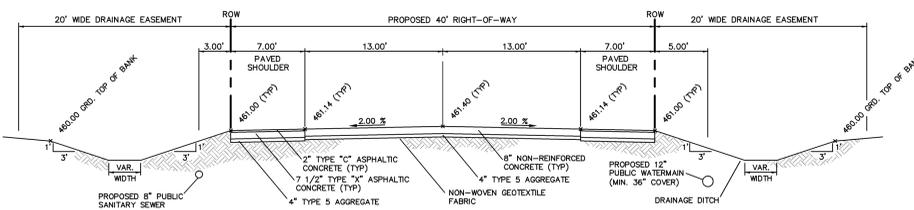
SITE ADDRESS	= 18630, 18650 & 18660 OLIVE STREET ROAD CHESTERFIELD, MO 63005
SITE AREA	= 52.819
OWNER	= SPIRIT VALLEY DEVELOPMENT L.L.C.
LOCATOR NO.	= 17W42-0035, 17W23-0010 & 17W23-0021
CITY	= CITY OF CHESTERFIELD
FLOOD MAP	= 29189C0120H
SEWER DISTRICT	= MSD
WATERSHED	= MISSOURI RIVER
FIRE DISTRICT	= MONARCH CHESTERFIELD
SCHOOL DISTRICT	= ROCKWOOD R-6
ELECTRIC SERVICE	= AMEREN U.E.
GAS SERVICE	= LALEDE GAS
PHONE SERVICE	= SWBT
WATER SERVICE	= MO. AMERICAN WATER CO.
CABLE SERVICE	= CHARTER COMMUNICATIONS



TYPICAL LOT

PROPOSED SETBACKS:

- BUILDING SETBACKS:**
- FIFTY (50) FEET FROM THE RIGHT-OF-WAY OF OLIVE STREET ROAD.
 - FIFTY (50) FEET FROM 18668 OLIVE STREET ROAD.
 - FIFTY (50) FEET FROM 18626 OLIVE STREET ROAD.
 - FIFTY (50) FEET FROM THE EASTERN AND WESTERN PROPERTY LINES OF THIS DEVELOPMENT.
 - FIFTY (50) FEET FROM THE SOUTHERN PROPERTY LINE OF THIS DEVELOPMENT.
- PARKING AND LOADING SETBACKS:**
- THIRTY (30) FEET FROM THE RIGHT-OF-WAY OF OLIVE STREET ROAD.
 - TEN (10) FEET FROM THE INTERNAL PROPERTY LINES, WITH THE EXCEPTION OF SHARED DRIVEWAYS.
 - FIFTEEN (15) FEET FROM THE PRINCIPAL INTERNAL STREET.
 - TEN (10) FEET FROM THE EASTERN AND WESTERN PROPERTY LINES OF THIS DEVELOPMENT.
 - TEN (10) FEET FROM THE SOUTHERN PROPERTY LINE OF THIS DEVELOPMENT.
 - TWENTY (20) FEET FROM 18668 OLIVE STREET ROAD.
 - TEN (10) FEET FROM 18626 OLIVE STREET ROAD.
- INTERNAL ROADWAYS AND DRIVES, EXCEPT POINTS OF INGRESS AND EGRESS:**
- THIRTY (30) FEET FROM THE RIGHT-OF-WAY OF OLIVE STREET ROAD.
 - THREE HUNDRED NINETY (390) FEET FROM THE SOUTHERN PROPERTY LINE OF THIS DEVELOPMENT.
 - THREE HUNDRED (300) FEET FROM THE SOUTHERN PROPERTY LINE OF THIS DEVELOPMENT.



TYPICAL INTERNAL ROAD SECTION

SCALE= NTS

PREPARED FOR:
SPIRIT VALLEY DEVELOPMENT, LLC
8235 FORSYTH BLVD. STE. 210
ST. LOUIS, MO 63106
PHONE: (314) 994-4068



MoDOT LOCATE (816) 340-4100

DNR BENCHMARK

SL-40 BRASS DISC STAMPED "SL-40, 1990" ON THE NORTH SIDE OF NORTH OLIVER 40 RD. ADDRESS FROM THE INTERSECTION OF SPIRIT OF ST. LOUIS BOULEVARD. ELEV.=486.55

UTILITY NOTE

UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEY RECORDS AND INFORMATION, AND THEREFORE DO NOT NECESSARILY REFLECT THE ACTUAL EXISTENCE, NON-EXISTENCE, SIZE, TYPE, NUMBER OR LOCATION OF THESE FACILITIES, STRUCTURES AND UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE ACTUAL LOCATION OF ALL UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES EITHER SHOWN OR NOT SHOWN ON THESE PLANS. THE UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES SHALL BE LOCATED IN THE FIELD PRIOR TO ANY GRADING, EXCAVATION OR CONSTRUCTION OF IMPROVEMENTS. THESE PROVISIONS SHALL IN NO WAY ABSOLVE ANY PARTY FROM COMPLYING WITH THE UNDERGROUND FACILITY SAFETY AND DAMAGE PREVENTION ACT, CHAPTER 219 RSMo.

STOCK AND ASSOCIATES CONSULTING ENGINEERS, INC. AND THE UNDERSIGNED ENGINEER HAVE NO RESPONSIBILITY FOR SERVICES PROVIDED BY OTHERS TO IMPLEMENT THE IMPROVEMENTS SHOWN ON THIS PLAN AND ALL OTHER DRAWINGS WHERE THE UNDERSIGNED ENGINEER'S SEAL APPLIES. THE CONTRACTOR MEANS AND METHODS ARE THE SOLE RESPONSIBILITY OF THE OWNER AND CONTRACTOR. STOCK AND ASSOCIATES CONSULTING ENGINEERS, INC. HAS NO RESPONSIBILITY TO VERIFY FINAL IMPROVEMENTS AS SHOWN ON THIS PLAN UNLESS SPECIFICALLY DESIGNATED AND AUTHORIZED TO DO SO BY THE OWNER OR CONTRACTOR.

MSD P # _____
BASE MAP # 17W

PRELIMINARY SITE DEVELOPMENT PLAN

SPIRIT VALLEY BUSINESS PARK

STOCK & ASSOCIATES
Consulting Engineers, Inc.

257 Chesterfield Business Parkway
St. Louis, MO 63005
PH. (636) 530-9100
FAX (636) 530-9130
e-mail: general@stockassoc.com
Web: www.stockassoc.com

DATE CHECKED BY: 11/20/06
DATE JOB NUMBER: 206-3923
DATE: 05/08/07
E-25116
DRAWN BY: A.C.D.
P.D.B.
11/20/06
1 of 4



STOCK PROJ.
205-3652

ORDINANCE No. 2236
STOCK PROJ.
205-3328
"NO APPROVED AND/OR RECORDED
SITE DEVELOPMENT CONCEPT PLAN"

WEST END BASIN 3
STOCK PROJ.
204-3423.1

STOCK PROJ.
206-4080

STOCK PROJ.
207-4062

ST. LOUIS COUNTY, MISSOURI
DB. 7219, PG. 1780
STOCK PROJ.
200-2211

ST. LOUIS COUNTY, MISSOURI
DB. 7871, PG. 876
STOCK PROJ.
200-2211

STOCK PROJ.
204-3423.2

- 4.) REVISED PER COUNTY COMMENTS 05/01/07
- 3.) REVISED PER COUNTY AND CLIENT COMMENTS 04/26/07
- 2.) REVISED PER CITY AND CLIENT COMMENTS 02/05/07
- 1.) REVISED PER CITY COMMENTS 01/25/07

PRELIMINARY SITE DEVELOPMENT PLAN
SPIRIT VALLEY BUSINESS PARK

STOCK & ASSOCIATES
Consulting Engineers, Inc.

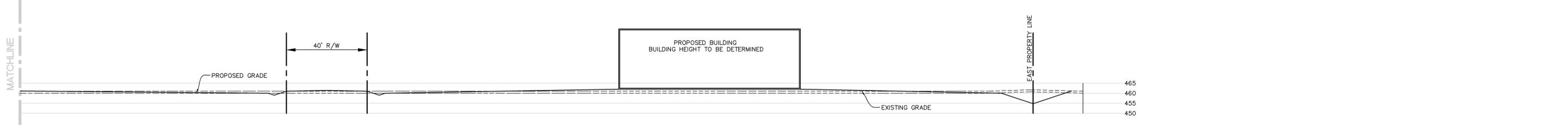
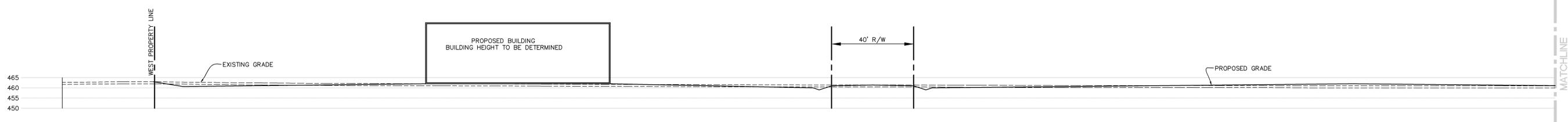
257 Chesterfield Business Parkway
St. Louis, MO 63005
PH. (636) 530-9100
FAX (636) 530-9130
e-mail: general@stockassoc.com
Web: www.stockassoc.com

MSP D #
BASE MAP # 17W



SECTION B-B

SCALE: HORIZONTAL 1"=20'
VERTICAL 1"=20'



SECTION A-A

SCALE: HORIZONTAL 1"=20'
VERTICAL 1"=20'

- 4.) REVISED PER COUNTY COMMENTS 05/01/07
- 3.) REVISED PER COUNTY AND CLIENT COMMENTS 04/26/07
- 2.) REVISED PER CITY AND CLIENT COMMENTS 02/05/07
- 1.) REVISED PER CITY COMMENTS 01/25/07

PRELIMINARY SITE DEVELOPMENT PLAN
SPIRIT VALLEY BUSINESS PARK

STOCK & ASSOCIATES
Consulting Engineers, Inc.

257 Chesterfield Business Parkway
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MSD P # _____
BASE MAP # 17W

05/08/07
GEORGE M. STOCK E-25116

DATE CHECKED BY: A.C.D. 11/20/06
DATE JOB NUMBER: D.P.B. 11/20/06
SHEET: 206-3923 3 of 4

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V.I.L.D.

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

Planning Commission Staff Report

Meeting Date: May 14, 2007

From: Annissa G. McCaskill-Clay, Assistant Director of Planning

Subject: Amendment to St. Louis County Conditional Use Permit #561 for the addition of "Hospital" as a use.

Location: 14701 Olive Boulevard (17R530197)

Petition: P.Z. 10-2007 Surrey Place (St. Luke's Episcopal Presbyterian Hospital) GOC

Proposal Summary

St. Luke's Episcopal Presbyterian Hospitals, the owner of Surrey Place, has submitted an application the above-referenced amendment to St. Louis County Conditional Use Permit #561 per the regulations of Section 1003.181 Conditional Use Permit Procedure (CUP) of the City of Chesterfield Zoning Ordinance. The subject site is located on the north side of Olive Boulevard, approximately .25 miles east of intersection of Olive and Ladue Roads and is zoned "R1A" Residence District.

The petitioner's request is for the addition of "Hospital" as a use for this site. Conditional Use Permit #561 currently authorizes a nursing home and associated self-care units and recreation facilities for residents who are at least 55 years of age.

Staff Recommendation

Staff recommends approval of the requested amendment for the addition of "Hospital" as a use for the subject site. A new Conditional Use Permit will be generated for this petition to replace the previous one written by St. Louis County.

Zoning Analysis

Section 1003.181 states: "It is hereby declared that certain land uses and developments present unique problems with respect to their proper location and relationship to other land uses. Therefore, analysis and judgment of the consequences of each development and use is necessary to preserve and to promote the public health, safety, and general welfare. Such land uses and developments are identified in each particular zoning district under conditional land use and development permits issued by the Commission."

The procedure for review/approval of a conditional use permit is set forth in Section 1003.181 of the Zoning Ordinance as follows:

- A public hearing is held at which time, unless further discussion is deemed necessary by the Commission, a vote is taken on the requested conditional use.
- If the Planning Commission votes to approve a conditional use, the Commission may impose conditions related to the requested conditional use as necessary.
- The Planning Commission, via Staff, files a report with the City Council granting or denying the application and stating the reasons therefore.
- Unless the City Council exercises its power of review or a protest is duly filed with the City Clerk, the Conditional Use Permit (C.U.P.) will become effective after 15 days of the City Council's receipt of the Planning Commission's report granting the application.
- If the Commission votes to deny a conditional use, the City Council may still exercise its power of review or the applicant may file an appeal within 15 days of the Planning Commission decision.

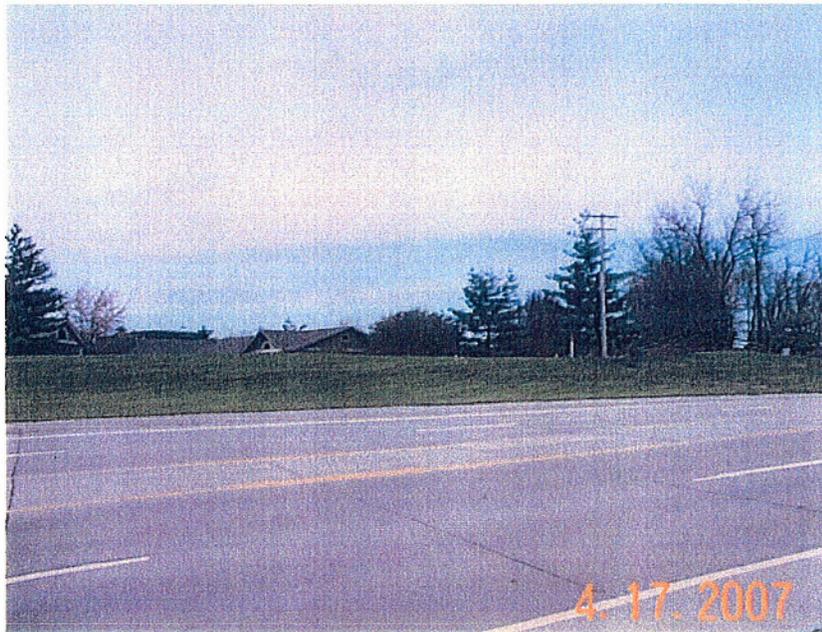
A preliminary plan accompanies all conditional use permit requests. When a vote is taken on the conditional use request, the vote is to approve the use(s), not to approve the accompanying preliminary plan which is provided for informational purposes only.

A Site Development Plan, if applicable, is required subsequent to the approval of a conditional use permit for review and approval by the City of Chesterfield.

Surrounding Land Use and Zoning

The land use and zoning for the properties surrounding this parcel are as follows:

- North and West: The property to the north is the Villages of Kendall Bluffs and is zoned "R1A" Planned Environment Unit (PEU).
- South: To the south of the subject site is Olive Boulevard. Beyond Olive is the "R2" with a Planned Environmental Unit-zoned Brunhaven Subdivision.
- East: To the east of Surrey Place is the "R1" Residence District-zoned Old Riverwoods Subdivision.



Looking north across Olive Boulevard at the existing development

Comprehensive Plan Analysis

The subject site is located within Ward 1 of the City of Chesterfield. The Comprehensive Plan calls for this area to be single family residential. This subject site

is not located in any sub-area identified by the Comprehensive Plan; therefore there are no additional development guidelines for this site.

Site Area History

In 1987, St. Louis County zoned the subject site "R1A" Residence District and also issued Conditional Use Permit #561 authorizing a "nursing home and associated self-care units and recreation facilities for the elderly." A portion of the original "R1A" Residence District is currently being developed with a Planned Environment Unit (PEU) as the Villages at Kendall Bluffs.

Issues

A public hearing regarding this request was held on April 23, 2007. At that time, the following issues were taken to be addressed:

1. The number of levels permitted in the development.

Staff Response: Per the discussion at the Commission's April 23, 2007 session, the Draft C.U.P. has been amended to limit building levels to two (2), with the exclusion of basement areas.

2. The setback with respect to the fire lane.

Staff Response: The setback for the fire lane is exempted from the parking and internal roadway setback as written in the Draft C.U.P. The actual distance of the lane, as located per Fire Protection District requirement, is feet from the property line.

3. Clarification of the buffer requirements.

Staff Response: The project will be held to the requirements of the City of Chesterfield Tree Manual (Ordinance 2335). As this property is within a residential subdivision and will abut another residential subdivision, it will be required to maintain a twenty (20) foot landscape buffer.

4. Provide information regarding traffic study for the Kendall Bluffs development.

Staff Response: Please see the attached email from Acting Director of Planning Michael O. Geisel.

5. Possibility of providing a disclosure to Kendall Bluffs property owners.

Staff Response: The requirement for disclosure to Kendall Bluffs property owners would be one that would have had to been established at the creation of the governing conditions for that development. Other than requesting the developer of Kendall Bluffs to voluntarily provide this disclosure, there are no means by which to provide it.

6. Adding language to the Attachment A, similar to language used in the St. John's Rehabilitation facility, with respect to "hospital" use – not allowing emergency use, operating rooms, surgical services, or heliport or helipad.

Staff Response: This language has been added to the Draft Conditional Use Permit. Please note there is no Attachment A.

7. Possibility of providing a traffic accident report from the Police Department.

Staff Response: Per the Commission's request, this information was requested. An email, dated April 24, 2007 from Acting Director Michael Geisel to the Commission, is attached advising of the information provided by the City of Chesterfield Police Department. There were no accidents in the area of the proposed development in 2003, 2004, 2005, 2006.

8. Provide the differential between the residential properties in the Kendall Bluffs development and the proposed development.

Staff Response: The Petitioner has advised that from the back of the building, closest to the property line, it is approximately a 70 foot drop down to the bottom of the hill.

9. Provide the requirements of the Certificate of Need provided by the State, as well as whether the use would include outpatient services.

Staff Response: The Petitioner has provided a copy of the Certificate of Need for the Commission's Review.

Request

Staff recommends repeal of St. Louis County Conditional Use Permit #561 and approval of City of Chesterfield Conditional Use Permit #25 for the "R1A" Residence District-zoned Surrey Place.

Respectfully submitted,



Annissa G. McCaskill-Clay, AICP
Assistant Director of Planning

Attachments

1. Draft City of Chesterfield Conditional Use Permit #25
2. Preliminary Plan

CITY OF CHESTERFIELD
CONDITIONAL USE PERMIT # 25

Petition: P.Z. 10-2007 Surrey Place (St. Luke's Episcopal Presbyterian Hospitals)

Public Hearing Date: April 23, 2007

Site Location: The north side of Olive Boulevard, approximately .25 miles east of intersection of Olive and Ladue Roads

Acreage for CUP: 10.751

Underlying Zoning District: "R1A" Residence District

The following Conditional Uses shall be allowed in this development:

1. Hospital. As proposed, this use is to be a specialized, limited use facility that will provide general physical rehabilitative and medical services, including nursing, clinical and therapeutic services.
 - i. The hospital use associated with this C.U.P. development shall have no more than 35 beds.
 - ii. There shall be no emergency room, operating room or surgical services permitted on the subject site. There shall be no heliport or helipad services permitted on the subject site.
2. Nursing home.
 - i. Nursing facilities shall be contained in not more than two (2) buildings.
 - ii. The full care nursing facility shall have no more than 140 beds.

All provisions of the City of Chesterfield City Code shall apply to this development except as specifically modified below:

1. All building shall not exceed one (1) building levels in height, exclusive of basement and cellar areas, underground parking, architectural features and rooftop mechanical equipment.

2. The buildings authorized via this C.U.P shall be of architectural style and building materials as approved by the Planning Commission on building elevations submitted in conjunction with the Site Development Plan.
3. Required parking for proposed uses shall dictate the actual developable square footage.
4. No direct access to Olive Boulevard shall be allowed. Access to Olive Boulevard shall be via Kendall Bluff Drive.
5. Provide a cross access easement guaranteeing permanent access from this site to the abutting properties to the west (Villages at Kendall Bluff) and northeast as directed by the City of Chesterfield.
6. Structure Setbacks:
No building or structure, other than boundary walls, retaining walls, signs, light standards, flag poles or fences, shall be located within the following setbacks:
 - a. One hundred (100) feet of the right of way of Olive Boulevard
 - b. Ten (10) feet from northeastern limits of this C.U.P.
 - c. Fifty (50) feet from the northern property boundary (N42°06'40"E)
 - d. One hundred ninety (190) feet from the western property boundary (N44°58'18"W)
 - e. Ninety (90) feet from the eastern property boundary (S26°46'00"E)
 - f. Distance between buildings shall be as approved by the Department of Planning and the Planning Commission on appropriate development plan.
7. No outdoor parking stall or internal roadway, except points of ingress, egress and the required fire lanes, shall be located within the following setbacks:
 - a. Twenty-five (25) feet of the right-of-way of Olive Street Road.
 - b. Twenty-five (25) feet of the northeast boundary of this C.U.P.
 - c. Sixty (60) feet from the northern property boundary (N42°06'40"E)
 - d. One hundred twenty (120) feet from the western property boundary (N44°58'18"W)

8. Provide a 5 foot wide sidewalk, conforming to ADA standards, along the Olive Boulevard frontage of the site. The sidewalk may be located within State right-of-way, if permitted by the Missouri Department of Transportation, or within a 6 foot wide sidewalk, maintenance and utility easement.

Property Description

A tract of land being part of Lot 2 of the Subdivision of Moss Hunton's Land according to the plat thereof as recorded in Surveyors Record Book 2, page 21 of the St. Louis County Records, being located in U.S. Surveys 120 and 206, Township 45 North, Range 4 East of the Fifth Principal Meridian, City of Chesterfield, St. Louis County, Missouri.

**NOTICE OF PUBLIC HEARING
CITY OF CHESTERFIELD
PLANNING COMMISSION**

NOTICE IS HEREBY GIVEN that the Planning Commission of the City of Chesterfield will hold Public Hearings on April 23, 2007, at 7:00 p.m. in the Council Chambers at the City Hall, 690 Chesterfield Parkway West, Chesterfield, Missouri 63017.

Said hearings will be as follows:

P.Z. 10-2007 Surrey Place (St. Luke's Episcopal Presbyterian Hospital): a request to amend Conditional Use Permit Number 561 to add hospital as a permitted use for an approximately 10.751 acre tract of land located at 14701 Olive Boulevard, east of the intersection of Ladue Road and Olive Boulevard. (17R530197)

Anyone interested in the proceedings will be given an opportunity to be heard. Copies of the request are available for review in the Department of Planning at the Chesterfield City Hall, 690 Chesterfield Parkway West during weekdays between the hours of 8:30 a.m. and 4:30 p.m.

CITY OF CHESTERFIELD
Maurice L. Hirsch, Chair
Chesterfield Planning Commission



Notification of Certificate of Need



TO WHOM ALL THESE MAY PRESENT:

WHEREAS, in accordance with §197.300-§197.366, no person shall develop or offer a new institutional health service without first obtaining a Certificate of Need (CON) from the Missouri Health Facilities Review Committee;

WHEREAS, no state agency charged by statute to license or certify health care facilities shall issue a license to, or certify any such facility, or distinct part of such facility, that is developed without obtaining a CON;

WHEREAS, no agency of state government may appropriate or grant funds to or make payment of any funds to any person or health care facility which has not first obtained every CON required under sections §197.300-§197.366;

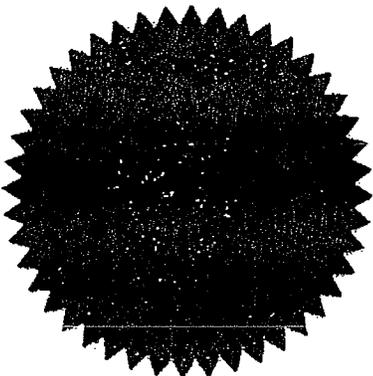
WHEREAS, on November 9, 2006, a CON application was submitted to establish a 35-bed rehabilitation hospital;

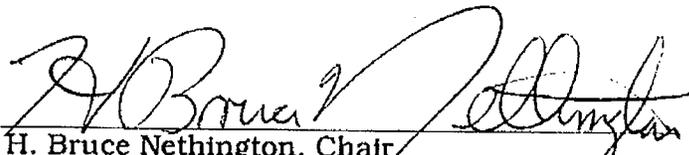
WHEREAS, the CON Program staff analyzed this application in accordance with §197.300-§197.366, and the Committee found that there was a need;

WHEREFORE, on January 22, 2007, to document and commemorate its affirmative finding of need, the Committee issues this CON as set forth in:

Project application #4007 HS per terms incorporated therein
to St. Luke's Episcopal-Presbyterian Hospitals and
St. Luke's Rehab Hospital,
to establish a 35-bed rehabilitation hospital
at 14701 Olive Boulevard, Chesterfield, MO 63017,
with a project cost of \$10,117,369.

FURTHER, the Missouri Health Facilities Review Committee commends all persons associated with the application for their assistance in the delivery of high-quality, low-cost health care to the people of the State of Missouri.




H. Bruce Nethington, Chair
Missouri Health Facilities Review Committee

From: Mike Geisel
To: Planning Commission 2007
CC: Connie Fults; Michael Herring; Senior Planners
Date: 4/24/2007 1:09 PM
Subject: Surrey Place

Just for your information:

I have recieved a copy of the traffic study for Kendall Bluffs, and will review it asap.

However, I also requested an accident report from the Chesterfield Police Department for the referenced intersection. They provided the following information. (by the way, Cordovan Commons is in Villas at Ladue Bluffs, adjacent to Kendall Bluffs\Surrey Place, to the west):

No accidents in 2003, 2004, 2005 or 2006. The closest accident was one in 2005, where a motorist hit the back of a stopped motorist, who was waiting to turn into Cordovan Commons.

From: Mike Geisel
To: Planning Commission 2007
CC: heggierob@aol.com; Libbey Malberg; Senior Planners
Date: 5/2/2007 8:37 AM
Subject: Surrey Place

As promised, I've reviewed the traffic study previously provided for Kendall Bluffs, and went back and looked at the City Wide transportation model as well. A voluminous amount of information.

First off, the last traffic study for Kendall Bluffs was related to the petitioner's and the petitioner's engineer's recommendation that there be two separate entrances for Kendall Bluffs and Surrey Place. Their traffic study was based upon, and resulted in a recommendation for two entrances. The City elected to NOT proceed in this fashion and demanded that the entrance to Kendall Bluffs be combined with that of Surrey Place. A secondary, emergency access was permitted west of the Surrey Place entrance. Inasmuch as the traffic study modeled a condition which was not constructed, it is of limited use or benefit. However, the results of the traffic study are descriptive of the problems that are expected to exist along each and every arterial roadway if and when the City is fully developed in conformance with the Comprehensive plan as it currently exists.

Finally, with regard to the intersection performance. The study confirms that under the projected full build out of the City (city wide traffic model), one movement at this intersection (entrance) will have a level of service F (only the left turn out, during the peak evening traffic). So will virtually every intersection and entrance along Olive, Clarkson, and every collector\distributor road in Chesterfield. Under full build out conditions, motorists will experience delays (exceeding 60 seconds at unsignalized intersections and in excess of 80 seconds at signalized intersections) to make a LEFT turn out onto any major arterial\collector roadway due to the increase in background traffic volumes. NOT related to the individual developments themselves. Obviously, this same criteria applies to those individual homes which have driveways that access directly onto arterial and collector roadways.

While not necessary for this development site, at some point we need to discuss the City wide traffic model and how that coincides with what is shown on the city wide plan. We also need to develop a better understanding of what the levels of service truly mean and whether those implied conditions are serious concerns or minor inconvenience.



VIII. E.

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Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

Planning Commission Staff Report

Meeting Date: May 14, 2007

From: Jennifer Yackley, Project Planner

Subject: Rezoning Vote Report

Location: Lot 5 Chesterfield Executive Park (612 & 614 Cepi)

Petition: P.Z. 11-2007 J&T Holdings, LLC

Proposal Summary

J&T Holdings, L.L.C. has submitted an application for a change of zoning from a "M3" Planned Industrial District to a "PC" Planned Commercial District per the regulations of the City of Chesterfield Zoning Ordinance Section 1003.140. The location of this site is south of the intersection of Chesterfield Airport Road and Cepi Drive.

The petitioner is also requesting consideration of a reduction in the parking requirement for this site. Per City of Chesterfield Zoning Ordinance Section 1003.165 the parking spaces for this site are to be calculated at 3.3 spaces/1,000 square feet for office/office buildings and 4.5 spaces/1,000 square feet for medical offices. The total number of required parking spaces is twenty-eight (28). The development has only twenty-six (26) parking spaces and therefore is seeking a reduction.

Staff Recommendation

The Attachment A for this request meets all of the development requirements of the City of Chesterfield and therefore, Staff recommends approval of the change of zoning from "M3" Planned Industrial District to a "PC" Planned Commercial District.

For commercial developments with two (2) or more uses, Section 1003.165 allows a total reduction of not more than twenty (20%) percent of the required parking for the site when it has been demonstrated that adequate parking would be provided. If the parking requirement was reduced by twenty percent (20%) the development would need twenty-two (22) parking spaces. A separate vote of the Planning Commission would be required for consideration of this request.

Zoning Analysis

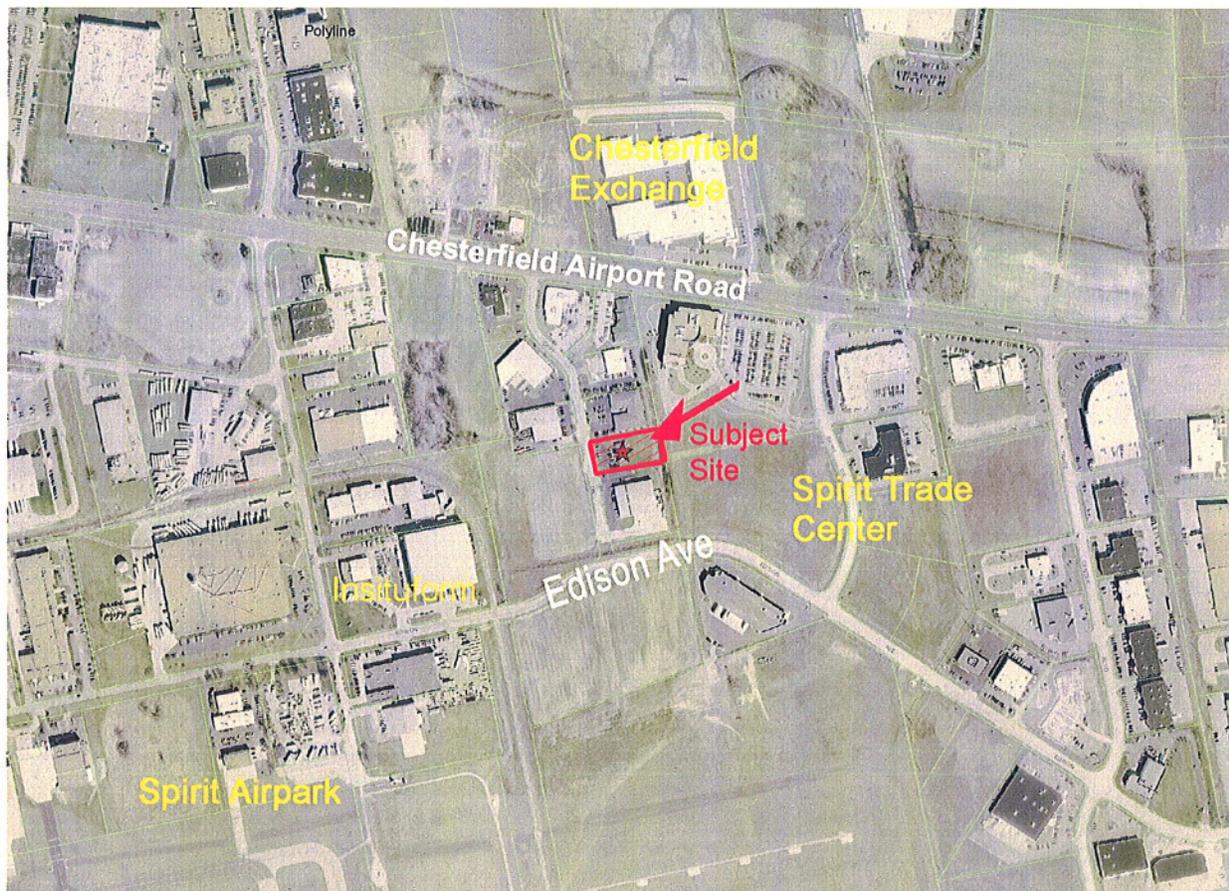
A preliminary plan accompanies all rezoning requests when the change of zoning is to a Planned District. When a vote is taken on a rezoning request, the vote is to approve the change of zoning with an Attachment A written by Staff. The vote is not to approve the accompanying preliminary plan which is provided for informational purposes only.

During site plan review, if a site development plan is submitted which does not meet the City of Chesterfield Code, the site development plan will not be considered for approval before the Planning Commission.

Surrounding Land Use and Zoning

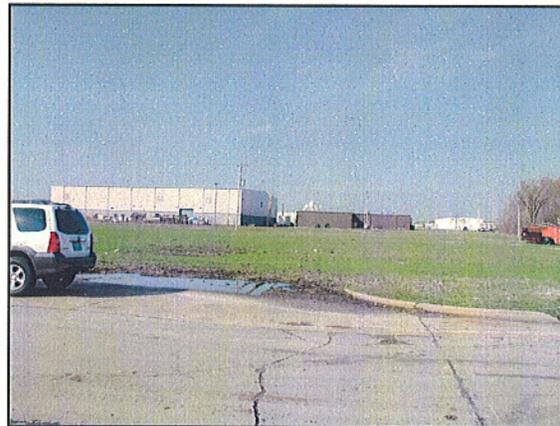
The land use and zoning for the properties surrounding this parcel are as follows:

- North: The property to the north is zoned "M3" Planned Industrial District.
- South: The property to the south is zoned "M3" Planned Industrial District.
- East: The property to the east and is zoned "M3" Planned Industrial District.
- West: The property to the west is zoned "PI" Planned Industrial District.





Looking south at adjacent property



Looking west across Cegi Drive



Existing building on site

Comprehensive Plan Analysis

The subject site is located within Ward 4 of the City of Chesterfield. The Comprehensive Plan designates this area as Service/Business Park. This subject site is not located in any sub-area identified by the Comprehensive Plan; therefore there are no additional development guidelines for this site.

Site Area History

The site which is part of the Chesterfield Executive Park was originally zoned "M-3" Planned Industrial District by St. Louis County Ordinance Number 11,403 on February 17, 1984. The original ordinance has been amended five times:

1. In 1992 City of Chesterfield Ordinance Number 674 amended the previous ordinances to allow for unlit volleyball courts.
2. In 1993 City of Chesterfield Ordinance Number 802 amended the previous ordinances to allow for a change in the number of lots requiring brick veneer.
3. In 1994 City of Chesterfield Ordinance Number 882 amended the previous ordinances to allow for an increased maximum building square footage for lot 8, overall square footage for the development and use of the term "gymnasium" to replace "soccer facility".

4. In 1995 City of Chesterfield Ordinance Number 1030 amended the previous ordinances to allow for a commercial bank with automatic banking and drive-through facilities as permitted uses.
5. In 2005 City of Chesterfield Ordinance Number 2264 amended the structure setbacks for the development.

The Amended Site Development Plan was approved on September 11, 2000.

Issues

The first public hearing was held on this request on April 9, 2007. At that time there were no speakers on this matter other than the Petitioners. At the hearing, several issues were identified including a possible discrepancy regarding the published legal description. After further research Staff found that a discrepancy did exist therefore a second public hearing is being held on May 14, 2007. The Petitioner's responses to the issues from the April 9, 2007 public hearing are attached.

Request

Staff recommends approval of the change of zoning from "M3" Planned Industrial District to a "PC" Planned Commercial District with the Attachment A as written.

Staff requests action on the Petitioner's request for a reduction in the parking requirement for the site. A separate vote of the Planning Commission would be required for approval.

Respectfully submitted,



Jennifer Yackley
Project Planner

Respectfully submitted,



Aimee Nassif
Senior Planner of Zoning
Administration

Attachments

1. Attachment A
2. Petitioner's Response
3. Preliminary Plan

ATTACHMENT A

All provisions of the City of Chesterfield City Code shall apply to this development except as specifically modified herein.

I. SPECIFIC CRITERIA

A. PERMITTED USES

1. The uses allowed in this "PC" Planned Commercial District shall be:
 - a. Associate work and storage areas required by a business, firm or service to carry on business operations.
 - b. Medical and dental offices.
 - c. Offices and office buildings.

B. FLOOR AREA, HEIGHT, BUILDING AND PARKING STRUCTURE REQUIREMENTS

1. A minimum of thirty percent (30%) open space is required for this development.

C. SETBACKS

1. STRUCTURE SETBACKS

No building or structure, other than: a freestanding project identification sign, boundary and retaining walls, light standards, flag poles or fences will be located within the following setbacks:

- a. Eighty (80) feet from the right-of-way of Cepi Drive on the western boundary of the "PC" District.
- b. Ten (10) feet from the southern boundary of the "PC" District.
- c. Thirty-six (36) feet from the eastern boundary of the "PC" District.

- d. Thirty (30) feet from the northern boundary of the "PC" District.
- e. Eight (8) feet from internal lot lines in this development.

2. PARKING SETBACKS

No parking stall, loading space, internal driveway, or roadway, except points of ingress or egress, will be located within the following setbacks:

- a. Twelve (12) feet from the right-of-way of Cepi Drive.
- b. Ten (10) feet from the southern boundary of the "PC" District.
- c. Thirty-three (33) feet from the eastern boundary of the "PC" District.
- d. Five (5) feet from the northern boundary of the "PC" District.
- e. Fifteen (15) feet from the internal lot lines.

D. PARKING AND LOADING REQUIREMENTS

- 1. Parking and loading spaces for this development will be as required in the City of Chesterfield Code.
- 2. Construction Parking
 - a. The streets surrounding this development and any street used for construction access thereto shall be cleaned throughout the day. The developer shall keep the road clear of mud and debris at all times.
 - b. Provide adequate off-street stabilized parking area(s) for construction employees and a washdown station for construction vehicles entering and leaving the site in order to eliminate the condition whereby mud from construction and employee vehicles is tracked onto the pavement causing hazardous roadway and driving conditions.
 - c. No construction related parking shall be permitted within the Cepi Drive right of way.

3. Parking lots shall not be used as streets.

E. LANDSCAPE AND TREE REQUIREMENTS

1. The developer shall adhere to the Tree Manual of the City of Chesterfield Code.

F. SIGN REQUIREMENTS

1. Ornamental Entrance Monument construction, if proposed, shall be reviewed by the City of Chesterfield, and/or the St. Louis County Department of Highways and Traffic, for sight distance considerations prior to installation or construction.
2. Signs shall be permitted in accordance with the regulations of the City of Chesterfield Code.

G. LIGHT REQUIREMENTS

1. Provide a lighting plan and cut sheet in accordance with the City of Chesterfield Code.

H. ARCHITECTURAL

1. The developer shall submit architectural elevations, including but not limited to, colored renderings and building materials. Architectural information is to be reviewed by the Architectural Review Board and the Planning Commission.
2. Building facades should be articulated by using color, arrangement or change in materials to emphasize the facade elements. The planes of the exterior walls may be varied in height, depth or direction. Extremely long facades shall be designed with sufficient building articulation and landscaping to avoid a monotonous or overpowering appearance.
3. Trash enclosures: The location, material, and elevation of any trash enclosures will be as approved by the Planning Commission on the Site Development Plan. All exterior trash areas will be enclosed with a six (6) foot high sight-proof enclosure complimented by adequate landscaping approved by the Planning Commission on the Site Development Plan.
4. Mechanical equipment will be adequately screened by roofing or other material as approved by the Planning Commission.

I. ACCESS/ACCESS MANAGEMENT

1. Access to Cepi Drive shall be limited to one (1) drive approach.

J. PUBLIC/PRIVATE ROAD IMPROVEMENTS, INCLUDING PEDESTRIAN CIRCULATION

1. Provide a special cash escrow for a five (5) foot wide sidewalk, conforming to ADA standards, along Cepi Drive. The sidewalk shall be privately maintained; therefore no public easements shall be required.

K. POWER OF REVIEW

1. The Mayor or a Councilmember of the Ward in which a development is proposed may request that the site plan be reviewed and approved by the entire City Council. This request must be made no later than twenty-four (24) hours before posting the agenda for the next City Council meeting after Planning Commission review and approval of the site plan. The City Council will then take appropriate action relative to the proposal.

L. STORMWATER

1. The site shall provide for the positive drainage of storm water and it shall be discharged at an adequate natural discharge point or connected to an adequate piped system.
2. Detention/retention and other storm water quantity and quality management measures are to be provided in each watershed as required by the City of Chesterfield. The storm water quantity management facilities, related to flood and channel protection, shall be operational prior to paving of any driveways or parking areas in non-residential development or issuance of building permits exceeding sixty percent (60%) of approved dwelling units in each plat, watershed or phase of residential developments. The location and types of storm water management facilities shall be identified on the Site Development Plan.

P. GEOTECHNICAL REPORT.

1. Provide a geotechnical report, prepared by a registered professional engineer licensed to practice in the State of Missouri, as directed by the City of Chesterfield. The report shall verify the suitability of grading and proposed improvements with soil and geologic conditions and address the existence of any potential sinkhole, ponds, dams, septic fields, etc., and recommendations for treatment. A statement of compliance, signed and sealed by the

geotechnical engineer preparing the report, shall be included on all Site Development Plans and Improvement Plans.

Q. MISCELLANEOUS

1. All utilities will be installed underground. The development of this parcel will coordinate the installation of all utilities in conjunction with the construction of any roadway on site.

II. TIME PERIOD FOR SUBMITTAL OF SITE DEVELOPMENT CONCEPT PLANS AND SITE DEVELOPMENT PLANS

- A. The developer shall submit a concept plan within eighteen (18) months of City Council approval of the change of zoning.
- B. In lieu of submitting a Site Development Concept Plan and Site Development Section Plans, the petitioner may submit a Site Development Plan for the entire development within eighteen (18) months of the date of approval of the change of zoning by the City.
- C. Failure to comply with these submittal requirements will result in the expiration of the change of zoning and will require a new public hearing.
- D. Said Plan shall be submitted in accordance with the combined requirements for Site Development Section and Concept Plans. The submission of Amended Site Development Plans by sections of this project to the Planning Commission shall be permitted if this option is utilized.
- E. Where due cause is shown by the developer, this time interval for plan submittal may be extended through appeal to and approval by the Planning Commission.

III. COMMENCEMENT OF CONSTRUCTION

- A. Substantial construction shall commence within two (2) years of approval of the site development concept plan or site development plan, unless otherwise authorized by ordinance.
- B. Where due cause is shown by the developer, the Commission may extend the period to commence construction for not more than one (1) additional year.

IV. GENERAL CRITERIA

A. Site Development Concept Plan, Site Development Section Plan, Site Development Plans.

1. Any site development plan shall show all information required on a sketch plan as required in the City of Chesterfield Code.
2. Include a conceptual landscape plan in accordance with the City of Chesterfield Code to indicate proposed landscaping along arterial and collector roadways.
3. Include a lighting plan in accordance with the City of Chesterfield Code to indicate proposed lighting along arterial collector roadways.
4. Provide comments/approvals from the appropriate Fire District, the Metropolitan St. Louis Sewer District, the St. Louis County Department of Highways and Traffic, Monarch Levee District, Spirit of St. Louis Airport and the Missouri Department of Transportation.

V. CHESTERFIELD VALLEY TRUST FUND

Roads

The roadway improvement contribution is based on land and building use. The roadway contributions are necessary to help defray the cost of engineering, right of way acquisition, and major roadway construction in accordance with the Chesterfield Valley Road Improvement Plan on file with the St. Louis County Department of Highways and Traffic. The amount of the developer's contribution to this fund shall be computed based on the following:

<u>Type of Development</u>	<u>Required Contribution</u>
Commercial	\$2.07/ sq. ft. of building space
Office	\$1.44/sq. ft. of building space
Industrial	\$4,986/acre

If types of development proposed differ from those listed, rates shall be provide by the St. Louis County Department of Highways and Traffic.

Credits for roadway improvements required will be awarded as directed by St. Louis County Highways and Traffic. Any portion of the roadway improvement contribution that remains following completion of road improvements required by the development shall be retained in the appropriate Trust Fund.

The roadway improvement contribution shall be deposited with the St. Louis County Department of Highways and Traffic. The deposit shall be made before

the issuance of a Special Use Permit (S.U.P.) by the St. Louis County Highways and Traffic or a Building Permit by St. Louis County Public Works Department. Funds shall be payable to "Treasurer, St. Louis County".

Water Main

The primary water line contribution is based on gross acreage of the development land area. The contribution shall be a sum of \$654.66 per acre for the total area as approved on the Site Development Plan to be used solely to help defray the cost of construction of the primary water line serving the Chesterfield Valley area.

The primary water line contribution shall be deposited with the St. Louis County Department of Highways and Traffic. The deposit shall be made before approval of the Site Development Plan by the St. Louis Department of Highways and Traffic. Funds shall be payable to "Treasurer, St. Louis County".

Stormwater

The storm water contribution is based on gross acreage of the development land area. These funds are necessary to help defray the cost of engineering and construction improvements for the collection and disposal of storm water from the Chesterfield Valley in accordance with the Master Plan on file with and jointly approved by St. Louis County and the Metropolitan St. Louis Sewer District. The amount of the storm water contribution will be computed based on \$2,077.15 per acre for the total area as approved on the Site Development Plan. The storm water contributions to the Trust Fund shall be deposited with the St. Louis County Department of Highways and Traffic. The deposit shall be made before the issuance of a Special Use Permit (S.U.P.) by St. Louis County Highways and Traffic or a Building Permit by St. Louis County Public Works Department. Funds shall be payable to "Treasurer, St. Louis County".

Sanitary Sewer

The sanitary sewer contribution is collected as the Caulks Creek Impact Fee.

The sanitary sewer contribution within Chesterfield Valley area shall be deposited with the Metropolitan St. Louis Sewer District as required by the District.

The amount of these required contributions for the roadways, storm water and primary water line improvements, if not submitted by January 1, 2008 shall be adjusted on that date and on the first day of January in each succeeding year thereafter in accordance with the construction cost index as determined by the St. Louis County Department of Highways and Traffic.

Trust Fund contributions shall be deposited with St. Louis County in the form of a cash escrow prior to the issuance of building permits.

VI. RECORDING

Within 60 days of approval of any development plan by the City of Chesterfield, the approved Plan will be recorded with the St. Louis County Recorder of Deeds. Failure to do so will result in the expiration of approval of said plan and require re-approval of a plan by the Planning Commission.

VII. ENFORCEMENT

- A. The City of Chesterfield, Missouri will enforce the conditions of this ordinance in accordance with the Site Development Plan approved by the City of Chesterfield and the terms of this Attachment A.
- B. Failure to comply with any or all the conditions of this ordinance will be adequate cause for revocation of approvals/permits by reviewing Departments and Commissions.
- C. Non-compliance with the specific requirements and conditions set forth in this Ordinance and its attached conditions or other Ordinances of the City of Chesterfield shall constitute an ordinance violation, subject, but not limited to, the penalty provisions as set forth in the City of Chesterfield Code.
- D. Waiver of Notice of Violation per the City of Chesterfield Code.
- E. This document shall be read as a whole and any inconsistency to be integrated to carry out the overall intent of this Attachment A.

BOISAUBIN & AGATHEN
ATTORNEYS AT LAW
SUITE 610
130 SOUTH BEMISTON AVENUE
CLAYTON, MISSOURI 63105



ALFRED L. BOISAUBIN (RET.)
ALAN J. AGATHEN

TELEPHONE (314) 862-8621
FACSIMILE (314) 862-8623
E-MAIL agathenlaw@sbcglobal.net

May 7, 2007

Ms. Jennifer Yackley
Project Planner
City of Chesterfield
690 Chesterfield Parkway W
Chesterfield, MO 63017

Re: P.Z. 11-2007
I&T Holdings (612 Cepi)

Dear Ms. Yackley:

This is in response to your issues letter on the above-referenced project dated April 16, 2007. I will respond using the numbering contained in that letter.

Issues

1. Be advised that the parking requirements for this site, per Section 1003.165 of the City of Chesterfield Zoning Ordinance, are as follows:

- a. Medical office: 4.5 spaces/1,000 square feet of gross floor area
- b. Office buildings: 3.3 spaces/1,000 square feet of gross floor area

Please confirm that the site meets the parking requirements.

Response:

The plat which was submitted to the City in conjunction with our application for rezoning reflects that following square footage: existing building, 612 Cepi Drive: 3,100 s.f.; new building, 614 Cepi Drive: 4,200 s.f. My calculation is therefore as follows:

612 Cepi Drive (rezoned to medical office building):

$$3.1 \times 4.5 = 13.95$$

614 Cepi (remains office building):

$$4.2 \times 3.3 = \underline{13.86}$$

27.81

The plat reflects that there are 26 parking spaces, although I count 27 from the drawing itself. In either event, we are requesting a reduction of the parking spaces required for this project, i.e., from 27.81 to 27 (or 26 if the written description of the parking available governs over the drawn plat, in the City's view).

Ms. Jennifer Yackley
May 7, 2007
Page 2

2. Provide comments from the Valley Master Plan Committee.

Response:

The comments of Mr. Bill Kirchoff of the Valley Master Plan Committee are enclosed.

The following conditions related to site specific issues will be applied to the rezoning petition:

Open Space

1. The development shall have a minimum open space of thirty percent (30%).

Response:

The plat shows a total lot area of 32,670 s.f. The developed area is 20,531 s.f. The open space is, therefore, 37.16% of the total lot, obviously exceeding the required 30% figure. The project, therefore, currently complies with the condition and will continue to comply with the condition.

Setbacks

2. No building or structure, other than: a freestanding project identification sign, boundary and retaining walls, light standards, flag poles or fences will be located within the following setbacks:
 - a. Eighty (80) feet from the right of way of Cepi Road.
 - b. Ten (10) feet from the southern boundary of this "PC" District.
 - c. Forty (40) feet from the eastern boundary of the this "PC" District.
 - d. Thirty (30) feet from the western boundary of this "PC" District.

Response:

With regard to the all the setbacks, the project currently complies with the condition and will continue to comply with the condition.

Access

3. Access to Cepi Drive shall be limited to one drive approach.

Response:

The project will continue to have only the one access to Cepi Drive. It therefore currently complies and will continue to comply with the condition.

Pedestrian Circulation

4. Provide a special cash escrow for a five (5) foot wide sidewalk, conforming to ADA standards, along Cepi Drive. The sidewalk shall be privately maintained; therefore, no public easements shall be required.

Ms. Jennifer Yackley

May 7, 2007

Page 3

Response:

The project owners will comply with the cash escrow requirement for the sidewalk.

Parking

5. No construction related parking shall be permitted within the Cepi Drive right of way.

Response:

No construction is contemplated in this project and the condition is therefore moot. The project therefore complies with the condition and will continue to comply with the condition.

6. St. Louis County Department of Highways and Traffic has submitted the following comments:
- a. The project is located within the Chesterfield Valley Trust Fund. Current trust fund rates will be included in the Attachment A for this development.
 - b. The development must contribute monies for storm water, sanitary sewer and primary water line improvements. Contribution amounts will be included in the Attachment A for this development.
 - c. Traffic generation assessment contributions shall be deposited with St. Louis County prior to the issuance of building permits. If development phasing is anticipated, the developer shall provide the traffic generation assessment contribution prior to issuance of building permits for each phase of development.
 - d. Provide adequate temporary off-street parking for construction employees. Parking on non-surfaced areas shall be prohibited in order to eliminate the condition whereby mud from construction and employee vehicles is tracked onto the pavement causing hazardous roadway and driving conditions.

Response:

The project will comply with the St. Louis County Department of Highways and Traffic conditions as specified in subparagraphs a, b, c and d.

Please let me know if the City of Chesterfield needs anything additional from this office at this time. Thank you.

Very truly yours,


Alan Agathen

AJA/sh

Enclosure

TREE SCHEDULE

QUANTITY	COMMON NAME	BOT. NAME	SIZE	ROOT COND.	REMARKS
4	Red Maple	Acer rubrum "Red Sunset"	2" - 3 1/2" cal.	1" B&D	Spring Planting
3	Parrotia Dogwood	Cornus alternifolia	2" (10" W)	B&D	none
6	Flowering Shrub		2" - 3" spread	pot	none
8	Evergreen Shrub		2" - 3" spread	pot	none

⊗ EXISTING TREE (T.B.R.) ⊕ EXISTING TREE (T.R.)

GRAPHIC SCALE



AMENDED SITE DEVELOPMENT PLAN,
LOT 5, CHESTERFIELD EXECUTIVE PARK

OWNER / DEVELOPER

J. & T. ENTERPRISES,
A MISSOURI PARTNERSHIP
612 CEPI DRIVE
CHESTERFIELD, MO 63005

REGISTERED

15021 VOLKMAN
13225 OLIVE STREET
894 ROCKWELL ROAD, SUITE 200
ST. LOUIS, MISSOURI 63114
P. (636) 256-3800 F. (636) 256-3811

OUTBOUNDARY SURVEY BY

VOLE ENGINEERING AND SURVEYING, INC.
15849 INDIAN HEAD INDUSTRIAL BOULEVARD
ST. LOUIS, MISSOURI 63133
DATE OF SURVEY: 7/20/85

TOPOGRAPHIC SURVEY BY

AME ENGINEERING COMPANY, INC.
500 MADISON STREET
ST. CHARLES, MISSOURI 63301
DATE OF SURVEY: 7/20/85

A TRACT OF LAND BEING

PART OF LOT 5 OF "DANAW PROVISIONS ESTATE PARTITION" IN U.S. SURVEY 150, T. 45 N. R. 3 E., CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS: ALL OF LOT 5 OF THE AMENDED PLAN OF CHESTERFIELD EXECUTIVE PARK AS RECORDED IN PLAT BOOK 231, PAGE 59 OF THE ST. LOUIS COUNTY RECORDS.

PROJECT BENCHMARK

CORNER IN CENTERLINE OF FRONTAGE OF CEPI DRIVE AT P.I. OF CURVE STATION 544+25
13225 OLIVE STREET ROAD
CHESTERFIELD, MO 63077
ELEVATION = 461.07

FIRE DISTRICT

CHESTERFIELD FIRE PROTECTION DISTRICT
13225 OLIVE STREET ROAD
CHESTERFIELD, MO 63077
(314) 336-4571

SCHOOL DISTRICT

ROCKWOOD SCHOOL DISTRICT
113 EAST NORTH STREET
EUREKA, MO 63025
(314) 336-2200

SITE INFORMATION

EXISTING ZONING: M-3 PLANNED INDUSTRIAL DISTRICT / FLOOD PLAN DISTRICT
TOTAL AREA OF LOT: 20,531 SQ. FT. OR 0.472 ACRES
DEVELOPED AREA: 20,531 SQ. FT. OR 0.472 ACRES
BUILDING AREA: 3,100 SQ. FT. (TWO)
BUILDING AREA: 4,200 SQ. FT. (TWO)
7,000 SQ. FT. (TWO)

PROPOSED USE: OFFICE BUILDING

FLOODPLAIN DATA

THE 100 YEAR FLOOD PLAN ELEVATION IS 458 PER THE FEMA FIRM NUMBER 28186C0120H DATED 12/25/83 FOR ST. LOUIS COUNTY, MO AND INCORPORATED AREAS AND HEIGHTS TO REFLECT CORN DATED 4/17/80.

PARKING CALCULATIONS

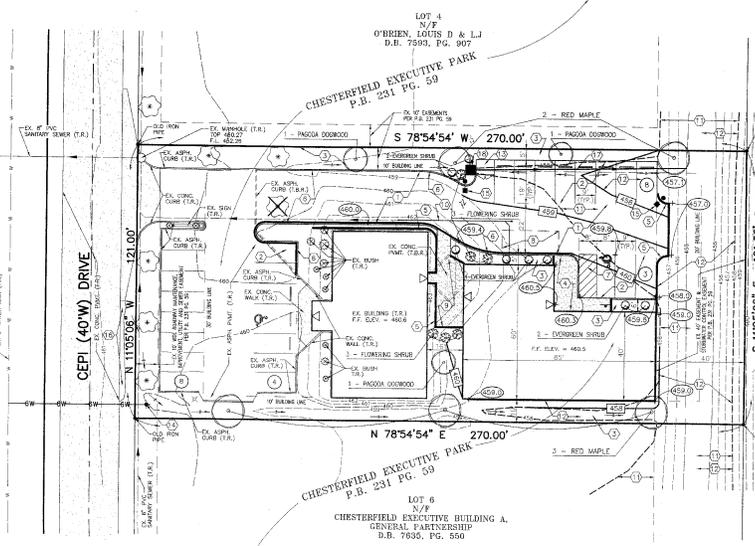
PARKING REQUIRED:
3.33 SPACES / 1000 S.F. OF BUILDING AREA
= (3.33 / 1,000) * (7,500)
= 25 SPACES
PARKING PROVIDED:
STANDARD SPACES = 24 SPACES
VAN ACCESSIBLE SPACES = 2 SPACES

GENERAL NOTES

- CONCRETE AND DRAINAGE SHALL BE IN ACCORDANCE WITH ST. LOUIS COUNTY, THE CITY OF CHESTERFIELD AND U.S.D. STANDARDS.
- ALL DIMENSIONS ARE TO FACE OF CURB UNLESS NOTED OTHERWISE.
- EXISTING UNDERGROUND UTILITIES SHOWN HEREON ARE FROM INFORMATION PROVIDED BY UTILITIES AND MUST BE CONSIDERED AS APPROXIMATE. CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING EXACT LOCATION PRIOR TO COMMENCING WORK.
- ALL MANHOLES AND ACCESS REQUIREMENTS SHALL COMPLY WITH SECTION 512.4 OF THE ST. LOUIS COUNTY BUILDING CODE.
- SELECTION CONTROL TO BE PROVIDED AT PROPERTY LINES AND SHOWN ON FINAL PLANS.
- SEWERAGE SYSTEMS WILL BE DESIGNED IN ACCORDANCE WITH THE CITY OF CHESTERFIELD AND U.S.D. STANDARDS.
- SUPPLY SHALL NOT EXCEED 3 (HORIZONTAL) TO 1 (VERTICAL), UNLESS SPECIFICALLY APPROVED BY THE DISTRICT ENGINEER OF PUBLIC WORKS.
- ALL WORK IN CEPI DRIVE SHALL BE IN ACCORDANCE WITH THE CITY OF CHESTERFIELD STANDARDS.
- ALL WORK DISTURBED BY CONSTRUCTION SHALL BE RESTORED TO ORIGINAL CONDITION.
- ALL ENHANCEMENTS TO MEET THE CITY OF CHESTERFIELD COMMERCIAL ENTRANCE STANDARDS.
- SELECTION CONTROL MEASURES ARE REQUIRED AS DIRECTED BY ST. LOUIS COUNTY AND CITY OF CHESTERFIELD.
- ALL STORMWATER SHALL BE DISCHARGED AT AN ADEQUATE NATURAL DISCHARGE POINT.
- PROVIDE ADEQUATE TEMPORARY OFF-STREET PARKING FOR CONSTRUCTION EMPLOYEES. PARKING ON NON-SURFACED AREAS SHALL BE PROHIBITED.
- GAS, WATER, ELECTRIC AND TELEPHONE SERVICE CONNECTIONS TO NEW BUILDING SHALL BE THROUGH EXISTING CONDUITS.
- THIS PLAN IS FOR PLANNING AND ZONING PURPOSES ONLY AND SHALL NOT BE USED FOR A SITE CONSTRUCTION DRAWING.

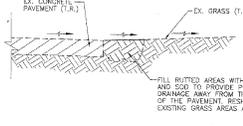
KEYED NOTES

- NEW PAVED DRIVE ASLE.
- NEW PAVED PARKING AREA.
- NEW LANDSCAPE AREA.
- NEW CONCRETE WALK.
- NEW RETAINING WALL.
- NEW 6" HIGH CURB.
- NEW STRIPED ASLE.
- NEW 10" X 20" LANDING AREA.
- NEW PATIO.
- NEW SANITARY LATERAL.
- EXISTING 100 YEAR FLOODPLAIN LIMITS.
- PROPOSED 100 YEAR FLOODPLAIN LIMITS.
- NEW 6" - 8" HIGH-PIECE TRASH ENCLOSURE WITH 6" - 8" HIGH CEDAR "SHADOW BOX" STOCKADE FENCE.
- NEW FIRE HYDRANT.
- NEW 30" HIGH SITE LIGHT.
- FALL RUTS WITH TOP SOIL AND 500 PER THE RUT DETAIL ON THIS SHEET.
- NEW STORM DRAIN PIPE.
- NEW AREA INLET.

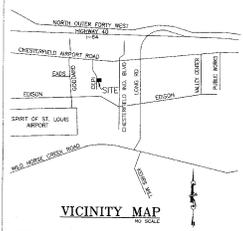


LEGEND

- ⊙ KEYED NOTE DESIGNATOR
- ⊕ NUMBER OF PARKING STALLS IN A PARKING BANK
- ⊗ CONCRETE FINISHMENT
- ⊘ FLOW LINE
- ⊙ FRESH FLOOR
- ⊙ TO REMAIN
- ⊙ TO BE REMOVED
- ⊙ TO BE REMOVED AND REPLACED
- ⊙ TO BE RELOCATED
- ⊙ EXISTING
- ⊙ TOP OF WALL
- ⊙ BOTTOM OF WALL
- ⊙ BUILDING ENTRY
- ⊙ EXISTING 30' HIGH SITE LIGHT (T.R.)
- ⊙ PROPOSED CONTOUR



RUT DETAIL
NO SCALE



VICINITY MAP

UTILITY COMPANIES

ST. LOUIS COUNTY WATER
310 NORTH NEW BALLAS ROAD
ST. LOUIS, MO 63314
(314) 991-3400

TELEPHONE
SOUTHWESTERN BELL TELEPHONE
14760 MANCHESTER RD.
BALTIMORE, MO 63011
(314) 948-1312

STORM & SANITARY SEWERS

METROPOLITAN ST. LOUIS
SEWER DISTRICT
2300 HAMPTON AVENUE
ST. LOUIS, MO 63139
(314) 768-6200

NATURAL GAS
LACLEDE GAS COMPANY
725 OLIVE ST., RM. 408
ST. LOUIS, MO 63101
(314) 542-0685

ELECTRIC

UNION ELECTRIC
PO BOX 148
ST. LOUIS, MO 63116
(314) 504-4509

F&P
FARNSWORTH & WOLFE
Engineers
Architects
144 West Lockwood Ave.
St. Louis, Missouri 63118
314.762.7900, 314.476.1253 fax
www.fandw.com
a member of
the Farnsworth Wolfe Group

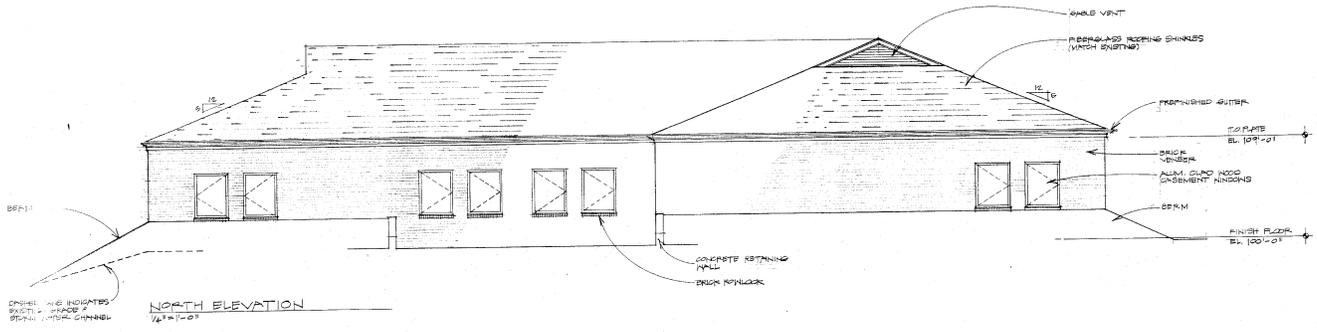
STATE OF MISSOURI
DAVID B. HART
REGISTERED PROFESSIONAL ENGINEER
NO. 12438
AUG 29 2000
MISS. E. & S. 1988

CONSULTANT:
PROJECT:
THE HAWLEY BUILDING
612 CEPI DRIVE
CHESTERFIELD, MISSOURI 63005

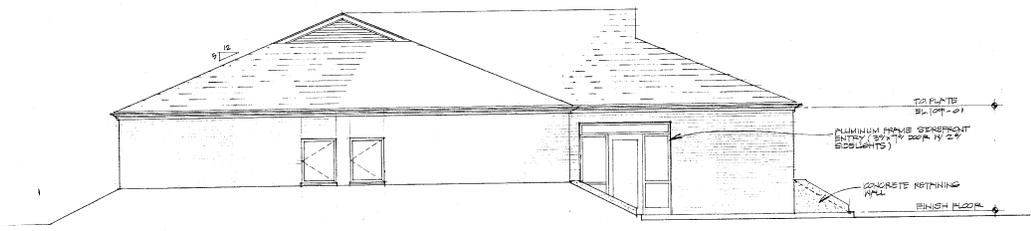
REVISION:
6/2/00 CITY COMMENTS
7/5/00 CITY COMMENTS
8/10/00 CITY COMMENTS
8/29/00 CITY COMMENTS

DATE: 2/22/00
DRAWN: MLJR
APPROVED: DLH
BOOK NO.: 0
FILE NO.: 200007SDP.DWG

SHEET TITLE
AMENDED SITE DEVELOPMENT PLAN
LOT 5, CHESTERFIELD EXECUTIVE PARK
MAY 13 2000
NON-ELECTRIC CO.
SHEET NUMBER
SDP-1
PROJECT NO.: 20007



NORTH ELEVATION
1/2" = 1'-0"



EAST ELEVATION
1/2" = 1'-0"

Date:
Revisions:
Sheet:

Project:
**NEW OFFICE BUILDING for
HAWLEY BROKERAGE**
612 Cepi Road
Chesterfield, Missouri 63005

volding architecture
884 Woods Mill Road
Suite 200
St. Louis, MO 63011
636.256.3800



690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

Planning Commission Staff Report

Meeting Date: May 14, 2007

Subject: **Rezoning Issues Report**

Meeting Date: April 23, 2007

From: Annissa G. McCaskill-Clay, Assistant Director of Planning

Location: 1707 Wilson Road

Petition: P.Z. 14-2007a Wilson Bluffs (SMS Group, L.L.C.):

Proposal Summary

Sean Sortor of SMS Group, LLC., has submitted an application for a change of zoning from "NU" Non-Urban to "E Half Acre" District, per the regulations of City of Chesterfield Zoning Ordinance Section 1003.107 (Estates Districts). The location of the site is on the west side of Wilson road, directly north of Wilson Manors II Subdivision.

Staff Recommendation

The Attachment A for this request meets all of the development requirements of the City of Chesterfield and therefore, Staff recommends approval of the change of zoning from "NU" Non-Urban District to a "E-One Acre" District.

Zoning Analysis

A preliminary plan and Attachment A are not necessary as the Petitioner is attempting a "straight zoning." That is, the subject site will be developed directly from the requirements of the Estate District section of the City of Chesterfield Zoning Ordinance without exemption

Surrounding Land Use and Zoning

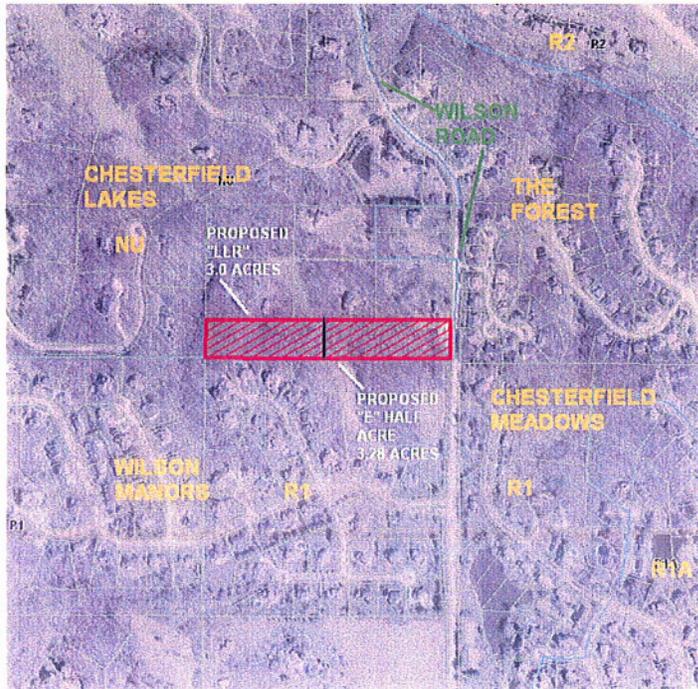
The land use and zoning for the properties surrounding this parcel are as follows:

North: The properties to the north are zoned "NU" Non-Urban.

South
And

West: The properties to the south and to the west are part of the "R1" (PEU)-zoned Wilson Manors subdivision.

East: Directly to the east is Wilson Road; "R1" Residence District-zoned Chesterfield Meadows is located directly across Wilson.



Comprehensive Plan Analysis

The subject site is located within Ward 4 of the City of Chesterfield. The Comprehensive Plan calls for this area to be single family residential. There is no recommendation for density for this area and it is not located in any sub-area identified by the Comprehensive Plan; therefore there are no additional development guidelines for this site.

Site Area History

The subject site was zoned "NU" Non-Urban prior to incorporation by the City of Chesterfield.

Issues

A Public Hearing was held March 26, 2007. At that time several issues were identified. Those issues along with the Petitioner's responses are attached. Please note, the Petitioner has agreed to amend his initial request from "E Half Acre" District to an "E

One Acre" District. . There were no additional issues raised at the April 23, 2007 meeting.

Request

Staff recommends approval of the change of zoning from "NU" Non-Urban District to "E One Acre" District.

Respectfully submitted,

Annissa McCaskill-Clay

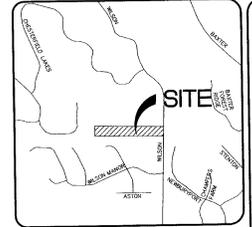
Annissa G. McCaskill-Clay, AICP
Assistant Director of Planning

WILSON BLUFFS

A TRACT OF LAND LOCATED IN THE SOUTH PART OF THE
SOUTHWEST 1/4 OF THE NORTHWEST 1/4 OF SECTION 16,
TOWNSHIP 45 NORTH, RANGE 4 EAST
ST. LOUIS COUNTY, CITY OF CHESTERFIELD, MO

PROJECT DATA

SITE ADDRESS: 1702 WILSON AVENUE
LOCAL OR HAMBLETT: 18700019
NAME OF TRACT: SEAN GORTER (UNDER CONTRACT)
DATE: 7/24/07
EXISTING ZONING: U/LR - LARGE LOT RESIDENTIAL
APPROVED ZONING: U/LR - LARGE LOT RESIDENTIAL
PROJECT OWNER: SEAN GORTER
PROJECT ADDRESS: 1702 WILSON AVENUE
PROJECT PHONE: 636.425.2594
PROJECT FAX: 636.425.2594
PROJECT E-MAIL: SEAN@SEANWILSON.COM
DATE: 7/24/07
DRAWN BY: M. DEATY
CHECKED BY: M. DEATY
DATE: 7/24/07
SCALE: AS SHOWN
PROJECT LOCATION: 1702 WILSON AVENUE
PROJECT ADDRESS: 1702 WILSON AVENUE
PROJECT PHONE: 636.425.2594
PROJECT FAX: 636.425.2594
PROJECT E-MAIL: SEAN@SEANWILSON.COM

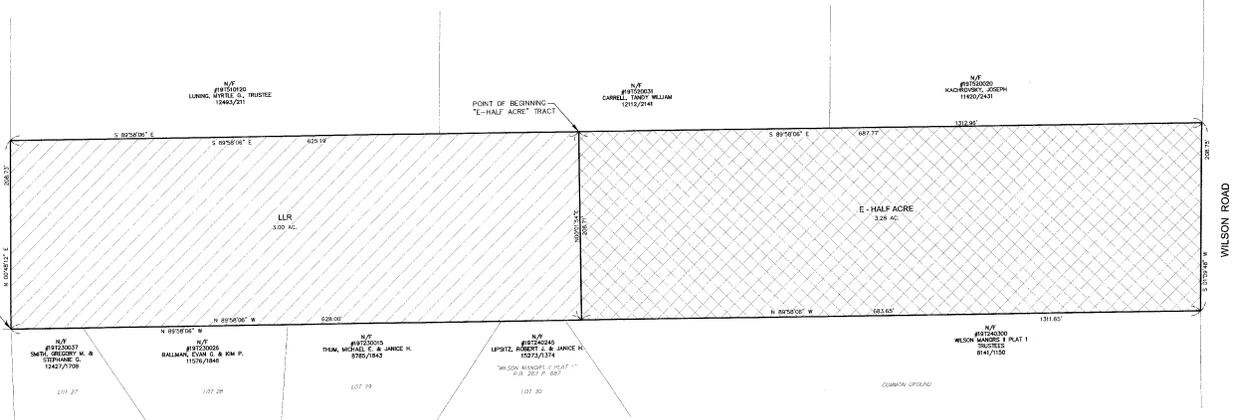


LOCATION MAP

SEAN GORTER
1702 WILSON AVENUE
ST. LOUIS, MO 63137
636.425.2594
636.425.2594
SEAN@SEANWILSON.COM

Vance Engineering, Inc.
1702 Wilson Rd.
St. Louis, MO 63132
636.425.2594
636.425.2594
SEAN@SEANWILSON.COM

WILSON BLUFFS
PRELIMINARY PLAN



LEGAL DESCRIPTION - WILSON BLUFFS
A TRACT OF LAND IN THE SOUTH PART OF THE SOUTHWEST ONE QUARTER OF THE NORTHWEST ONE QUARTER OF SECTION 16, IN TOWNSHIP 45 NORTH, RANGE 4 EAST, CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI AND BEING MORE PARTICULARLY DESCRIBED AS:
BEGINNING AT THE NORTHWEST CORNER OF LOT 21 OF WILSON MANORS 8 PLAT 1 AS RECORDED IN PLAT BOOK 282 PAGE 68 OF THE ST. LOUIS COUNTY RECORDS, BEING ALSO THE SOUTHWEST CORNER OF LOT 20 OF "DUSTYFIELD LANE" AS RECORDED IN PLAT BOOK 100 PAGE 38 OF THE ST. LOUIS COUNTY RECORDS, THENCE NORTH 00 DEGREES 48 MINUTES 12 SECONDS EAST 200 FEET ALONG THE EAST LINE OF SAID LOT 20 OF "DUSTYFIELD LANE" TO THE SOUTHWEST CORNER OF PROPERTY OWNED BY MARVELL G. LANGSTON, THENCE SOUTH 89 DEGREES 58 MINUTES 00 SECONDS EAST 150 FEET ALONG THE SOUTH LINE OF SAID PROPERTY TO THE POINT OF BEGINNING AND CONTAINING 0.38 ACRES MORE OR LESS.

LEGAL DESCRIPTION - "E - HALF ACRE"
A TRACT OF LAND IN THE SOUTH PART OF THE SOUTHWEST ONE QUARTER OF THE NORTHWEST ONE QUARTER OF SECTION 16, IN TOWNSHIP 45 NORTH, RANGE 4 EAST, CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI AND BEING MORE PARTICULARLY DESCRIBED AS:
BEGINNING AT THE NORTHWEST CORNER OF LOT 21 OF WILSON MANORS 8 PLAT 1 AS RECORDED IN PLAT BOOK 282 PAGE 68 OF THE ST. LOUIS COUNTY RECORDS, BEING ALSO THE SOUTHWEST CORNER OF LOT 20 OF "DUSTYFIELD LANE" AS RECORDED IN PLAT BOOK 100 PAGE 38 OF THE ST. LOUIS COUNTY RECORDS, THENCE NORTH 00 DEGREES 48 MINUTES 12 SECONDS EAST 200 FEET ALONG THE EAST LINE OF SAID LOT 20 OF "DUSTYFIELD LANE" TO THE SOUTHWEST CORNER OF PROPERTY OWNED BY MARVELL G. LANGSTON, THENCE SOUTH 89 DEGREES 58 MINUTES 00 SECONDS EAST 150 FEET ALONG THE SOUTH LINE OF SAID PROPERTY TO THE POINT OF BEGINNING AND CONTAINING 3.28 ACRES MORE OR LESS.

LEGAL DESCRIPTION - "LLR"
A TRACT OF LAND IN THE SOUTH PART OF THE SOUTHWEST ONE QUARTER OF THE NORTHWEST ONE QUARTER OF SECTION 16, IN TOWNSHIP 45 NORTH, RANGE 4 EAST, CITY OF CHESTERFIELD, ST. LOUIS COUNTY, MISSOURI AND BEING MORE PARTICULARLY DESCRIBED AS:
BEGINNING AT THE NORTHWEST CORNER OF LOT 21 OF WILSON MANORS 8 PLAT 1 AS RECORDED IN PLAT BOOK 282 PAGE 68 OF THE ST. LOUIS COUNTY RECORDS, BEING ALSO THE SOUTHWEST CORNER OF LOT 20 OF "DUSTYFIELD LANE" AS RECORDED IN PLAT BOOK 100 PAGE 38 OF THE ST. LOUIS COUNTY RECORDS, THENCE NORTH 00 DEGREES 48 MINUTES 12 SECONDS EAST 200 FEET ALONG THE EAST LINE OF SAID LOT 20 OF "DUSTYFIELD LANE" TO THE SOUTHWEST CORNER OF PROPERTY OWNED BY MARVELL G. LANGSTON, THENCE SOUTH 89 DEGREES 58 MINUTES 00 SECONDS EAST 150 FEET ALONG THE SOUTH LINE OF SAID PROPERTY TO THE POINT OF BEGINNING AND CONTAINING 3.28 ACRES MORE OR LESS.

PRELIMINARY PLAN DISCLAIMER
THIS PLAN WAS PREPARED USING RECORDS AVAILABLE TO THE PUBLIC. THE PREPARATION OF THIS PLAN WAS BASED ON THE INFORMATION PROVIDED BY THE CLIENT. VANCE ENGINEERING, INC. DOES NOT WARRANT THE ACCURACY OF THE INFORMATION PROVIDED BY THE CLIENT. THE CLIENT IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES. THIS PLAN IS A PRELIMINARY PLAN AND IS NOT TO BE USED FOR CONSTRUCTION.



REVISED
06/076
BASEMAP NO.
01/31/07
1
COPYRIGHT 2007



VIII. G.

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

Planning Commission Staff Report

Subject: Ordinance Amendment Vote Report

Meeting Date: May 14, 2007

From: Mara M. Perry, AICP, Senior Planner of Site Plan Review

Location: North of the intersection of Spirit Drive North and Chesterfield Airport Road, South of I-64/ U.S. 40-61, West of Trade Center Boulevard

Petition: P.Z. 15-2007 Cambridge Engineering (Kramer Commerce Center)

Proposal Summary

Kramer Assets Group has submitted an application for an ordinance amendment to City of Chesterfield Ordinance 1717. The petitioner is requesting an amendment to the parking setbacks to allow for shared driveways within the existing parking setbacks. The petitioner is also requesting a change from “green space” to “open space” and the removal of the reference to “stormwater drainage structures”.

Staff Recommendation

The Attachment A for this request meets all of the development requirements of the City of Chesterfield and therefore, Staff recommends approval of the ordinance amendment request.

Ordinance Amendment Analysis

The petitioner is requesting an amendment to the parking setbacks to allow for a shared driveway within the existing parking setbacks which will provide access from Long Road Crossing Drive through Lot 2 for Lot 1. There is an existing ingress egress easement in that location.

The petitioner is also requesting a change from “green space” to “open space” and the removal of the reference to “stormwater drainage structures”. This will be consistent with the current use of “openspace” in City ordinances.

Surrounding Land Use and Zoning

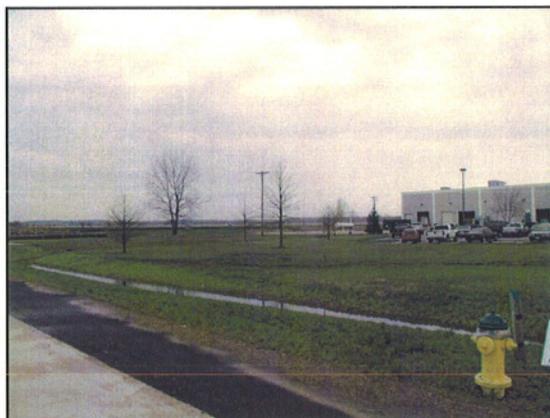
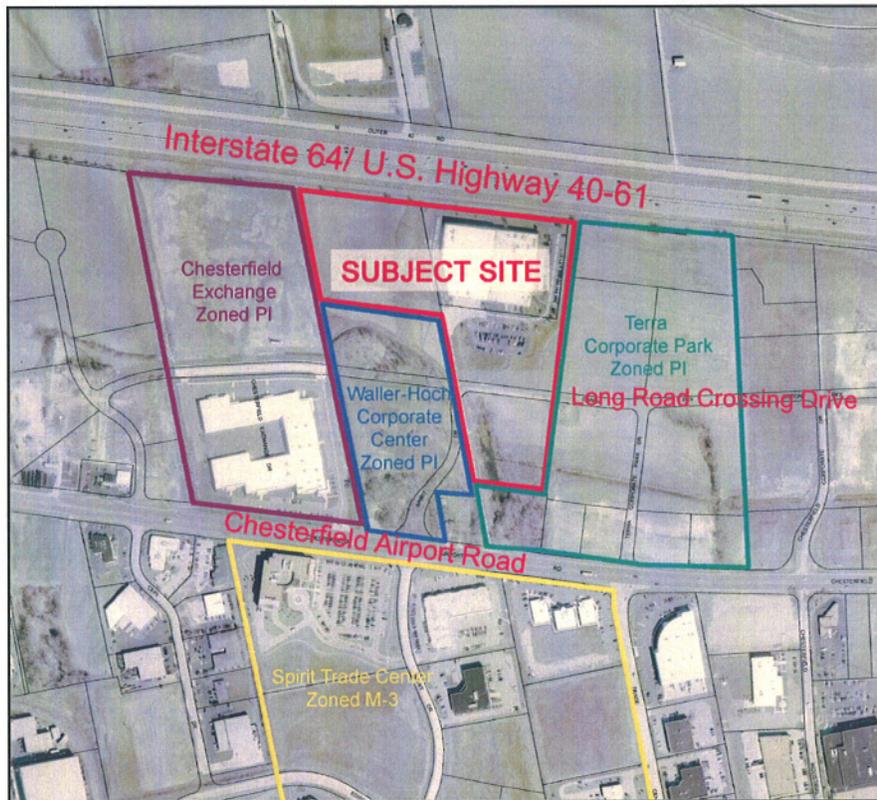
The land use and zoning for the properties surrounding this parcel are as follows:

North: The property to the north is I-64/ U.S 40-61.

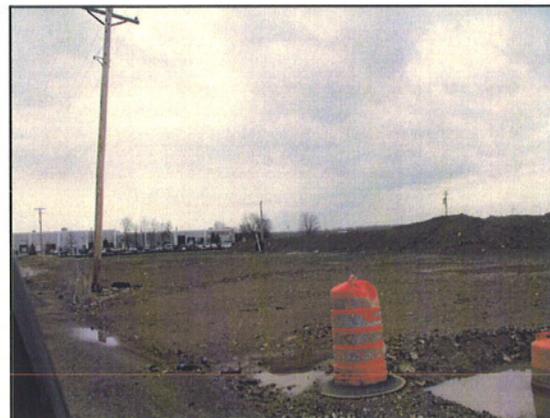
South: The property to the south is Spirit Trade Center and is zoned "M-3" Planned Industrial District

East: The property to the east is Terra Corporate Park and is zoned "PI" Planned Industrial District.

West: The property to the west is Waller-Hoch Corporate Park and Chesterfield Exchange and is zoned "PI" Planned Industrial District.



View looking north from Long Rd. Crossing Dr. across Lot 2 towards Lot 1



View Looking north from Lot 3

Comprehensive Plan Analysis

The subject site is located within Ward 4 of the City of Chesterfield. The Comprehensive Plan calls for Lot 1 to be Office Park Use and for Lots 2 and 3 to be Mixed Commercial Use. This subject site is not located in any sub-area identified by the Comprehensive Plan; therefore there are no additional development guidelines for this site.

Site Area History

On February 21, 2001, the City of Chesterfield approved Ordinance 1717 which changed the zoning from "M-3" Planned Industrial District to "PI" Planned Industrial District.

A public hearing is being held on this request tonight.

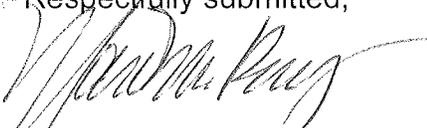
Issues

The City of Chesterfield has no outstanding issues.

Request

Staff recommends approval of the ordinance amendment with the Attachment A as written.

Respectfully submitted,



Mara M. Perry, AICP
Senior Planner of Site Plan Review

Attachments

1. Attachment A
2. Preliminary Plan (attached with Public Hearing Notice)

ATTACHMENT A

All provisions of the City of Chesterfield City Code shall apply to this development except as specifically modified herein.

I. SPECIFIC CRITERIA

A. PERMITTED USES

1. The uses allowed in this "PI" Planned Industrial District shall be:
 - a. Business, professional, and technical schools.
 - b. Business service establishments.
 - c. Cafeterias for employees and guests only.
 - d. Child care centers, nursery school, and day nurseries.
 - e. Dwelling or lodging units, only for watchmen, caretakers, or other personnel whose residence on the premises is essential to the operation of a permitted use or uses.
 - f. Financial institutions.
 - g. Manufacturing, fabrication, assembly, processing, or packaging of any commodity except:
 - i. Facilities producing or processing explosives or flammable gases or liquids;
 - ii. Facilities for animal slaughtering, meat packing or rendering;
 - iii. Sulpher plants, rubber reclamation plants, or cement plants; and
 - iv. Steel mills, foundries, or smelters.
 - h. Medical and dental offices.
 - i. Offices or office buildings.
 - k. Outpatient substance abuse treatment facilities.
 - l. Parking areas, including garages, for automobiles, but not including any sales of automobiles, or the storage of

wrecked or otherwise damaged and immobilized automotive vehicles for a period in excess of seventy-two (72) hours.

- m. Plumbing, electrical, air conditioning, and heating equipment sales, warehousing and repair facilities (indoor use only).
- n. Printing and duplicating services (indoor use only).
- o. Research facilities, professional and scientific laboratories, including photographic processing laboratories used in conjunction therewith.
- p. Warehousing, storage, or wholesaling of manufactured commodities, excluding live animals, explosives, or flammable gases and liquids.

2. The following Ancillary Uses shall be permitted:

- a. Automatic vending facilities for:
 - i. Ice and solid carbon dioxide (dry ice);
 - ii. Beverages;
 - iii. Confections.
- b. Cafeterias for employees and guests only.

3. The above uses shall be restricted as follows:

- a. Proposed Building "C" shall be primarily office according to the Chesterfield Valley Master Plan.

B. FLOOR AREA, HEIGHT, BUILDING AND PARKING STRUCTURE REQUIREMENTS

1. HEIGHT

- a. The maximum height of the building, exclusive of roof screening, shall not exceed thirty (30) feet or two stories whichever is less measured from final grade. The height of the existing building shall be permitted.

2. BUILDING REQUIREMENTS

- a. A minimum of thirty percent (30%) openspace is required for this development.

C. SETBACKS

1. STRUCTURE SETBACKS

No building or structure, other than: a freestanding project identification sign, boundary and retaining walls, light standards, flag poles or fences will be located within the following setbacks:

- a. Ninety (90) feet from the right-of-way of I-64/ U.S. 40-61. The existing Cambridge Engineering structure is setback sixty (60) feet from the new right-of-way of I-64/ U.S. 40-61 and shall remain as built. Upon the removal of the aforementioned structure, the ninety (90) foot setback shall be in effect.
- b. Fifty-five (55) feet from the southern boundary of this development at Building "B".
- c. One hundred sixty-five (165) feet from the southern boundary of this development at Building "C".
- d. Thirty (30) feet from the western boundary of this development.
- e. Ninety (90) feet from the eastern boundary of this development.
- f. Sixty (60) feet from Long Road Crossing Drive.
- g. Ten (10) feet from internal property lines.

2. PARKING SETBACKS

No parking stall, loading space, internal driveway, or roadway, except points of ingress and egress, will be located within the following setbacks:

- a. Thirty (30) feet from the right-of-way of I-64/U.S. 40-61. No parking shall be allowed in front of the existing Cambridge Engineering Building. Existing pavement is provided for Fire Protection District only.
- b. Thirty (30) feet from the southern boundary of this development.
- c. Twenty-five (25) feet from the east and west boundaries of this development with the exception of a shared driveway

along the western property line of Lot 2 bearing N 10° 51' 03"
W north of Long Road Crossing Drive.

- d. Fifteen (15) feet from Long Road Crossing Drive.
- e. Ten (10) feet from internal property lines with the exception of shared driveways.

D. PARKING AND LOADING REQUIREMENTS

- 1. Parking and loading spaces for this development will be as required in the City of Chesterfield Code.
- 2. Construction Parking
 - a. The streets surrounding this development and any street used for construction access thereto shall be cleaned throughout the day. The developer shall keep the road clear of mud and debris at all times.
 - b. Provide adequate off-street stabilized parking area(s) for construction employees and a washdown station for construction vehicles entering and leaving the site in order to eliminate the condition whereby mud from construction and employee vehicles is tracked onto the pavement causing hazardous roadway and driving conditions.
- 3. Parking lots shall not be used as streets.
- 4. No construction related parking shall be permitted within the Chesterfield Airport Road right of way.
- 5. No parking shall be permitted on any roadway in or adjacent to the development. The parking restriction and requirement for signage shall be indicated on the Site Development Plan and improvement plans. Signage shall be posted prior to approval of improvement plans or issuance of building permits for adjacent lots.

E. LANDSCAPE AND TREE REQUIREMENTS

The developer shall adhere to the Tree Manual of the City of Chesterfield Code.

F. SIGN REQUIREMENTS

1. Ornamental Entrance Monument construction, if proposed, shall be reviewed by the City of Chesterfield, and/or the St. Louis County Department of Highways and Traffic, for sight distance considerations prior to installation or construction.
2. No advertising signs, temporary signs, portable signs, off site signs, or attention getting devices shall be permitted in this development.
3. Signs shall be permitted in accordance with the regulations of the City of Chesterfield Code.

G. LIGHT REQUIREMENTS

Provide a lighting plan and cut sheet in accordance with the City of Chesterfield Code.

H. ARCHITECTURAL

1. The developer shall submit architectural elevations, including but not limited to, colored renderings and building materials. Architectural information is to be reviewed by the Architectural Review Board and the Planning Commission.
2. Building facades should be articulated by using color, arrangement or change in materials to emphasize the facade elements. The planes of the exterior walls may be varied in height, depth or direction. Extremely long facades shall be designed with sufficient building articulation and landscaping to avoid a monotonous or overpowering appearance.
3. Building "C" shall have the appearance of an office building. All other buildings shall be built of a combination of glass and concrete tilt-up panels and/or brick (or design similar to Spirit Trade Center structures) as approved by the Architectural Review Board.
4. No loading docks shall face north, east or west for a distance of Three hundred (300) feet from the south right-of-way of I-64/ U.S. 40-61.
5. Trash enclosures: The location, material, and elevation of any trash enclosures will be as approved by the Planning Commission on the Site Development Plan. All exterior trash areas will be enclosed with a six (6) foot high sight-proof enclosure complimented by adequate landscaping approved by the Planning Commission on the Site Development Plan.

6. Mechanical equipment will be adequately screened by roofing or other material as approved by the Planning Commission.

I. ACCESS/ACCESS MANAGEMENT

Access to Long Road Crossing Drive shall be limited to a maximum of two (2) drive entrances on the north side and two (2) drive entrances on the south. All drive entrances shall be a minimum of Fifty (50) feet from the eastern property line of the tract.

J. PUBLIC/PRIVATE ROAD IMPROVEMENTS, INCLUDING PEDESTRIAN CIRCULATION

1. Any request to install a gate at the entrance to this development must be approved by the City of Chesterfield and the St. Louis County Department of Highways and Traffic. No gate installation will be permitted on public right of way.
2. If a gate is installed on a street in this development, the streets within the development or that portion of the development that is gated shall be private and remain private forever.
3. Provide a forty (40) foot right of way with a minimum of ten (10) foot roadway improvement, maintenance, utility and drainage easements on both sides for Long Road Crossing Drive (f/k/a Chesterfield Business Parkway North) across the tract in accordance with the plans entitled "Chesterfield Business Parkway North Road Improvements" on file in the Department of Public Works. In addition, provide additional right of way and easements for the Spirit Drive North/Long Road Crossing Drive intersection and associated roundings as depicted on said plans.

The roadways have been constructed to provide a twenty-six (26) foot travelway with seven (7) foot shoulders on both sides. Appurtenant storm drainage facilities shall be provided as directed by the Department of Public Works when adjacent lots develop.

4. Provide a five (5) foot wide sidewalk, conforming to ADA standards, along both sides of Long Road Crossing Drive. The sidewalk shall be privately maintained; therefore, no public easements shall be required.

K. POWER OF REVIEW

The Mayor or a Councilmember of the Ward in which a development is proposed may request that the site plan be reviewed and approved by the entire City Council. This request must be made no later than 24 hours

before posting the agenda for the next City Council meeting after Planning Commission review and approval of the site plan. The City Council will then take appropriate action relative to the proposal.

L. STORMWATER

1. The site shall provide for the positive drainage of storm water and it shall be discharged at an adequate natural discharge point or connected to an adequate piped system.
2. Detention/retention and other storm water quantity and quality management measures are to be provided in each watershed as required by the City of Chesterfield. The storm water quantity management facilities, related to flood and channel protection, shall be operational prior to paving of any driveways or parking areas in non-residential development or issuance of building permits exceeding sixty (60%) of approved dwelling units in each plat, watershed or phase of residential developments. The location and types of storm water management facilities shall be identified on the Site Development Plan.
3. Per the Chesterfield Valley Master Storm Water Plan, ten (10) foot wide flat bottom ditches have been constructed along the north, east and west property lines of this site, with drainage directed to the east to the pump station at Long Road. Provide Chesterfield Valley Storm Water Easements along the north, east and west property lines, as required and directed by the Department of Public Works, to accommodate the Chesterfield Valley Master Storm Water Plan channel in those areas, and depict the channel on all Site Development Plans and improvement plans. Maintenance of the required channel shall be the responsibility of the property owner.

M. GEOTECHNICAL REPORT.

Provide a geotechnical report, prepared by a registered professional engineer licensed to practice in the State of Missouri, as directed by the Department of Public Works. The report shall verify the suitability of grading and proposed improvements with soil and geologic conditions and address the existence of any potential sinkhole, ponds, dams, septic fields, etc., and recommendations for treatment. A statement of compliance, signed and sealed by the geotechnical engineer preparing the report, shall be included on all Site Development Plans and Improvement Plans.

N. MISCELLANEOUS

1. All utilities will be installed underground. The development of this parcel will coordinate the installation of all utilities in conjunction with the construction of any roadway on site.
2. Prior to the issuance of an occupancy permit, any existing stormwater channel located on this site and between this site and the pump station to which it should drain shall be regraded to restore the channel to the line and grade of the original design.

II. TIME PERIOD FOR SUBMITTAL OF SITE DEVELOPMENT CONCEPT PLANS AND SITE DEVELOPMENT PLANS

- A. The developer shall submit a concept plan within eighteen (18) months of City Council approval of the change of zoning.
- B. In lieu of submitting a Site Development Concept Plan and Site Development Section Plans, the petitioner may submit a Site Development Plan for the entire development within eighteen (18) months of the date of approval of the change of zoning by the City.
- C. Failure to comply with these submittal requirements will result in the expiration of the change of zoning and will require a new public hearing.
- D. Said Plan shall be submitted in accordance with the combined requirements for Site Development Section and Concept Plans. The submission of Amended Site Development Plans by sections of this project to the Planning Commission shall be permitted if this option is utilized.
- E. Where due cause is shown by the developer, this time interval for plan submittal may be extended through appeal to and approval by the Planning Commission.

III. COMMENCEMENT OF CONSTRUCTION

- A. Substantial construction shall commence within two (2) years of approval of the site development concept plan or site development plan, unless otherwise authorized by ordinance.
- B. Where due cause is shown by the developer, the Commission may extend the period to commence construction for not more than one additional year.

IV. GENERAL CRITERIA

A. SITE DEVELOPMENT CONCEPT PLAN SUBMITTAL REQUIREMENTS

The Site Development Concept Plan shall include, but not be limited to, the following:

1. Outboundary plat and legal description of the property.
2. Location of all roadways adjacent to the property and general location, size, and pavement widths of all interior roadways.
3. General design of the development including unit types (i.e., single-family detached, single-family attached, garden apartment), number of each unit type proposed, location of units, minimum and maximum size of single-family lots, approximate size of multiple-family structures, and location and size of common areas and recreation facilities.
4. Location and size of any commercial uses, types of uses proposed and general parking layout.
5. Zoning district lines and floodplain boundaries.
6. Density calculations.
7. Provide a conceptual landscape plan in accordance with the City of Chesterfield Code.
8. Provide a lighting plan in accordance with the City of Chesterfield Code.

B. SITE DEVELOPMENT SECTION PLAN SUBMITTAL REQUIREMENTS

The Site Development Section Plan shall adhere to the above criteria and to the following:

1. Location map, north arrow, and plan scale. The scale shall be no greater than one (1) inch equals one hundred (100) feet.
2. Parking calculations. Including calculation for all off street parking spaces, required and proposed, and the number, size and location for handicap designed.
3. Provide open space percentage for overall development including separate percentage for each lot on the plan.

4. Provide Floor Area Ratio (F.A.R.).
5. A note indicating all utilities will be installed underground.
6. A note indicating signage approval is separate process.
7. Depict the location of all buildings, size, including height and distance from adjacent property lines and proposed use.
8. Specific structure and parking setbacks along all roadways and property lines.
9. Indicate location of all existing and proposed freestanding monument signs
10. Zoning district lines, subdivision name, lot number, dimensions, and area, and zoning of adjacent parcels where different than site.
11. Floodplain boundaries.
12. Depict existing and proposed improvements within 150 feet of the site as directed. Improvements include, but are not limited to, roadways, driveways and walkways adjacent to and across the street from the site, and significant natural features, such as wooded areas and rock formations, that are to remain or be removed.
13. Depict all existing and proposed easements and rights-of-way within 150 feet of the site and all existing or proposed off-site easements and rights-of-way required for proposed improvements.
14. Indicate the location of the proposed storm sewers, detention basins, sanitary sewers and connection(s) to the existing systems.
15. Depict existing and proposed contours at intervals of not more than one (1) foot, and extending 150 feet beyond the limits of the site as directed.
16. Address trees and landscaping in accordance with the City of Chesterfield Code.
17. Comply with all preliminary plat requirements of the City of Chesterfield Subdivision Regulations per the City of Chesterfield Code.
18. Signed and sealed in conformance with the State of Missouri Department of Economic Development, Division of Professional

Registration, Missouri Board for Architects, Professional Engineers and Land Surveyors requirements.

19. Provide comments/approvals from the appropriate Fire District, the Metropolitan St. Louis Sewer District, Monarch Levee District, Spirit of St. Louis Airport and the Missouri Department of Transportation.
20. Compliance with Sky Exposure Plane.

V. CHESTERFIELD VALLEY TRUST FUND

A. Roads

The roadway improvement contribution is based on land and building use. The roadway contributions are necessary to help defray the cost of engineering, right-of-way acquisition, and major roadway construction in accordance with the Chesterfield Valley Road Improvement Plan on file with the St. Louis County Department of Highways and Traffic. The amount of the developer's contribution to this fund shall be computed on the basis of the following:

<u>Type of Development</u>	<u>Required Contribution</u>
Industrial	\$3,685.05/acre of gross acreage

If the types of development proposed differ from those listed, rates shall be provided by the St. Louis County Department of Highways and Traffic.

Credits for roadway improvements required in condition will be awarded as directed by the St. Louis County Highways and Traffic. Any portion of the roadway improvement contribution which remains, following completion of road improvements required by the development shall be retained in the trust fund.

The roadway improvement contribution shall be deposited with the St. Louis County Department of Highways and Traffic. The deposit shall be made prior to the issuance of a Special Use Permit (S.U.P.) by St. Louis County Highways and Traffic. Funds shall be payable to the Treasurer, St. Louis County.

B. Water Main

The primary water line contribution is based on gross acreage of the development land area. The contribution shall be a sum of \$486.43 per acre for the total area as approved on the Site Development Plan to be used solely to help defray the cost of constructing the primary water line serving the Chesterfield Valley area.

The primary water line contributions shall be deposited with the St. Louis County Department of Highways and Traffic. The deposit shall be made prior to approval of the Site Development Plan unless otherwise directed by the St. Louis County Department of Highways and Traffic. Funds shall be payable to the Treasurer, St. Louis County.

C. Stormwater

The stormwater contribution is based on the gross acreage of the development land area. These funds are necessary to help defray the cost of engineering and construction improvements for the collection and disposal of stormwater from the Chesterfield Valley in accordance with the Master Plan on file with and jointly approved by St. Louis County and the Metropolitan St. Louis Sewer District. The amount of the stormwater contribution will be computed on the basis of \$1,534.99 per acre for the total area as approved on the Site Development Plan.

The stormwater contributions to the Trust Fund shall be deposited with the St. Louis County Department of Highways and Traffic. The deposit shall be made prior to the issuance of a Special Use Permit (S.U.P.) by St. Louis County Department of Highways and Traffic. Funds shall be payable to the Treasurer, St. Louis County.

D. Sanitary Sewer

The sanitary sewer contribution is collected as the Caulks Creek impact fee.

The sanitary sewer contributions within Chesterfield Valley area shall be deposited with the Metropolitan Sewer District as required by the District.

The amount of this required contribution for the roadway, stormwater and primary waterline improvements, if not submitted by January 1, 2001 shall be adjusted on that date and on the first day of January in each succeeding year thereafter in accord with the construction cost index as determined by the St. Louis County Departments of Highway and Traffic.

Trust fund contributions shall be deposited with St. Louis County in the form of a cash escrow prior to the issuance of building permits.

VI. RECORDING

Within 60 days of approval of any development plan by the City of Chesterfield, the approved Plan will be recorded with the St. Louis County Recorder of Deeds. Failure to do so will result in the expiration of approval of said plan and require re-approval of a plan by the Planning Commission.

VII. ENFORCEMENT

- A. The City of Chesterfield, Missouri will enforce the conditions of this ordinance in accordance with the Site Development Concept Plan and Site Development Section Plans approved by the City of Chesterfield and the terms of this Attachment A.
- B. Failure to comply with any or all the conditions of this ordinance will be adequate cause for revocation of approvals/permits by reviewing Departments and Commissions.
- C. Non-compliance with the specific requirements and conditions set forth in this Ordinance and its attached conditions or other Ordinances of the City of Chesterfield shall constitute an ordinance violation, subject, but not limited to, the penalty provisions as set forth in the City of Chesterfield Code.
- D. Waiver of Notice of Violation per the City of Chesterfield Code.
- E. This document shall be read as a whole and any inconsistency to be integrated to carry out the overall intent of this Attachment A.



VIII.H

690 Chesterfield Pkwy W • Chesterfield MO 63017-0760
Phone: 636-537-4000 • Fax 636-537-4798 • www.chesterfield.mo.us

Planning Commission Staff Report

Subject: Ordinance Amendment Vote Report

Meeting Date: May 14, 2007

From: Aimee Nassif, Senior Planner of Zoning Administration

Location: 16845 North Outer Forty Road

Petition: P.Z. 16-2007 Valley Gates Subdivision

Proposal Summary

George Stock, on behalf of Summit Outer Forty Investors, has submitted an application for an ordinance amendment to allow for a change to the parking and structure setbacks, a change in the building height requirement and to allow for additional buildings on the site.

The current ordinance, Ordinance Number 2154 requires a maximum height of two (2) stories and permits only two (2) buildings to be developed. The Petitioner is requesting a maximum height for buildings to be three (3) stories and to allow for an increased number of buildings to be built on site.

Staff Recommendation

The Attachment A for this request meets all of the development requirements of the City of Chesterfield and therefore, Staff recommends approval of the ordinance amendment.

Ordinance Amendment Analysis

The Petitioner is requesting an ordinance amendment to allow for a change to the parking and structure setbacks, change in building height, and to allow for additional buildings on the site. The preliminary plan submitted meets all the requirements of the Attachment A as written.

Surrounding Land Use and Zoning

The land use and zoning for the properties surrounding this parcel are as follows:

- North: The property to the north is zoned "FPNU" Flood Plain Non-Urban District.
- South: To the south is North Outer Forty Road and beyond that is Chesterfield Commons East which is zoned "PC" Planned Commercial District.
- East: The property to the east is the Chesterfield Valley Nursery and is zoned "NU" Non-Urban District.
- West: The property to the west is Summit Center and is zoned "PC" Planned Commercial District.



View looking north across the subject site.

Comprehensive Plan Analysis

The subject site is located within Ward 4 of the City of Chesterfield. The Comprehensive Plan calls for this area to be Mixed Commercial Use. This subject site is not located in any sub-area identified by the Comprehensive Plan; therefore there are no additional development guidelines for this site.

Site Area History

The subject site was zoned from "NU" Non-Urban District to "PC" Planned Commercial District in February 2005. At the time of the original rezoning request, the Petitioner had only requested two (2) buildings be developed on this site.

Issues

A public hearing was held on this request on April 23, 2007. There were no speakers regarding this request. At the time of the public hearing, the only outstanding issue was the City of Chesterfield was awaiting agency comments to complete the ordinance request. Those comments have been received and are incorporated into the Attachment A.

Request

Staff recommends approval of the ordinance amendment with the Attachment A as written.

Respectfully submitted,



Aimee Nassif
Senior Planner of Zoning Administration

Attachments

1. Attachment A
2. Preliminary Plan

ATTACHMENT A

All provisions of the City of Chesterfield City Code shall apply to this development except as specifically modified herein.

I. SPECIFIC CRITERIA

A. PERMITTED USES

1. The uses allowed in this "PC" District shall be:
 - a. Arenas and stadiums.
 - b. Associated work and storage areas required by a business, firm, or service to carry on business operations.
 - c. Auditoriums, churches, clubs, lodges, meeting rooms, libraries, reading rooms, theaters, or any other facility for public assembly.
 - d. Child care centers, nursery schools, and day nurseries.
 - e. Colleges and universities.
 - f. Financial institutions.
 - g. Hospitals.
 - h. Hotels and motels.
 - i. Medical and dental offices.
 - j. Offices or office buildings.
 - k. Recreational facilities, indoor and illuminated outdoor facilities, including swimming pools, golf courses, golf practice driving ranges, tennis courts, and gymnasiums, and indoor theaters, including drive-in theaters.
 - l. Research facilities, professional and scientific laboratories, including photographic processing laboratories used in conjunction therewith.
 - m. Restaurants, fast food. (No drive-thru)
 - n. Restaurants, sit down.

- o. Schools for business, professional or technical training, but not including outdoor areas for driving or heavy equipment training.
- p. Stores, shops, markets, service facilities, and automatic vending facilities in which goods or services of any kind, including indoor sale of motor vehicles, are being offered for sale or hire to the general public on the premises.

B. FLOOR AREA, HEIGHT, BUILDING AND PARKING STRUCTURE REQUIREMENTS

1. HEIGHT

- a. The maximum height of retail buildings, exclusive of roof screening, shall not exceed two (2) stories.
- b. The maximum height of all other buildings, exclusive of roof screening, shall not exceed three (3) stories.

2. BUILDING REQUIREMENTS

- a. A minimum of forty percent (40%) openspace is required for this development.

C. SETBACKS

1. STRUCTURE SETBACKS

No building or structure, other than: a freestanding project identification sign, boundary and retaining walls, light standards, flag poles or fences will be located within the following setbacks:

- a. One hundred and twenty feet (120) from the new right-of-way of North Outer Forty.
- b. Nineteen (19) feet from the eastern boundary of the "PC" District.
- c. Twenty five (25) feet from the western boundary of the "PC" District.
- d. Two hundred (200) feet from the northern boundary of the "PC" District.

2. PARKING SETBACKS

- a. No parking stall, loading space, internal driveway, or roadway, except points of ingress or egress, will be located within the following setbacks:

- i. Thirty (30) feet from the right-of-way of North Outer Forty Road.
- ii. Nineteen (19) feet from the eastern boundary of this development.
- iii. Twenty five (25) feet from the western boundary of the "PC" District.
- iv. One hundred seventy (170) feet from the northern boundary of the "PC" District.
- v. Ten (10) feet from internal property lines, with the exception of shared driveways.
- vi. Fifteen (15) feet from the principal internal street.

D. PARKING AND LOADING REQUIREMENTS

1. Parking and loading spaces for this development will be as required in the City of Chesterfield Code.
2. Construction Parking
 - a. The streets surrounding this development and any street used for construction access thereto shall be cleaned throughout the day. The developer shall keep the road clear of mud and debris at all times.
 - b. Provide adequate off-street stabilized parking area(s) for construction employees and a washdown station for construction vehicles entering and leaving the site in order to eliminate the condition whereby mud from construction and employee vehicles is tracked onto the pavement causing hazardous roadway and driving conditions.
3. Parking lots shall not be used as streets.

E. LANDSCAPE AND TREE REQUIREMENTS

1. The developer shall adhere to the Tree Manual of the City of Chesterfield Code.
2. Since landscaping is not permitted in the underseepage berm area, landscape provisions for said area, including but not exclusive to landscaped parking islands and perimeter landscaping requirements will not be required to adhere to the Tree Manual.

Landscaping plans will include additional landscaping provisions in the front portion of the building area which will buffer the lack of landscaping in the rear of the property. The landscape features may include but not be limited to water features, brick planters and outdoor artwork.

F. SIGN REQUIREMENTS

1. Ornamental Entrance Monument construction, if proposed, shall be reviewed by the City of Chesterfield, and/or the St. Louis County Department of Highways and Traffic, for sight distance considerations prior to installation or construction.
2. Signs shall be permitted in accordance with the regulations of the City of Chesterfield Code.

G. LIGHT REQUIREMENTS

1. Provide a lighting plan and cut sheet in accordance with the City of Chesterfield Code.

H. ARCHITECTURAL

1. The developer shall submit architectural elevations, including but not limited to, colored renderings and building materials. Architectural information is to be reviewed by the Architectural Review Board and the Planning Commission.
2. Building facades should be articulated by using color, arrangement or change in materials to emphasize the facade elements. The planes of the exterior walls may be varied in height, depth or direction. Extremely long facades shall be designed with sufficient building articulation and landscaping to avoid a monotonous or overpowering appearance.
3. Trash enclosures: The location, material, and elevation of any trash enclosures will be as approved by the Planning Commission on the Site Development Plan. All exterior trash areas will be enclosed with a six (6) foot high sight-proof enclosure complimented by adequate landscaping approved by the Planning Commission on the Site Development Plan.
4. Mechanical equipment will be adequately screened by roofing or other material as approved by the Planning Commission.

I. ACCESS/ACCESS MANAGEMENT

1. Provide cross access easements or other appropriate legal instruments guaranteeing permanent access to adjacent properties as directed by the City of Chesterfield.
2. Direct access to the North Outer Forty Road shall be as approved by MoDOT and the City of Chesterfield.
3. The centerline of the single entrance onto the North Outer Forty Road shall be located no closer than two hundred and twenty (220) feet from the western property line, or as directed by the City of Chesterfield and MoDOT.

J. PUBLIC/PRIVATE ROAD IMPROVEMENTS, INCLUDING PEDESTRIAN CIRCULATION

1. Provide any additional right-of-way and pavement widening to the North Outer Forty Road as required by MoDOT.
2. Provide a five (5) foot wide sidewalk, conforming to ADA standards, parallel to North Outer Forty Road per the Valley Guidelines of the Comprehensive Plan and a four (4) foot wide sidewalk conforming to ADA standards along both sides of all interior roadways, as directed by the City of Chesterfield. The sidewalks shall be privately maintained; therefore, no public easements shall be required.
3. Provide a 40 foot right of way with minimum of 10 foot roadway improvement, maintenance, utility and sewer easements on both sides for all interior roadways. Minimum roadway sections shall provide a 26 foot travel way with 7 foot shoulders on both side and appurtenant storm drainage facilities as directed by the Department of Public Works.

K. TRAFFIC STUDY

1. Provide a traffic study as directed by the City of Chesterfield and/or the Missouri Department of Transportation. The scope of the study shall include internal and external circulation and may be limited to site specific impacts, such as the need for additional lanes, entrance configuration, geometrics, sight distance, traffic signal modifications or other improvements required, as long as the density of the proposed development falls within the parameters of the City's traffic model. Should the density be other than the density assumed in the model, regional issues shall be addressed as directed by the City of Chesterfield.

L. MONARCH-CHESTERFIELD LEVEE DISTRICT

1. The developer shall dedicate an underseepage berm easement adjacent to the existing levee, as directed by the Monarch Chesterfield Levee District and the City of Chesterfield's Department of Public Works.
2. Prior to approval of any grading permit or improvement plans for development, an underseepage study may be required for review/approval as directed by the Monarch Chesterfield Levee District, the US Army Corps of Engineers and the City of Chesterfield's Department of Public Works.
3. No improvements or use that would alter, diminish, damage or interfere with the performance of the Monarch Chesterfield Levee including seepage berm is permitted.
4. Any drainage structures or systems, including or not limited to under drains, installed for the relief of hydrostatic head or seepage water shall be designed, construed, operated, and maintained to prevent infiltration of soil/ground loss.
5. Trees, vegetation and landscaping which have roots which extend more than six (6) inches in depth below the ground are not permitted in the Permanent Underseepage Berm Easement. Buildings and other structures are also not permitted within this easement.

M. RECREATIONAL EASEMENT

1. An easement, for recreational and trail purposes shall be provided for this site as directed by the Monarch Chesterfield Levee District and the City of Chesterfield's Department of Public Works. The easement is anticipated to be located on the crest of the levee.

N. POWER OF REVIEW

1. The Mayor or a Councilmember of the Ward in which a development is proposed may request that the site plan be reviewed and approved by the entire City Council. This request must be made no later than 24 hours before posting the agenda for the next City Council meeting after Planning Commission review and approval of the site plan. The City Council will then take appropriate action relative to the proposal.

O. STORMWATER

1. The site shall provide for the positive drainage of storm water and it shall be discharged at an adequate natural discharge point or connected to an adequate piped system.
2. Detention/retention and other storm water quantity and quality management measures are to be provided in each watershed as required by the City of Chesterfield. The storm water quantity management facilities, related to flood and channel protection, shall be operational prior to paving of any driveways or parking areas in non-residential development or issuance of building permits exceeding sixty (60%) of approved dwelling units in each plat, watershed or phase of residential developments. The location and types of storm water management facilities shall be identified on the Site Development Plan.
3. Due to the inherent nature of development, the specific size, location, and configuration of the stormwater infrastructure are conceptual in nature. The exact location, size, and type of each segment of stormwater infrastructure are to be reviewed and approved in conjunction with the development of specific sites. It is expected that developers will submit alternate plans, propose alternative geometry, size, and type for these infrastructure improvements, along with supporting hydraulic computations. The Public Works Department will review said proposals for functional equivalence. Functional equivalence is said to be achieved when, as determined by the Director of Public Works, the alternate proposal provides the same hydraulic function, connectivity, and system wide benefits without adversely affecting water surface profiles at other locations or adjacent properties.
4. The Chesterfield Valley Master Storm Water Plan indicates that the drainage shall be directed to the east to a future pump station, via a 30 foot wide flat bottom ditch with 4:1 side slopes, and a double 8' x 4' driveway culvert. Please be advised that the petitioner shall coordinate improvements with the adjacent parcel owners. In addition, in the event that the ultimate permanent improvements cannot be constructed at the time of development, the site shall be designed in such a manner as to allow the ultimate construction of the master plan at a future date, and interim drainage facilities shall be provided. The interim facilities may include a temporary pump station as necessary to provide the required positive drainage. Interim facilities shall be removed promptly at such time as the permanent stormwater improvements can be constructed as required. The petitioner will be required to provide a special cash escrow to the City for the cost of constructing the master plan drainage ditch along their property in the event that the ditch is not constructed at the time of development. No building permits for the

development will be issued until the City has received the special cash escrow. The special cash escrow shall include all work associated with constructing the master plan drainage facility, or reconfiguring the interim improvement. This shall include, but is not limited to, grading, siltation control, revegetation, and removal of driveway entrances, removal of driveway culverts, the construction of culverts and the reconstruction of driveway entrances.

5. The maintenance of the required stormwater/ditch system shall be the responsibility of the property owner(s).
6. Stormwater drainage improvements shall be operational prior to the paving of any driveways or parking lots. Roadway and related improvements shall be constructed prior to 60% occupancy of the retail portion of the site.
7. If any development in, or alteration of, the floodplain is proposed, the developer shall submit a Floodplain Development Permit/Application to the City of Chesterfield Department of Public Works for approval. Be advised that in conjunction with any site alteration the developer will be required to demonstrate that there will be no adverse effect on other properties located within the Chesterfield Valley. The Floodplain Development Permit/Application must be approved by the City of Chesterfield Department of Public Works prior to the approval of the Improvement Plans or Grading Plans. If any change in the location of the Special Flood Hazard Area is proposed, the Developer shall be required to obtain a Letter of Map Revision (LOMR) from the Federal Emergency Management Agency. The LOMR must be issued by FEMA prior to the final release of any escrow held by the City of Chesterfield for improvements in the development.
8. If any building is proposed to be located in an existing, or proposed, Special Flood Hazard Area the building shall be clearly labeled as being located in the Floodplain on the Approved Site Development Plan and Improvement Plan. The lowest Reference Level (floor), as defined by FEMA, shall be constructed a minimum of one (1) foot above the base flood elevation unless a LOMR has been issued by FEMA prior to construction. The minimum elevation for the Reference Level for each building shall also be noted on the approved Site Development Plan and Improvement Plan.

P. SANITARY SEWER

1. Hydraulic calculations of downstream sewers will be required by the Metropolitan St. Louis Sewer District and the City of Chesterfield. Downstream sanitary reaches may need to be replaced or

upgraded, as directed by the Metropolitan St. Louis Sewer District and the City of Chesterfield.

Q. GEOTECHNICAL REPORT.

Provide a geotechnical report, prepared by a registered professional engineer licensed to practice in the State of Missouri, as directed by the Department of Public Works. The report shall verify the suitability of grading and proposed improvements with soil and geologic conditions and address the existence of any potential sinkhole, ponds, dams, septic fields, etc., and recommendations for treatment. A statement of compliance, signed and sealed by the geotechnical engineer preparing the report, shall be included on all Site Development Plans and Improvement Plans.

R. MISCELLANEOUS

1. All utilities will be installed underground. The development of this parcel will coordinate the installation of all utilities in conjunction with the construction of any roadway on site.
2. If any work is proposed in, near or above the natural watercourse through this site, a hydrologic/hydraulic study evaluation the impacts of the proposed work shall be provided as directed by the City of Chesterfield's Department of Public Works. Said study shall evaluate impacts on the entire length of stream, and any maintenance requirements. Appropriate permits and approvals from other agencies shall also be provided.

II. TIME PERIOD FOR SUBMITTAL OF SITE DEVELOPMENT CONCEPT PLANS AND SITE DEVELOPMENT PLANS

- A. The developer shall submit a concept plan within 18 months of City Council approval of the change of zoning.
- B. In lieu of submitting a Site Development Concept Plan and Site Development Section Plans, the petitioner may submit a Site Development Plan for the entire development within 18 months of the date of approval of the change of zoning by the City.
- C. Failure to comply with these submittal requirements will result in the expiration of the change of zoning and will require a new public hearing.
- D. Said Plan shall be submitted in accordance with the combined requirements for Site Development Section and Concept Plans. The submission of Amended Site Development Plans by sections of this project to the Planning Commission shall be permitted if this option is utilized.

- E. Where due cause is shown by the developer, this time interval for plan submittal may be extended through appeal to and approval by the Planning Commission.

III. COMMENCEMENT OF CONSTRUCTION

- A. Substantial construction shall commence within two (2) years of approval of the site development concept plan or site development plan, unless otherwise authorized by ordinance.
- B. Where due cause is shown by the developer, the Commission may extend the period to commence construction for not more than one additional year.

IV. GENERAL CRITERIA

- A. Site Development Concept Plan, Site Development Section Plan, Site Development Plans.**
 - 1. Any site development plan shall show all information required on a sketch plan as required in the City of Chesterfield Code.
 - 2. Include a conceptual landscape plan in accordance with the City of Chesterfield Code to indicate proposed landscaping along arterial and collector roadways.
 - 3. Include a lighting plan in accordance with the City of Chesterfield Code to indicate proposed lighting along arterial collector roadways.
 - 4. Provide comments/approvals from the appropriate Fire District, the Metropolitan St. Louis Sewer District, the St. Louis County Department of Highways and Traffic, Monarch Levee District, Spirit of St. Louis Airport and the Missouri Department of Transportation.

V. CHESTERFIELD VALLEY TRUST FUND

The Developer shall contribute to the Chesterfield Valley Trust Fund.

A. Roads

The roadway improvement contribution is based on land and building use. The roadway contributions are necessary to help defray the cost of engineering, right-of-way acquisition, and major roadway construction in accordance with the Chesterfield Valley Road Improvement Plan on file with the St. Louis County Department of Highways and Traffic. The amount of the developer's contribution to this fund shall be computed on

the following basis:

<u>Type of Development Contribution</u>	<u>Required</u>
Commercial	\$1.80/SF

If the types of development proposed differ from those listed, rates shall be provided by the St. Louis County Department of Highway and Traffic.

Credits for roadway improvements required in condition will be awarded as directed by the St. Louis County Highways and Traffic. Any portion of the roadway improvement contribution which remains, following completion of road improvements required by the development shall be retained in the trust fund.

The roadway improvement contribution shall be deposited with the St. Louis County Department of Highways and Traffic. The deposit shall be made prior to the issuance of a Special Use Permit (S.U.P.) by St. Louis County Highways and Traffic. Funds shall be payable to the Treasurer, St. Louis County.

B. Water Main

The primary water line contribution is based on gross acreage of the development land area. The contribution shall be a sum of \$568.82 per acre for the total area as approved on the Site Development Plan to be used solely to help defray the cost of constructing the primary water line serving the Chesterfield Valley area.

The primary water line contributions shall be deposited with the St. Louis County Department of Highways and Traffic. The deposit shall be made prior to approval of the Site Development Plan unless otherwise directed by the St. Louis County Department of Highway and Traffic. Funds shall be payable to the Treasurer, St. Louis County.

C. Stormwater

The stormwater contribution is based on gross acreage of the development land area. These funds are necessary to help defray the cost of engineering and construction improvements for the collection and disposal of stormwater from the Chesterfield Valley in accordance with the Master Plan on file with and jointly approved by St. Louis County and the Metropolitan St. Louis Sewer District. The amount of the stormwater contribution will be computed on the basis of \$1,804.75 per acre for the total area as approved on the Site Development Plan.

The stormwater contributions to the Trust Fund shall be deposited with the St. Louis County Department of Highways and Traffic. The deposit shall be made prior to the issuance of a Special Use Permit (S.U.P.) by St. Louis County Department of Highways and Traffic. Funds shall be payable to the Treasurer, St. Louis County.

D. Sanitary Sewer

The sanitary sewer contribution is collected as the Caulks Creek impact fee and is subject to a surcharge of \$2,750/acre. The sanitary sewer contributions within Chesterfield Valley area shall be deposited with the Metropolitan Sewer District as required by the District.

The amount of this required contribution for the roadway, stormwater and primary waterline improvements, if not submitted by January 1, 2005 shall be adjusted on that date and on the first day of January in each succeeding year thereafter in accord with the construction cost index as determined by the St. Louis County Departments of Highway and Traffic.

Except as otherwise noted, trust fund contributions shall be deposited with St. Louis County in the form of a cash escrow prior to the issuance of building permits

VI. RECORDING

Within 60 days of approval of any development plan by the City of Chesterfield, the approved Plan will be recorded with the St. Louis County Recorder of Deeds. Failure to do so will result in the expiration of approval of said plan and require re-approval of a plan by the Planning Commission.

VII. ENFORCEMENT

- A. The City of Chesterfield, Missouri will enforce the conditions of this ordinance in accordance with the Plan approved by the City of Chesterfield and the terms of this Attachment A.
- B. Failure to comply with any or all the conditions of this ordinance will be adequate cause for revocation of approvals/permits by reviewing Departments and Commissions.
- C. Non-compliance with the specific requirements and conditions set forth in this Ordinance and its attached conditions or other Ordinances of the City of Chesterfield shall constitute an ordinance violation, subject, but not limited to, the penalty provisions as set forth in the City of Chesterfield Code.
- D. Waiver of Notice of Violation per the City of Chesterfield Code.
- E. This document shall be read as a whole and any inconsistency to be integrated to carry out the overall intent of this Attachment A.

